

**AGENDA**  
**COMMITTEE OF THE WHOLE MEETING**  
**Village Hall – Board Room**  
**Monday, April 8, 2019**  
**Following Regular Village Board Meeting**

*Reasonable accommodations / auxiliary aids will be provided to enable persons with disabilities to effectively participate in any public meetings of the Board. Please contact the Village Administrative Office (847.883.8600) 48 hours in advance if you need special accommodations to attend. The Committee of the Whole will not proceed past 10:30 p.m. unless there is a consensus of the majority of the Trustees to do so. Citizens wishing to address the Board on agenda items may speak when the agenda item is open, prior to Board discussion.*

**CALL TO ORDER**

**1.0 ROLL CALL**

**2.0 APPROVAL OF MINUTES**

- 2.1 Acceptance of the March 18, 2019 Committee of the Whole Meeting Minutes

**3.0 ITEMS OF GENERAL BUSINESS**

3.1 Planning, Zoning and Land Use

- 3.11 Consideration of Approval of Roof-Mounted Solar Panel Installation for Building C of Extra Space Storage Facility – 200 Parkway Drive (Rethink Electric LLC)
- 3.12 Consideration of an Ordinance Amending Ordinance #11-3191-13A Related to Marriott's Lincolnshire Resort Master Sign Plans (Marriott Lincolnshire Resort – 10 Marriott Drive)
- 3.13 Discussion and Direction Regarding the Village of Buffalo Grove's Draft Plan for the Prairie View Metra Station Area Plan as it Relates to the Existing Boundary and Planning Agreement Between the Village of Lincolnshire and Village of Buffalo Grove (Village of Lincolnshire)

3.2 Finance and Administration

3.3 Public Works

3.4 Public Safety

3.5 Parks and Recreation

- 3.51 Consideration of a Picnic Permit Fee Waiver Request Pursuant to Section 8-1B-2(O) of the Village Code (Girl Scout Troop #45713 & Troop #45936)
- 3.52 Consideration of a Name and Dedication Date for Lincolnshire Downtown Pocket Park (Village of Lincolnshire)

3.6 Judiciary and Personnel

**4.0 UNFINISHED BUSINESS**

**5.0 NEW BUSINESS**

**6.0 EXECUTIVE SESSION**

**7.0 ADJOURNMENT**



**MINUTES  
COMMITTEE OF THE WHOLE MEETING  
Monday, March 18, 2019**

Present:

Mayor Brandt	Trustee Harms Muth
Trustee Grujanac	Trustee Hancock
<del>Trustee McDonough</del>	Trustee Servi
Trustee Leider	<del>Village Clerk Mastandrea</del>
Village Attorney Asprooth	Village Manager Burke
Finance Director/Treasurer Peterson	Public Works Director Woodbury
<del>Chief of Police Leonas</del>	Assistant Village Manager/Community &
Planning & Development Manager Zozulya	Economic Development Director Gilbertson

**ROLL CALL**

Mayor Brandt called the meeting to order at 7:25 p.m., and Village Manager Burke called the Roll.

**2.0 APPROVAL OF MINUTES**

**2.1 Acceptance of the March 11, 2019 Committee of the Whole Meeting Minutes**

The minutes of the March 11, 2019 Committee of the Whole Meeting were approved as submitted.

**3.0 ITEMS OF GENERAL BUSINESS**

**3.1 Planning, Zoning and Land Use**

**3.11 Preliminary Evaluation of a Site Plan and Building Elevations for New Medical Office Building - 231 Olde Half Day Road (Alexander and Julia Katsnelson)**

Assistant Village Manager/Community & Economic Development (CED) Director Gilbertson provided a summary of a site plan and building elevations for a proposed medical office building – 231 Olde Half Day Road. The petitioner plans to acquire the property, demolish the existing building, and build a new 5,000 square foot oral maxillofacial and dermatology medical office. Staff has encouraged the petitioner to work with the neighbors and talk about features of the site and possible cross access to Oak Tree Corners.

Dr. Alexander Katsnelson provided a presentation regarding a proposed site plan and building elevations for a proposed medical office building - 231 Olde Half Day Road which included benefits for the Village, renderings of the new building, site plan, parking of the proposed building, and a summary of his practice.

Trustee Hancock asked how placement of the building was determined for the proposed new structure. Dr. Katsnelson stated the proposed building was designed from the inside out and it was determined the need was for a wide building which fit best on the site where proposed.

Mayor Brandt asked if there was a fence between the property and Half Day School. Dr. Katsnelson confirmed there is a fence between the property and the school that would remain.

Trustee Grujanac stated there were many trees on the site and asked if the plan was to remove most of the trees, and are there any heritage trees on the site. Assistant Village Manager/CED Director Gilbertson stated he was not sure if there were heritage trees on the site, but this information would be obtained when reviewing the landscape/tree removal plan for the project.

Trustee Hancock asked if there was concern with the cross access to Oak Tree Corners. Dr. Katsnelson stated they would encourage the cross access to Oak Tree Corners and had plans to discuss this with the shopping center owners. Trustee Hancock asked if they would be willing to remove some parking spaces for access to Oak Tree Corners. Assistant Village Manager/CED Director Gilbertson noted staff would not encourage cross access if it is not feasible to meet the parking requirements on the proposed site.

Mayor Brandt asked what the hours of operation would be for the office. Dr. Katsnelson stated he believed the hours of operation would be normal business hours; 8 a.m. – 5 p.m.

A brief conversation regarding proposed signage followed.

It was the consensus of the Board to refer this item on the Architectural Review Board and Zoning Board.

3.2 Finance and Administration

3.3 Public Works

3.4 Public Safety

3.5 Parks and Recreation

3.6 Judiciary and Personnel

**4.0 UNFINISHED BUSINESS**

**5.0 NEW BUSINESS**

**6.0 EXECUTIVE SESSION**

**7.0 ADJOURNMENT**

Trustee Grujanac moved and Trustee Servi seconded the motion to adjourn. Upon a voice vote, the motion was approved unanimously and Mayor Brandt declared the meeting adjourned at 7:49 p.m.

Respectfully submitted,

**VILLAGE OF LINCOLNSHIRE**

Bradly J. Burke  
Deputy Village Clerk

**REQUEST FOR BOARD ACTION  
Committee of the Whole  
April 8, 2019**

**Subject:** Solar Panel Installation – Extra Space Storage – 200 Parkway Drive – CityPark Center

**Action Requested:** Approval of Roof-Mounted Solar Panel Installation for Building C of Extra Space Storage Facility

**Petitioner:** Rethink Electric LLC

**Originated By/Contact:** Tonya Zozulya, Planning & Development Manager

**Advisory Board Review:** Architectural Review Board

**Background:**

- Rethink Electric LLC, the petitioner representing Extra Space Storage, seeks to install roof-mounted solar panels on Building C of the storage facility at 200 Parkway Drive, as shown on Figure 1 and attached location map. The request is supported by Extra Space Properties Ninety Seven LLC, the property owner.
- The four-acre Extra Space Storage property is part of the 46.5-acre CityPark Center, located at the southwest corner of Milwaukee Avenue and Aptakisic Road. The subject property is located in the southern portion of the Center, west of the Wildfire and Big Bowl restaurants, along the Parkway Drive. The property is accessible from Aptakisic Road and Milwaukee Avenue.
- In 1996, the property received rezoning from R1 Single-Family Residence to B2 General Business and Final Planned Unit Development approvals (Ordinance #96-1453-37).
- The storage facility was constructed in 1998 and has been operated by U.S. Storage, Metro Storage, and recently by Extra Space Storage. The facility is comprised of six one-story buildings.



**Project Overview & Staff Comments:**

- The proposal is to install a 57.1 kilowatt, 168-panel solar system on the south side of the standing seam roof for the 9.5' tall Building C to generate power for the entire storage property (see Document 2). The building roof is silver in color, and the solar panels will be

silver-framed, with black/blue glass panels. The panels would be shielded from Parkway Drive by the existing Extra Space Storage buildings and evergreen trees, as shown in the attached Document 2 photographs.

- Section 6-17-6 of the Zoning Code (see attached Document 3) regulates the placement, approval, and installation of solar panels throughout the Village. The current request meets code requirements regarding the placement. This is the first commercial solar proposal for the Village. Staff expects these requests to grow as solar projects become more accessible and prevalent.

**Architectural Review Board Recommendation:**

On March 19, 2019, the Architectural Review Board (ARB) reviewed the request and unanimously recommended approval (see attached Document 4).

**Approval Process:**

Solar panel proposals for B2-zoned properties are subject to review by the Architectural Review Board and final approval by the Village Board (see attached Document 3). No preliminary evaluation at the Village Board or an amendment to the CityPark Planned Unit Development (PUD) is required since the PUD is silent on accessory uses, thereby deferring to the Village Code.

**Reports and Documents Attached:**

- Document 1: Location Map.
- Document 2: Petitioner's cover letter and presentation packet, prepared by Rethink Electric LLC, with the cover letter dated March 29, 2019.
- Document 3: Solar Energy Systems Code.
- Document 4: Unapproved March 19, 2019 Architectural Review Board meeting minutes.

<b>Meeting History</b>	
Architectural Review Board:	March 19, 2019
Committee of the Whole (current):	April 8, 2019



Map created on March 11, 2019.

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The GIS Consortium and MGP Inc. are not liable for any use, misuse, modification or disclosure of any map provided under applicable law.

Disclaimer: This map is for general information purposes only. Although the information is believed to be generally accurate, errors may exist and the user should independently confirm for accuracy. The map does not constitute a regulatory determination and is not a base for engineering design. A Registered Land Surveyor should be consulted to determine precise location boundaries on the ground.



Village of Lincolnshire  
Building Division  
1 Olde Half Day Rd.  
Lincolnshire, IL 60069

3/29/19

**Attn: Mayor Elizabeth Brandt & Village Trustees**

Hello,

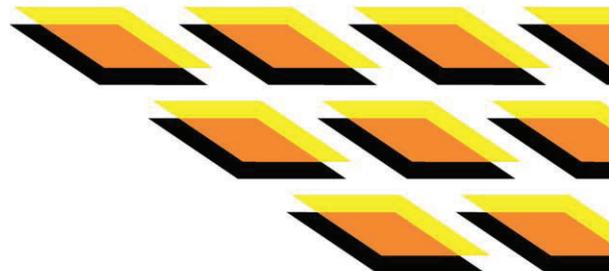
Please accept this as our application for the installation of a 57.12 kW, 168-panel, roof-mounted solar photovoltaic (PV) energy system at the commercial building of 200 Parkway Dr. Lincolnshire, IL 60069.

This system will produce clean energy to provide power to the storage unit facility. It will be located on Building C of the property, parallel to the roof slope, only a few inches above the roof. This will blend with the building, since the system will be flush with the roof and partially hidden by the trees surrounding the property. We received a unanimous recommendation from the ARB on March 19<sup>th</sup>, 2019.

Please let me know if you have any questions, or if any further information is needed for review of this application. Thank you, and we look forward to bringing this solar energy system to your Village.

Respectfully,

Tim Guth  
[tguth@rethinkelectric.com](mailto:tguth@rethinkelectric.com)  
630-747-4587



Client Name & Address

PIVOT ENERGY  
224 N 7TH ST  
ST. LOUIS, MO 63101

Project Name & Address

EXTRA SPACE STORAGE  
STORE #132  
200 PARKWAY DR  
LINCOLNSHIRE, IL 60069



**SCOPE OF WORK**

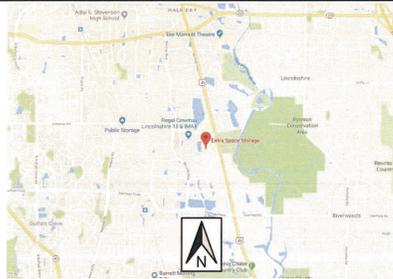
**OVERVIEW:**  
SUMMARY: ROOF-MOUNTED, GRID-TIED PV SYSTEM  
DC SYSTEM SIZE: 57.12 kW  
AC SYSTEM SIZE: 43.2 kW

**EQUIPMENT**  
MODULES: (168) TRINA SOLAR 340W TSM-340DE14A(II)  
INVERTER(S): (1) SOLAREDGE SE43.2KUS

**OPTIMIZERS:** (87) SOLAREDGE P730  
RAIL: ECOLIBRIUM ECOX  
ATTACHMENT: STANDING SEAM CLAMPS

**BUILDING**  
ROOF MATERIAL: STANDING SEAM  
ROOF HEIGHT: 9' 6"  
ROOF PITCH: 5°  
ARRAY AZIMUTH: 180°

**VICINITY MAP**



**AERIAL VIEW**



**GENERAL NOTES**

1. THIS PROJECT HAS BEEN DESIGNED IN COMPLIANCE WITH THE 2017 NEC AND THE VILLAGE OF LINCOLNSHIRE AND ASSOCIATED COUNTY BUILDING AND FIRE CODES.
2. ALL EQUIPMENT SHALL BE LISTED BY A NATIONALLY RECOGNIZED TESTING LABORATORY, AND INSTALLED PER THE MANUFACTURER INSTRUCTIONS AND LISTING REQUIREMENTS.
3. THE INSTALLATION OF EQUIPMENT AND ASSOCIATED WIRING AND INTERCONNECTIONS SHALL BE PERFORMED ONLY BY QUALIFIED PERSONS.
4. SOLAR ARRAY SHALL NOT OBSTRUCT ANY PLUMBING VENTS, SKYLIGHTS, VENTILATION INTAKES OR EXHAUST OUTLETS.
5. ALL OUTDOOR EQUIPMENT SHALL BE NEMA 3R MINIMUM.
6. ALL CONDUCTORS SHALL BE MARKED ON EACH END FOR UNIQUE IDENTIFICATION.
7. ALL CONDUIT EXPOSED TO WEATHER SHALL BE RATED FOR USE IN DIRECT SUNLIGHT.
8. CONDUIT FROM ARRAY TO INVERTER AND FROM INVERTER TO POINT OF INTERCONNECTION SHALL BE ELECTRICAL METAL TUBING (EMT).
9. RAINLIGHT FITTINGS SHALL BE USED FOR ALL OUTDOOR CONDUIT CONNECTIONS.
10. ALL MODULE WIRING SHALL BE UL4703 LISTED PV WIRE WHERE EXPOSED, AND PV WIRE OR THWN-2 WHERE IN CONDUIT.
11. MEANS SHALL BE PROVIDED TO DISCONNECT ALL UNGROUNDED DC CONDUCTORS FROM ALL OTHER CONDUCTORS IN THE BUILDING.
12. MEANS SHALL BE PROVIDED TO DISCONNECT PV SYSTEM WITHIN 5 FT OF LENGTH INSIDE THE BUILDING OR WITHIN 10' FROM THE ARRAY PER THE REQUIREMENTS OF NEC ART. 690.12 WHERE APPLICABLE.
13. PV MODULES, RACKING, INVERTERS, AND ASSOCIATED EQUIPMENT SHALL BE PROPERLY GROUNDED IN ACCORDANCE WITH NEC ART. 250.
14. EQUIPMENT GROUNDING CONDUCTOR SHALL BE MINIMUM 6 AWG WHERE EXPOSED TO PHYSICAL DAMAGE.
15. SWITCH AND CIRCUIT BREAKER REQUIREMENTS OF NEC ART. 690.17, AND ALL OTHER APPLICABLE REQUIREMENTS OF SECTION 690 SHALL BE MET.

**CODE VERSIONS**

APPLICABLE CODES, LAWS AND REGULATIONS  
2009 INTERNATIONAL BUILDING CODE (IBC)  
2009 INTERNATIONAL RESIDENTIAL CODE (IRC)  
2009 INTERNATIONAL FIRE CODE (IFC)  
2017 NATIONAL ELECTRICAL CODE (NEC)

**LEGEND**

- DRAIN
- HVAC UNIT
- VENT
- SKY LIGHT
- VENT FAN

**TABLE OF CONTENTS**

SHEET	DESCRIPTION	Revision	Description	Date
A.1	INITIAL PLAN SET CREATED			1/14/2019
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A-01	PROPERTY PLAN			
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PV-03	ELECTRICAL EQUIPMENT LAYOUTS			
PV-04	ARRAY LAYOUT			
PV-05	ELECTRICAL SCHEMATIC			
PV-06	SAFETY LABELING			
DS-01	INVERTER DATASHEET			
DS-02	OPTIMIZER DATASHEET			
DS-03	PV MODULE DATASHEET			
DS-04	AC DISCONNECT DATASHEET			
DS-05	RACKING DATASHEET			
DS-06	PV METER DATASHEET			

Project:	ESS-8132-LINCOLNSHIRE
Designer:	POLINA KOSEVA
Checker:	GARRISON RIEGEL
Date:	1/14/2019
Scale:	NTS
Sheet No.:	A.1
Sheet Title:	PV-01
Sheet Description:	TITLE SHEET



850 N Central Ave  
Wood Dale, IL 60191  
www.RethinkElectric.com  
(630) 747-4587

Client Name & Address

PIVOT ENERGY  
224 N 7TH ST  
ST. LOUIS, MO 63101

Project Name & Address

EXTRA SPACE STORAGE  
STORE 8132  
200 PARKWAY DR  
LINCOLNSHIRE, IL 60069

Professional Engineer Stamp



Revision	Description	Date
A.1	INITIAL PLAN SET CREATED	1/14/2019
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Project: ESS-8132-LINCOLNSHIRE

Designed by: POLINA KOSEVA

Reviewed by: GARRISON RIEGEL

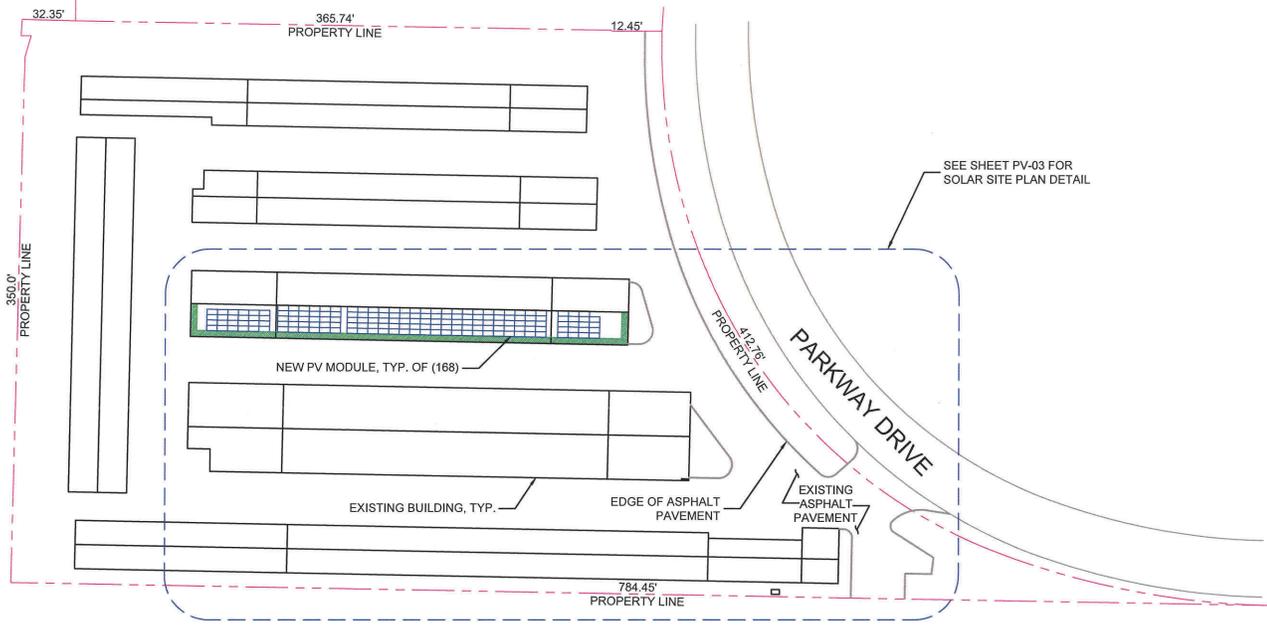
Date: 1/14/2019

Scale: NTS

A.1

A-01

PROPERTY PLAN



1 PROPERTY PLAN  
NOT TO SCALE





Client Name & Address

PIVOT ENERGY  
224 N 7TH ST  
ST. LOUIS, MO 63101

Project Name & Address

EXTRA SPACE STORAGE  
STORE 8132  
200 PARKWAY DR  
LINCOLNSHIRE, IL 60069



Revision	Description	Date
A.1	INITIAL PLAN SET CREATED	1/14/2019
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Project: ESS-8132-LINCOLNSHIRE

Designed by: POLINA KOSEVA

Checked by: GARRISON RIEGEL

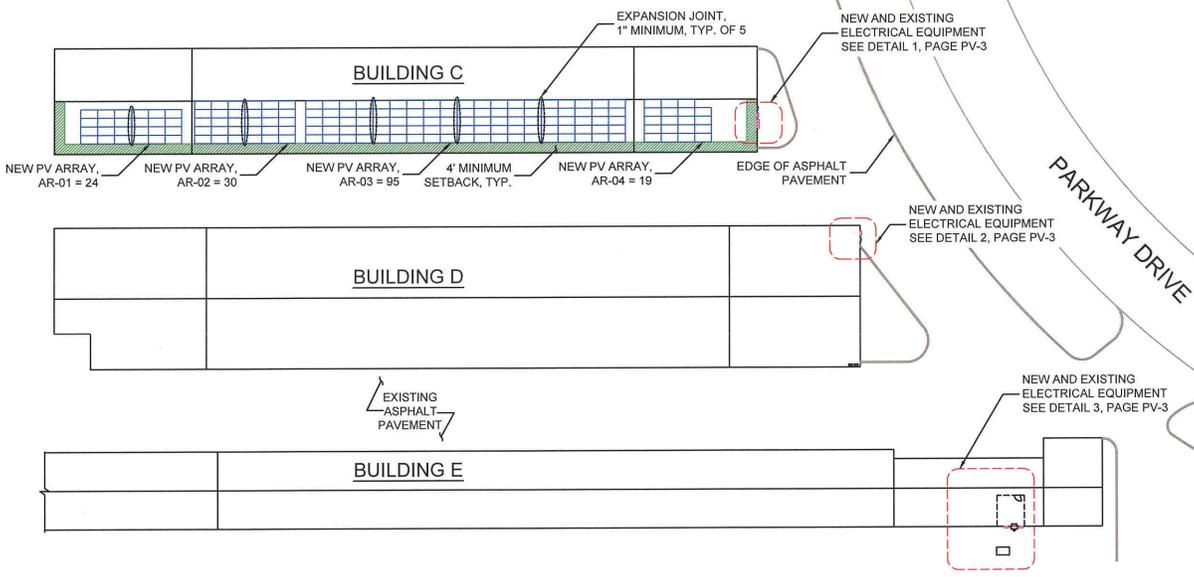
Date: 1/14/2019

Scale: NTS

A.1

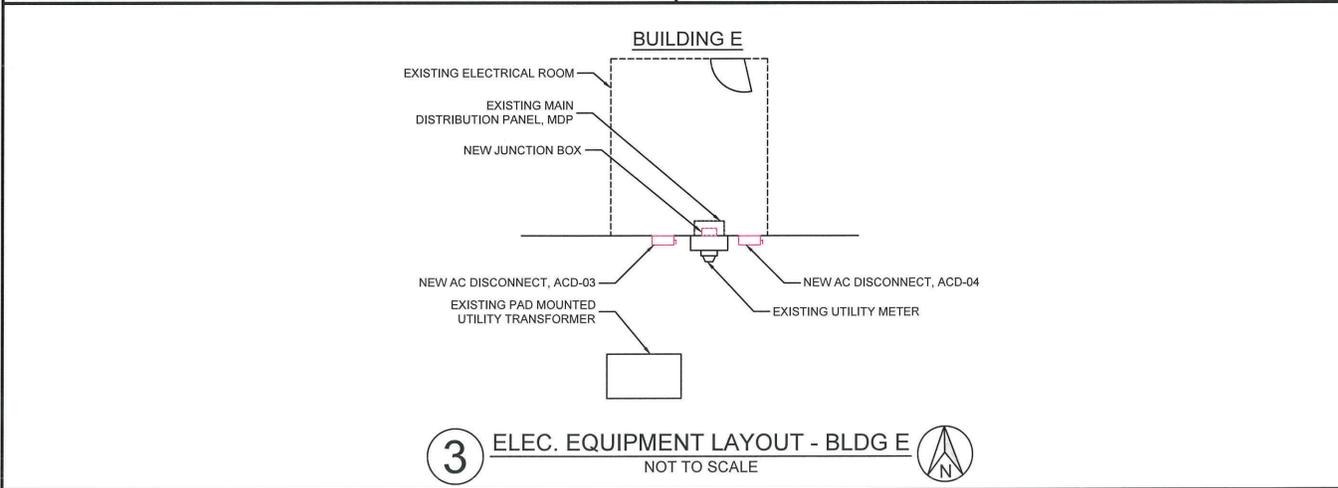
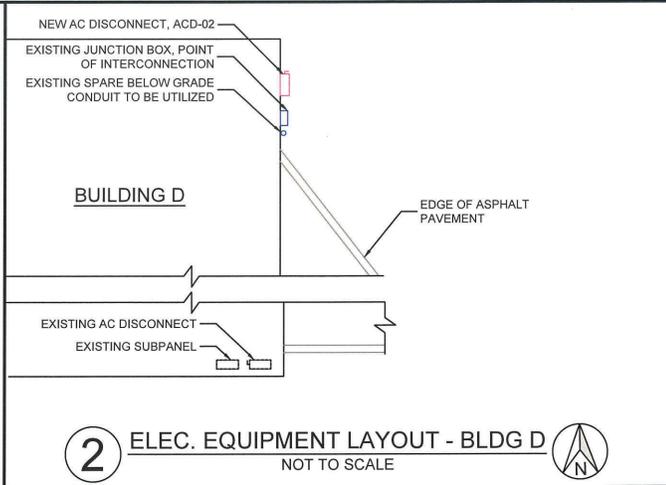
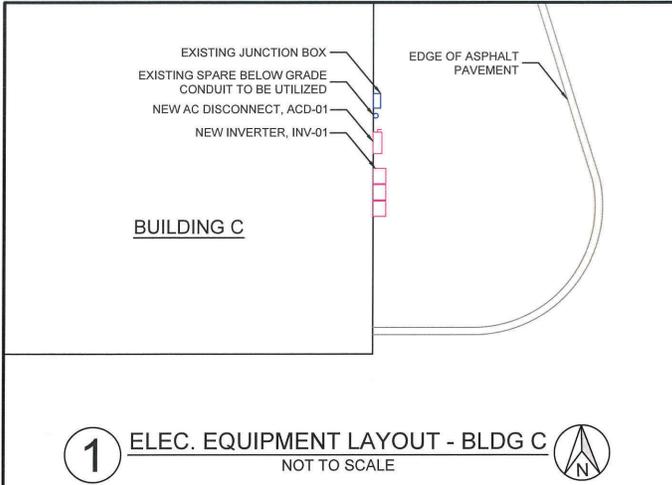
PV-02

SITE PLAN



**1** — SITE PLAN  
NOT TO SCALE





**RETHINK**  
ELECTRIC

850 N Central Ave  
Wood Dale, IL 60191  
www.RethinkElectric.com  
(630) 747-4587

Client Name & Address

PIVOT ENERGY  
224 N 7TH ST  
ST. LOUIS, MO 63101

Project Name & Address

EXTRA SPACE STORAGE  
STORE 8132  
200 PARKWAY DR  
LINCOLNSHIRE, IL 60069

Professional Engineer Stamp

STATE OF ILLINOIS  
PROFESSIONAL ENGINEER  
MATTHEW JOSEPH BOYCE  
062-070115

Review	Description	Date
A.1	INITIAL PLAN SET CREATED	1/14/2019

Project: ESS-8132-LINCOLNSHIRE

Designer: POLINA KOSEVA

Checker: GARRISON RIEGEL

Date: 1/14/2019

Scale: NTS

Sheet Number: **A.1**

Project Number: **PV-03**

Sheet Description: ELECTRICAL EQUIPMENT LAYOUTS

Client Name & Address

PIVOT ENERGY  
224 N 7TH ST  
ST. LOUIS, MO 63101

Project Name & Address

EXTRA SPACE STORAGE  
STORE 8132  
200 PARKWAY DR  
LINCOLNSHIRE, IL 60069

Professional Engineer Stamp



Revision	Description	Date
A.1	INITIAL PLAN SET CREATED	1/14/2019
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Project: ESS-8132-LINCOLNSHIRE

Designed by: POLINA KOSEVA

Checked by: GARRISON RIEGEL

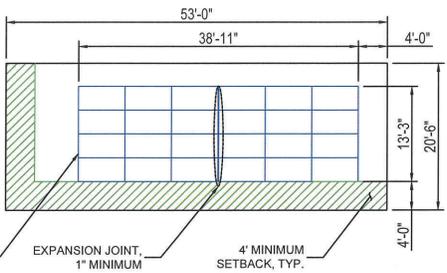
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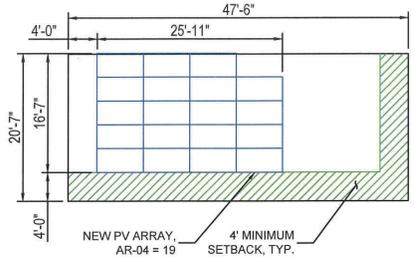
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PV-04

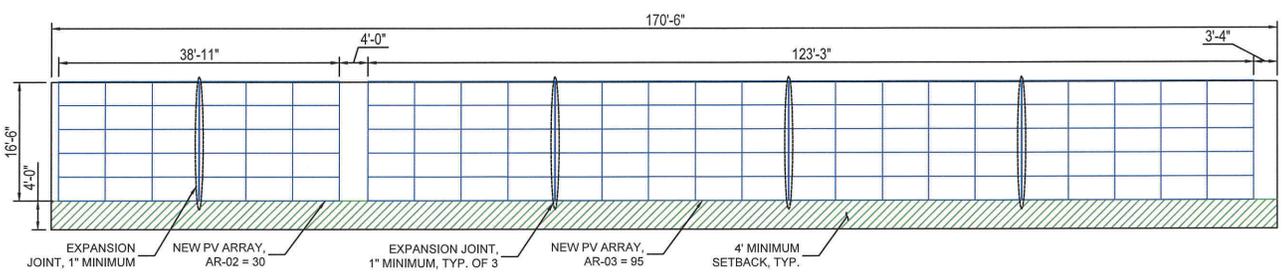
ARRAY LAYOUT



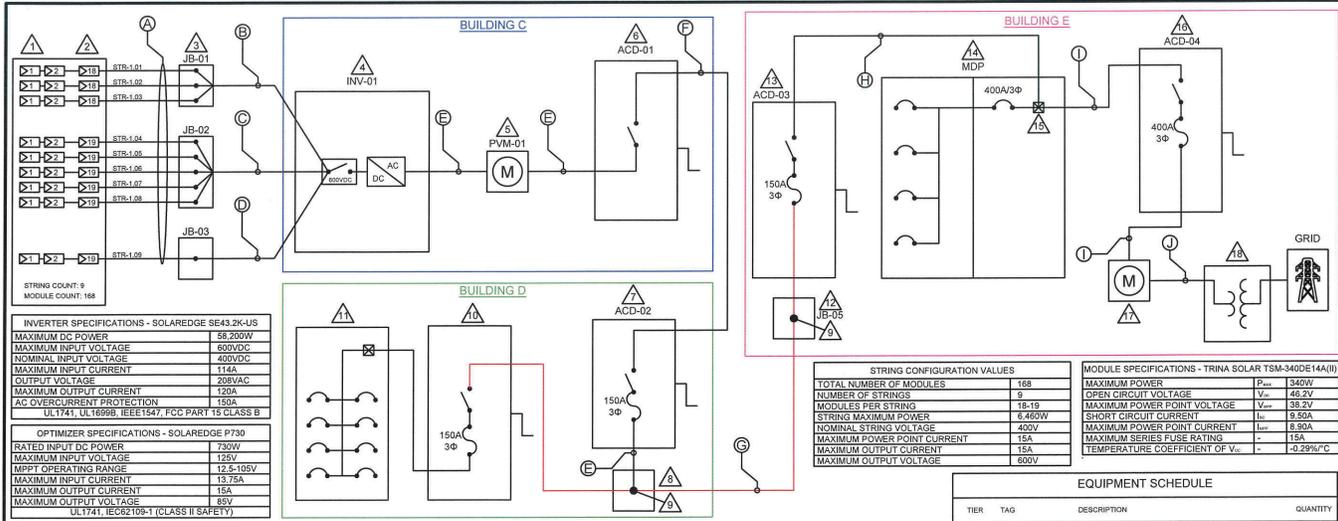
**1** ARRAY LAYOUT- AR-01  
NOT TO SCALE



**3** ARRAY LAYOUT- AR-04  
NOT TO SCALE



**2** ARRAY LAYOUT- AR-02 & AR-03  
NOT TO SCALE



**INVERTER SPECIFICATIONS - SOLAREDDGE SE43.2K-US**

MAXIMUM DC POWER	58,200W
MAXIMUM INPUT VOLTAGE	600VDC
NOMINAL INPUT VOLTAGE	400VDC
MAXIMUM INPUT CURRENT	114A
OUTPUT VOLTAGE	208VAC
MAXIMUM OUTPUT CURRENT	120A
AC OVERCURRENT PROTECTION	150A
UL1741, UL1699B, IEEE1547, FCC PART 15 CLASS B	

**OPTIMIZER SPECIFICATIONS - SOLAREDDGE P730**

RATED INPUT DC POWER	730W
MAXIMUM INPUT VOLTAGE	125V
MPPT OPERATING RANGE	12.5-105V
MAXIMUM INPUT CURRENT	13.75A
MAXIMUM OUTPUT CURRENT	15A
MAXIMUM OUTPUT VOLTAGE	85V
UL1741, IEC62109-1 (CLASS II SAFETY)	

**DESIGN CRITERIA**

HIGH TEMP (2% AVG.)	33°C
EXTREME MIN	-24°C
LOCATIONS/SOURCE	CHICAGO O'HARE INTL AIRPORT / ASHRAE

**CONDUIT SCHEDULE**

TIER	CONDUIT TYPE	CONDUIT SIZE	CONDUIT LENGTH	CONDUCTORS	CONDUIT FILL	MAXIMUM CURRENT	OC/PO WHEN REQUIRED	MAXIMUM VOLTAGE DROP	AMBIENT TEMP. CORRECTION FACTOR	CCC ADJUSTMENT FACTOR
A	FREE AIR	N/A	100'	18 x 10 AWG PV WIRE 1 x 10 AWG EGC	N/A	15.0A	---	1.20%	0.96	N/A
B	EMT	3/4"	100'	6 x 10 AWG THHN-2 1 x 10 AWG EGC	27.78%	15.0A	---	1.20%	0.96	0.80
C	EMT	1"	70'	10 x 10 AWG THHN-2 1 x 10 AWG EGC	26.93%	15.0A	---	0.52%	0.96	0.50
D	EMT	3/4"	45'	2 x 10 AWG THHN-2 1 x 10 AWG EGC	27.78%	15.0A	---	0.34%	0.96	1.00
E	EMT	2"	15'	3 x 3/0 AWG THHN-2 1 x 3/0 AWG THHN-2 (N) 1 x 8 AWG EGC	33.44%	120.0A	150.0A	0.09%	0.96	1.00
F	PVC (EXIST. CONDUIT)	2"	55'	3 x 3/0 AWG THHN-2 1 x 3/0 AWG THHN-2 (N) 1 x 8 AWG EGC	33.44%	120.0A	150.0A	0.34%	0.96	1.00
G	---	---	---	STARTING CONDUCTORS 2 x 3/0 AWG THHN-2 1 x 3/0 AWG THHN-2 (N) 1 x 8 AWG EGC	---	---	---	---	---	---
H	EMT	2"	15'	3 x 3/0 AWG THHN-2 1 x 3/0 AWG THHN-2 (N) 1 x 8 AWG EGC	33.44%	150.0A	150.0A	0.12%	0.96	1.00
I	EMT	3"	15'	8 x 300 KCMIL THHN-2 1 x 300 KCMIL THHN-2 (N) 1 x 3 AWG EGC	33.12%	400.0A	400.0A	0.10%	0.96	1.00
J	---	---	---	EXISTING CONDUCTORS 2 x 300 KCMIL THHN-2 1 x 300 KCMIL THHN-2 (N) 1 x 3 AWG EGC	---	---	---	---	---	---

**NOTES AND CALCULATIONS**

- NEC ART. 690.7(A) MAXIMUM PHOTOVOLTAIC SYSTEM VOLTAGE  
 $V_{max} = 48.2V \times 100\% + (24 - 25^\circ C) \times (-0.29\%/^\circ C)$   
 $= 48.2V \times 100\% + (-0.60V) \times (-0.29\%/^\circ C)$   
 $= 48.2V + 0.174V = 48.374V$
- NEC ART. 690.8(A)(1) PHOTOVOLTAIC SOURCE CIRCUIT CURRENT  
 $I = 15.50A \times 1.25 = 11.88A$
- NEC ART. 690.8(A)(2) PHOTOVOLTAIC OUTPUT CIRCUIT CURRENT  
 $I = 11.88A \times 1.25 = 14.85A$
- NEC ART. 690.8(B) PHOTOVOLTAIC AMPACITY AND OVERCURRENT SERVICE RATING  
 $I = 11.88A \times 1.25 = 14.85A$   
OC/PO WHEN REQUIRED = 15A
- NEC ART. 690.7(C) MAXIMUM DC CONVERTER OUTPUT CIRCUIT CURRENT (EACH)  
 $I = 15A$  MAX PER OPTIMIZER OUTPUT CIRCUIT
- NEC ART. 690.8(A)(3) INVERTER OUTPUT CIRCUIT CURRENT (EACH)  
INV-01 (SE43.2KUS)  
 $I = 120A$   
 $I = 120A \times 1.25 = 150A$   
OC/PO REQUIRED = 150A
- AMBIENT TEMPERATURE CORRECTION FACTORS ARE DETERMINED USING NEC ART. 310.15(B)(2)(a)
- ADJUSTMENT FACTORS FOR MORE THAN THREE CURRENT CARRYING CONDUCTORS ARE DETERMINED USING NEC ART. 310.15(B)(3)(a)

**STRING CONFIGURATION VALUES**

TOTAL NUMBER OF MODULES	168
NUMBER OF STRINGS	9
MODULES PER STRING	18-19
STRING MAXIMUM POWER	6,450W
NOMINAL STRING VOLTAGE	450V
MAXIMUM POWER POINT CURRENT	15A
MAXIMUM OUTPUT CURRENT	15A
MAXIMUM OUTPUT VOLTAGE	600V

**MODULE SPECIFICATIONS - TRINA SOLAR TSM-340DE14A(I)**

MAXIMUM POWER	P <sub>m</sub> = 340W
OPEN CIRCUIT VOLTAGE	V <sub>oc</sub> = 44.2V
MAXIMUM POWER POINT VOLTAGE	V <sub>mp</sub> = 38.2V
SHORT CIRCUIT CURRENT	I <sub>sc</sub> = 9.50A
MAXIMUM POWER POINT CURRENT	I <sub>mp</sub> = 8.50A
MAXIMUM SERIES FUSE RATING	15A
TEMPERATURE COEFFICIENT OF V <sub>oc</sub>	-0.29%/°C

**EQUIPMENT SCHEDULE**

TIER	TAG	DESCRIPTION	QUANTITY
A	MOD-001 ... MOD-168	SOLAR MODULES, TRINA SOLAR, 340W, TSM-340DE14A(I)	168
A	OPT-001 ... OPT-87	POWER OPTIMIZERS, SOLAREDDGE, P730	87
A	JB-01 ... JB-03	JUNCTION BOX, NEMA 3R TRANSITION FROM PV WIRE TO THHN-2 WIRE	3
A	INV-01	INVERTERS, 600VDC, 208VAC, 3Ø4W, NEMA 3R, SOLAREDDGE SE43.2KUS (1)	1
A	PVM-01	PHOTOVOLTAIC PRODUCTION METER, LGATE 325, 208/120V, 3Ø4W, NEMA 3R	1
A	ACD-01	AC DISCONNECT, 240VAC/VDC, 200A, 3Ø4W, NEMA 3R, NON-FUSIBLE	1
A	ACD-02	AC DISCONNECT, 240VAC/VDC, 200A, 3Ø4W, NEMA 3R, FUSED AT 150A	1
A	---	EXISTING JUNCTION BOX, NEMA 3R	1
A	---	TAP EXISTING CONDUCTORS IN JUNCTION BOX	3
A	---	EXISTING AC DISCONNECT, 208/120VAC/VDC, 200A, 3Ø4W, NEMA 3R, FUSED AT 150A	1
A	---	EXISTING SUBPANEL, 208/120V, 200A, 3Ø4W, MLD	1
A	JB-05	JUNCTION BOX, NEMA 1	1
A	ACD-03	AC DISCONNECT, 240VAC/VDC, 150A, 3Ø4W, NEMA 3R, FUSED AT 150A	1
A	MDP	EXISTING MAIN DISTRIBUTION PANEL, 208/120V, 400A, 3Ø4W	1
A	---	FEEDER TAP AT EXISTING SPARE LUGS/TERMINALS IN MDP	1
A	ACD-04	AC DISCONNECT, 240VAC/VDC, 400A, 3Ø4W, NEMA 3R, FUSED AT 400A	1
A	---	EXISTING UTILITY METER, 208/120V, 3Ø4W SERVICE, # 200 288 588	1
A	---	EXISTING COMED TRANSFORMER, PAD MOUNTED	1

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ST. LOUIS, MO 63101

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**Project Name & Address**

EXTRA SPACE STORAGE  
STORE 8132  
200 PARKWAY DR  
LINCOLNSHIRE, IL 60069

---

**Professional Engineer Seal**

STATE OF ILLINOIS  
MATTHEW JOSEPH BOYCE  
062-070115

---

Revision	Description	Date
A.1	INITIAL PLAN SET CREATED	1/14/2019

---

Project: ESS-8132-LINCOLNSHIRE

Designed by: POLINA KOSEVA

Checked by: GARRISON RIEGEL

Date: 1/14/2019

Scale: NTS

Sheet No: **A.1**

Project Title: **PV-05**

Sheet Description: **ELECTRICAL SCHEMATIC**

**WARNING**  
ELECTRIC SHOCK HAZARD  
TERMINALS ON THE LINE AND  
LOAD SIDES MAY BE ENERGIZED  
IN THE OPEN POSITION

TO BE LOCATED ON DC DISCONNECT

**DIRECT CURRENT  
PHOTOVOLTAIC POWER SOURCE**  
MAXIMUM VOLTAGE 600 VDC  
MAX CIRCUIT CURRENT 15 AMP/5

TO BE LOCATED ON DC DISCONNECT

**WARNING  
PHOTOVOLTAIC  
POWER SOURCE**  
2016 NEC 690.51(D)(4) pdlmark.com 05/18/17

TO BE LOCATED ON JUNCTION BOX

**SOLAR DC DISCONNECT**

TO BE LOCATED ON DC DISCONNECT

**WARNING  
DUAL POWER SUPPLY**  
SOURCES: UTILITY GRID AND  
PV SOLAR ELECTRICAL SYSTEM

TO BE LOCATED ON EXTERIOR OF  
MAIN ELECTRICAL PANEL AND METER

**PHOTOVOLTAIC SYSTEM  
AC DISCONNECT**  
RATED AC OUTPUT CURRENT 120 A  
NOMINAL OPERATING AC VOLTAGE 208V

TO BE LOCATED ON AC DISCONNECT, AC  
BREAKER

**RAPID SHUTDOWN  
SWITCH FOR  
SOLAR PV SYSTEM**

EXTERIOR AC DISCONNECT OR  
INVERTER IF OUTSIDE

**WARNING  
POWER SOURCE  
OUTPUT CONNECTION  
DO NOT RELOCATE THIS  
OVERCURRENT DEVICE**

TO BE LOCATED AT BACKFED BREAKER

**WARNING  
TURN OFF PHOTOVOLTAIC  
AC DISCONNECT PRIOR TO  
WORKING INSIDE PANEL**

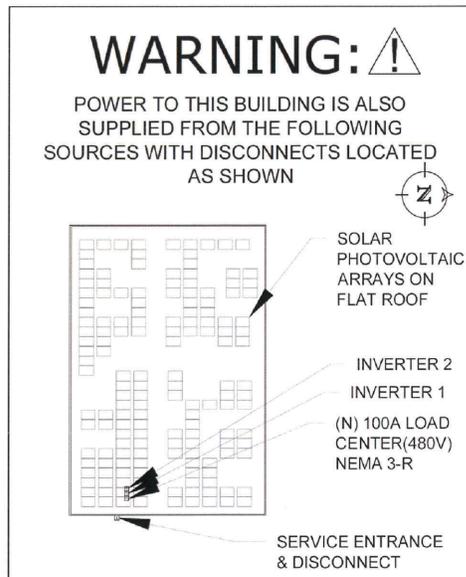
TO BE LOCATED AT MAIN SERVICE PANEL

**WARNING: PHOTOVOLTAIC  
POWER SOURCE**

TO BE APPLIED TO ALL INTERIOR AND/OR  
EXTERIOR CONDUIT AND RACEWAYS  
EVERY 10 FEET, AT TURNS AND ABOVE  
AND OR/BELOW ALL PENETRATIONS

**SOLAR PV SYSTEM  
EQUIPPED WITH  
RAPID SHUTDOWN**  
TURN RAPID SHUTDOWN SWITCH TO  
THE "OFF" POSITION TO SHUT DOWN  
PV SYSTEM AND REDUCE  
SHOCK HAZARD IN THE ARRAY

TO BE LOCATED ON METER, OR 3' FROM  
SERVICE  
DISCONNECTING MEANS METER



EXAMPLE PLACARD

ALL PLACARDS SHALL BE OF WEATHER PROOF CONSTRUCTION  
BACKGROUND ON ALL PLACARDS SHALL BE RED WITH WHITE  
LETTERING U.O.N.

PLACARD SHALL BE MOUNTED DIRECTLY ON THE EXISTING  
UTILITY ELECTRICAL SERVICE  
FASTENERS APPROVED BY THE LOCAL JURISDICTION

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Professional Engineer Stamp



Revision	Description	Date
A.1	INITIAL PLAN SET CREATED	1/14/2019
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---	---	---
---	---	---
---	---	---

Project: ESS-8132-LINCOLNSHIRE

Designer: POLINA KOBEVA

Checker: GARRISON RIEGEL

Date: 1/14/2019

Scale: NTS

A.1

PV-06

SAFETY LABELING



**Three Phase Inverter**  
with Synergy Technology for the 208V Grid  
For North America  
SE43.2KUS

INVERTERS



**Specifically designed to work with power optimizers**

- Easy two-person installation – each unit mounted separately, equipped with cables for simple connection between units
- Balance of System and labor reduction compared to using multiple smaller string inverters
- Independent operation of each unit enables higher uptime and easy serviceability
- No wasted ground area: wall/rail mounted, or horizontally mounted under the modules (10° inclination)
- Integrated arc fault protection and rapid shutdown for NEC 2014 and 2017, per article 690.11 and 690.12
- Built-in module-level monitoring with Ethernet or cellular GSM
- Fixed voltage inverter for superior efficiency (97%) and longer strings
- Integrated DC Safety Switch and optional surge protection & DC fuses (plus & minus)
- Built-in RS485 Surge Protection, to better withstand lightning events

www.solaredge.us

**solar edge Three Phase Inverter with Synergy Technology**  
for the 208V Grid for North America SE43.2KUS

SE43.2KUS		
<b>OUTPUT</b>		
Rated AC Power Output	43200	VA
Maximum AC Power Output	43200	VA
AC Output Line Connections	4-wire WYE (L1-L2-L3-N) plus PE or 3-wire Delta	
AC Output Voltage Minimum-Nominal-Maximum <sup>(1)</sup> (L-N)	105-120-132.5	Vac
AC Output Voltage Minimum-Nominal-Maximum <sup>(1)</sup> (L-L)	183-208-229	Vac
AC Frequency Min-Nom-Max <sup>(1)</sup>	50.3 - 60 - 60.5	Hz
Maximum Continuous Output Current (per Phase) @208V	120	A
GFI Threshold	1	A
Utility Monitoring, Islanding Protection, Configurable Power Factor, Country Configurable Thresholds	Yes	
<b>INPUT</b>		
Maximum DC Power (Module STC), Inverter / Unit	58200 / 19400	W
Transformer-less, Ungrounded	Yes	
Maximum Input Voltage DC to Grid	300	Vdc
Maximum Input Voltage DC to DC	600	Vdc
Nominal Input Voltage DC to Grid	200	Vdc
Nominal Input Voltage DC to DC	400	Vdc
Maximum Input Current	114	Adc
Maximum Input Short Circuit Current	135	Adc
Reverse-Polarity Protection	Yes	
Ground Fault Isolation Detection	350kΩ Sensitivity per Unit	
CEC Weighted Efficiency	97	%
Nighttime Power Consumption	< 12	W
<b>ADDITIONAL FEATURES</b>		
Supported Communication Interfaces	RS485, Ethernet, Cellular GSM (optional)	
Rapid Shutdown	NEC2014 and NEC2017 compliant/certified, upon AC Grid Disconnect	
DC Safety Switch	Built-in	
DC Disconnect	1000V / 3 x 40A	
DC Surge Protection	Optional, Type II, Field replaceable	
DC Fuses on Plus & Minus	Optional, 25A	
<b>STANDARDS COMPLIANCE<sup>(2)</sup></b>		
Safety	UL1741, UL1741 SA, UL1699B, UL1998, CSA 2.22	
Grid Connection Standards	IEEE 1547, Rule 21, Rule 34 (H)	
Emissions	FCC part15 class A	
<b>INSTALLATION SPECIFICATIONS</b>		
Number of units	3	
AC Output Conduit Size / Max AWG / Max PE AWG	2" / #10 / #4	
DC Output Conduit Size / Terminal Block AWG Range / Number of Strings <sup>(3)</sup>	2 x 1.25" / 6-14 / 9 strings	
Dimensions (H x W x D)	Primary Unit: 37 x 12.5 x 10.5 / 940 x 315 x 260; Secondary Unit: 21 x 12.5 x 10.5 / 540 x 315 x 260	in / mm
Weight	Primary Unit: 105.8 / 45; Secondary Unit: 59.2 / 25	lb / kg
Operating Temperature Range	-40 to +45 / -40 to +105 <sup>(4)</sup>	F / °C
Cooling	Fan (user replaceable)	
Noise	< 60	dBA
Protection Rating	IP65	
Bracket Mounted (Brackets Provided)	NEMA 3B	

<sup>(1)</sup> For other regional settings please contact SolarEdge support  
<sup>(2)</sup> See full list of certifications on our website  
<sup>(3)</sup> Larger rack option per unit (up to 30kW) available  
<sup>(4)</sup> For power derating information refer to: <https://www.solaredge.com/Content/Static/Files/Che temperature derating notes.pdf>



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**Project Name & Address**

EXTRA SPACE STORAGE  
STORE 8132  
200 PARKWAY DR  
LINCOLNSHIRE, IL 60069



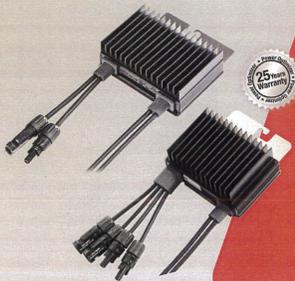
Revision	Description	Date
A.1	INITIAL PLAN SET CREATED	1/14/2019
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Project: ESS-8132-LINCOLNSHIRE  
 Designer: POLINA KOSEVA  
 Designer: GARRISON RIEGEL  
 Date: 1/14/2019  
 Scale: NTS  
 Drawing No: A.1  
 Title: DS-01  
 Description: INVERTER DATASHEET



**SolarEdge Power Optimizer**  
Module Add-On for Commercial Installations  
for North America P600 / P700 / P730 /  
P800p / P800s

POWER OPTIMIZER



PV power optimization at the module-level  
The most cost effective solution for commercial and large field installations

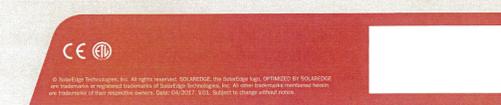
- Specifically designed to work with SolarEdge Inverters
- Up to 25% more energy
- Superior efficiency (99.5%)
- Balance of System cost reduction; 50% less cables, fuses and combiner boxes, over 2x longer string lengths possible
- Fast installation with a single bolt
- Advanced maintenance with module-level monitoring
- Module-level voltage shutdown for installer and firefighter safety
- Use with two PV modules connected in series or in parallel

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**SolarEdge Power Optimizer Module Add-On For**  
Commercial Installations for North America P600 / P700 /  
P730 / P800p / P800s

	P600 (for 2 x 60-cell PV modules)	P700 (for 2 x 72-cell PV modules)	P730 (for 2 x high power 72-cell PV modules)	P800p (for parallel connection of 2x 60-cell PV or 1x 72-cell PV module)	P800s (for series connection of 2x high power or 60-cell modules)	
<b>INPUT</b>						
Rated Input DC Power <sup>(1)</sup>	600	700	730	800	800	W
Absolute Maximum Input Voltage (Use at lowest temperature)	56	125		83	120	VDC
MPPT Operating Range	12.5 - 80	12.5 - 105	11	12.5 - 83	12.5 - 105	VDC
Maximum Short-Circuit Current (Isc)	35.1	33	14	14	22.5	A <sub>DC</sub>
Maximum DC Input Current	12.65	13.75	17.5	15.63	15.63	A <sub>DC</sub>
Maximum Efficiency			99.5			%
Weighted Efficiency			98.6			%
Overcharge Category			II			
<b>OUTPUT DURING OPERATION (POWER OPTIMIZER CONNECTED TO OPERATING SOLAREGE INVERTER)</b>						
Maximum Output Current		15		18		A <sub>DC</sub>
Maximum Output Voltage		85				VDC
<b>OUTPUT DURING STANDBY (POWER OPTIMIZER DISCONNECTED FROM SOLAREGE INVERTER OR SOLAREGE INVERTER OFF)</b>						
Standby Output Voltage per Power Optimizer		5				VDC
<b>STANDARD COMPLIANCE</b>						
EMC	FCC Part 15 Class B, ICES 0006-2, 2, ICES 0006-3					
Safety	IEC62320-2 (Class II safety), UL1741					
Material	UL94 (5-VIAL) UV Resistant					
RoHS	Yes					
<b>INSTALLATION SPECIFICATIONS</b>						
Compatible SolarEdge Inverters	Three phase inverters					
Maximum Allowed System Voltage	1000					
Dimensions (W x H x D)	128 x 152 x 43 / 5 x 5.97 x 1.69	128 x 152 x 50 / 5 x 5.97 x 1.96	128 x 152 x 50 / 5 x 5.97 x 1.93	109 / 2.4	106 / 2.34	mm / in
Weight (including cables)	904 / 2.2	1064 / 2.34	1099 / 2.4	1064 / 2.34		#7 lb
Input Connector	MCA Compatible		Double Insulated, MCA Compatible	(Single or Dual Input) <sup>(2)</sup>	MCA Compatible	
Output Wire Type / Connector	Double Insulated, MCA Compatible					
Output Wire Length	1.8 / 6.9	2.1 / 6.9	1.8 / 6.9	2.1 / 6.9		m / ft
Operating Temperature Range <sup>(3)</sup>	-40 - 85 / -40 - 185					
Protection Rating	IP68 / NEMA6P					
Relative Humidity	0 - 100					
<sup>(1)</sup> Based on maximum of the module. Module size of up to 1000W power is allowed.						
<sup>(2)</sup> For ambient temperatures above 40°C / 104°F a 10% power derating is applied. Refer to Power Optimizer's Temperature Derating Application Note for more details.						
<sup>(3)</sup> For ambient temperatures above 40°C / 104°F a 10% power derating is applied.						
<b>PV SYSTEM DESIGN USING A SOLAREGE INVERTER<sup>(4)</sup></b>						
	THREE PHASE 208V		THREE PHASE 480V			
Compatible Power Optimizers	P600, P700 & P730 <sup>(5)</sup>	P800 <sup>(6)</sup>	P600, P700 & P730	P800		
Minimum String Length	Power Optimizers	35	35	35		
	PV Modules	30	30	30		
Maximum String Length	Power Optimizers	30	30	30		
	PV Modules	60	60	60		
Maximum Power per String		600W	720W	1275W <sup>(7)</sup>	1530W	W
Parallel Strings of Different Lengths or Orientations						
				Yes		



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Project Name & Address

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200 PARKWAY DR  
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Revision	Description	Date
A-1	INITIAL PLAN SET CREATED	1/14/2019
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---	---	---
---	---	---
---	---	---

Project: ESS-8132-LINCOLNSHIRE

Designer: POLINA KOSEVA

Reviewer: GARRISON RIEGEL

Date: 1/14/2019

Scale: NTS

Sheet: A.1

Sheet Title: DS-02

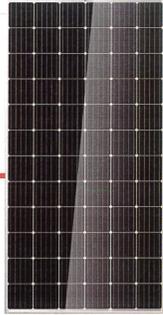
Sheet Content: OPTIMIZER DATASHEET

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Mono Multi Solutions

# THE TALLMAX<sup>M</sup> plus<sup>+</sup>

## FRAMED 72-CELL MODULE(1500W)



**72 CELL**  
MONOCRYSTALLINE MODULE

**340-375W**  
POWER OUTPUT RANGE

**19.3%**  
MAXIMUM EFFICIENCY

**0~+5W**  
POSITIVE POWER TOLERANCE

Founded in 1997, Trina Solar is the world's leading comprehensive solutions provider for solar energy. We believe solar integration with our clients is critical to success. Trina Solar now distributes its PV products to over 100 countries all over the world. Trina is able to provide exceptional service to each customer in each market and supplement our innovative, reliable products with the backing of Trina as a strong, reliable partner. We are committed to building strategic, mutually beneficial collaboration with installers, developers, distributors and other partners.

### Comprehensive Products And System Certificates

IEC61215/IEC61730/UL1709/IEC61701/IEC62716  
ISO 9001: Quality Management System  
ISO 14001: Environmental Management System  
ISO 45001: Greenhouse gases Emissions Verification  
OHSAS 18001: Occupational Health and Safety Management System



**Trinasolar**



#### Ideal for large scale installations

- Reduce BOS cost by connecting more modules in a string
- 1500V UL/1500V IEC certified



#### Maximize limited space with top-end efficiency

- Up to 193 W/m<sup>2</sup> power density
- Low thermal coefficients for greater energy production at high operating temperatures



#### Highly reliable due to stringent quality control

- Over 30 in-house tests (UV, TC, HF, and many more)
- In-house testing goes well beyond certification requirements
- 100% EL double inspection

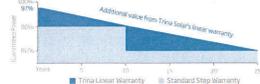


#### Certified to withstand the most challenging environmental conditions

- 2400 Pa wind load
- 5400 Pa snow load

### LINEAR PERFORMANCE WARRANTY

10 Year Product Warranty - 25 Year Linear Power Warranty



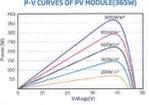
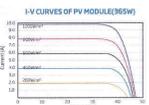
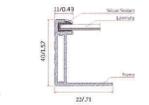
## TALLMAX<sup>plus</sup>

### FRAMED 72-CELL MODULE(1500W)

#### PRODUCTS POWER RANGE

TSM-DE440H STD MONO 340-350W  
TSM-DE440H PERC MONO 355-375W

#### DIMENSIONS OF PV MODULE(mm/inches)



#### ELECTRICAL DATA (STC)

Peak Power Watts P <sub>max</sub> (Wp)	340	345	350	355	360	365	370	375
Power Output Tolerance P <sub>max</sub> (W)	0 ~ +5							
Maximum Power Voltage V <sub>mp</sub> (V)	38.2	38.5	38.7	38.8	39.0	39.3	39.7	40.0
Maximum Power Current I <sub>mp</sub> (A)	8.90	8.96	9.04	9.14	9.24	9.30	9.33	9.37
Open Circuit Voltage V <sub>oc</sub> (V)	46.2	46.7	47.0	47.4	47.7	48.0	48.3	48.5
Short Circuit Current I <sub>sc</sub> (A)	9.50	9.55	9.60	9.65	9.70	9.77	9.83	9.88
Module Efficiency η <sub>p</sub> (%)	17.5	17.7	18.0	18.3	18.5	18.8	19.0	19.3

STC: Irradiance 1000W/m<sup>2</sup>; Cell Temperature 25°C; Air Mass 1.5; Module Temperature 25°C

#### ELECTRICAL DATA (NOCT)

Maximum Power P <sub>max</sub> (Wp)	253	257	261	264	268	272	276	279
Maximum Power Voltage V <sub>mp</sub> (V)	35.4	357	35.9	36.0	36.2	36.4	36.8	37.1
Maximum Power Current I <sub>mp</sub> (A)	7.15	7.20	7.26	7.34	7.42	7.47	7.50	7.53
Open Circuit Voltage V <sub>oc</sub> (V)	42.9	43.4	43.7	44.1	44.3	44.6	44.9	45.1
Short Circuit Current I <sub>sc</sub> (A)	7.67	7.71	7.75	7.79	7.83	7.89	7.94	7.98

NOCT: Irradiance at 800W/m<sup>2</sup>; Ambient Temperature 32°C; Wind Speed 1m/s

#### MECHANICAL DATA

Solar Cells	Monocrystalline 156.75 × 156.75 mm (6 inches)
Cell Orientation	72 cells (5 × 12)
Module Dimensions	1960 × 992 × 40 mm (77.2 × 39.1 × 1.57 inches)
Weight	22.5 kg (49.6 lb)
Glass	3.2 mm (0.13 inches), High Transmission, AR Coated Tempered Glass
Backsheet	White
Frame	Silver Anodized Aluminum Alloy
J-Box	IP 67 or IP 68 rated
Cables	Photovoltaic Technology Cable 4.0mm <sup>2</sup> (0.006 inches <sup>2</sup> ), 1200 mm (47.2 inches)
Connector	Trina T54
Fuse Type	Type 1 or Type 2

#### TEMPERATURE RATINGS

NOCT Thermal Working Cell Temperature	44°C (112°F)	Operational Temperature	-40 ~ +85°C
Temperature Coefficient of P <sub>max</sub>	-0.39%/°C	Maximum System Voltage	1500V DC (IEC)
Temperature Coefficient of V <sub>oc</sub>	-0.29%/°C	Maximum DC (UL)	1500V DC (UL)
Temperature Coefficient of I <sub>sc</sub>	0.05%/°C	Max Series Fuse Rating	15A (Power ≤ 350W) 20A (Power > 350W)

DO NOT connect P<sub>max</sub> in Combiner Box with fuses in one string (parallel connection)

#### WARRANTY

10 year Product Workmanship Warranty	PACKAGING CONFIGURATION
25 year Linear Power Warranty	Modules per box: 27 pieces
(Please refer to product warranty for details)	Modules per 40' container: 648 pieces

CAUTION: READ SAFETY AND INSTALLATION INSTRUCTIONS BEFORE USING THE PRODUCT.  
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Version number: TSM\_EN\_2018 - AUG www.trinasolar.com

**Trinasolar**

**RETHINK**  
ELECTRIC

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Revision	Description	Date
A.1	INITIAL PLAN SET CREATED	1/14/2019
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ESS-8132-LINCOLNSHIRE

Designer: POLINA KOSEVA

Prepared by: GARRISON RIEGEL

Date: 1/14/2019

Sheet: NTS

A.1

DS-03

PV MODULE DATASHEET

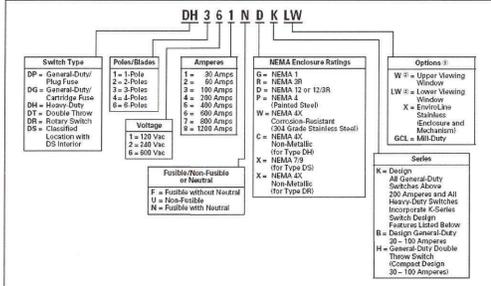
8-18 Switching Devices Safety Switches



February 2007

Product Selection

Table 8-38. Safety Switch Catalog Numbering System



1 See Pages 8-8 through 8-11 for additional Rec Center options.  
 2 Effective August 2003, 30 - 100 ampere window switches are required by a full view window which allows blade position verification and blow team indication. See Page 8-37 for catalog numbers.  
 Note: This table is intended for use in breaking down existing catalog numbers. It is not intended for building new catalog numbers.



February 2007

Switching Devices Safety Switches

8-15

Technical Data and Specifications

Dimensions

Note: Dimensions are for estimating purposes only.

Table 8-31. Heavy-Duty, Non-Fusible, 600 Volt, 3-Pole, Single Throw

Rating	NEMA 1, 3R				NEMA 12, 4X Stainless Steel, 4			
	Width (W)	Height (H)	Depth (D)	Depth (D2)	Width (W)	Height (H)	Depth (D)	Depth (D2)
30	8.13 (206.5)	15.88 (403.4)	10.00 (254.0)	5.25 (133.3)	8.13 (206.5)	12.13 (308.1)	10.00 (254.0)	5.50 (139.7)
60	8.13 (206.5)	18.88 (478.4)	10.00 (254.0)	5.25 (133.3)	8.13 (206.5)	15.13 (384.1)	10.00 (254.0)	5.50 (139.7)
100	11.13 (282.7)	21.69 (550.9)	10.00 (254.0)	5.25 (133.3)	11.13 (282.7)	24.00 (609.6)	10.25 (260.4)	5.50 (139.7)
200	16.00 (406.4)	27.63 (702.8)	11.25 (285.8)	6.14 (156.0)	16.00 (406.4)	34.38 (873.3)	11.50 (292.1)	6.44 (163.6)
400	23.00 (584.2)	46.19 (1172.7)	12.63 (320.9)	7.27 (184.7)	23.00 (584.2)	67.63 (1718.2)	12.63 (320.9)	7.19 (182.6)
600	24.67 (620.6)	52.70 (1340.0)	14.28 (362.8)	8.96 (228.0)	24.67 (620.6)	81.28 (2064.8)	14.28 (362.8)	8.67 (219.8)
800	28.30 (718.7)	68.69 (1745.0)	14.25 (361.8)	8.96 (228.0)	28.30 (718.7)	106.38 (2702.0)	14.25 (361.8)	8.88 (226.0)
1200	41.47 (1053.3)	70.31 (1785.9)	19.84 (504.0)	12.44 (315.0)	41.47 (1053.3)	117.05 (2968.0)	19.84 (504.0)	13.51 (342.9)

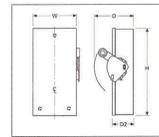


Figure 8-4. NEMA 1-3R Heavy-Duty 30 - 1200 Amperes

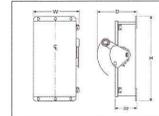


Figure 8-5. NEMA 4, 4X and 12 Heavy-Duty 30 - 1200 Amperes

Table 8-32. Heavy-Duty, Fusible, 240 Volt, 3-Pole Solid Neutral, Single Throw

Rating	NEMA 1, 3R				NEMA 12, 4X Stainless Steel, 4			
	Width (W)	Height (H)	Depth (D)	Depth (D2)	Width (W)	Height (H)	Depth (D)	Depth (D2)
30	8.13 (206.5)	15.88 (403.4)	10.00 (254.0)	5.25 (133.3)	8.13 (206.5)	17.88 (453.2)	10.00 (254.0)	5.50 (139.7)
60	8.13 (206.5)	18.88 (478.4)	10.00 (254.0)	5.25 (133.3)	8.13 (206.5)	20.88 (530.2)	10.00 (254.0)	5.50 (139.7)
100	11.13 (282.7)	21.69 (550.9)	10.00 (254.0)	5.25 (133.3)	11.13 (282.7)	23.88 (604.2)	10.25 (260.4)	5.50 (139.7)
200	16.00 (406.4)	27.63 (702.8)	11.25 (285.8)	6.14 (156.0)	16.00 (406.4)	31.13 (789.2)	11.50 (292.1)	6.44 (163.6)
400	23.00 (584.2)	46.19 (1172.7)	12.63 (320.9)	7.27 (184.7)	23.00 (584.2)	54.38 (1384.2)	12.63 (320.9)	7.19 (182.6)
600	24.67 (620.6)	52.70 (1340.0)	14.28 (362.8)	8.96 (228.0)	24.67 (620.6)	61.63 (1564.2)	14.28 (362.8)	8.67 (219.8)
800	28.30 (718.7)	68.69 (1745.0)	14.25 (361.8)	8.96 (228.0)	28.30 (718.7)	77.13 (1958.2)	14.25 (361.8)	8.88 (226.0)
1200	41.47 (1053.3)	70.31 (1785.9)	19.84 (504.0)	12.44 (315.0)	41.47 (1053.3)	88.63 (2253.2)	19.84 (504.0)	13.51 (342.9)

CAB010010E

For more information visit [www.eaton.com](http://www.eaton.com)



850 N Central Ave  
 Wood Dale, IL 60191  
 www.RethinkElectric.com  
 (630) 747-4587

Client Name & Address

PIVOT ENERGY  
 224 N 7TH ST  
 ST. LOUIS, MO 63101

Project Name & Address

EXTRA SPACE STORAGE  
 STORE 8132  
 200 PARKWAY DR  
 LINCOLNSHIRE, IL 60069



Revision	Description	Date
A.1	INITIAL PLAN SET CREATED	1/14/2019
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---	---	---
---	---	---
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ESS-8132-LINCOLNSHIRE

DESIGNED BY: POLINA KOSEVA

CHECKED BY: GARRISON RIEGEL

Date: 1/14/2019

Scale: NTS

A.1

DS-04

AC DISCONNECT DATASHEET

# EcoX

The new EcoX is an innovative, rail-less racking system, proven to organize the installation process. The flexible design offers a clean aesthetic, simplified logistics, and delivers a higher quality installation at a lower cost per watt.



## Fast.

Modules drop in from above and there is never a need to reach over or walk on modules. Pre-assembled components and quick connections make EcoX easy to install.

## Simple.

Universal components mount to standard framed modules. With a single socket size and a wide range of adjustment, it is quick and easy to install any array with a clean, finished look.

## Supported.

The Ecolibrium field support team offers on-site installation training and ongoing technical support. And from project planning to logistics to installation, we are dedicated to customer service.



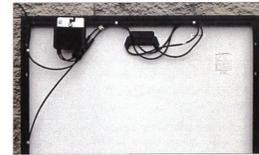
**EcolibriumSolar**

sales@ecolibrumsolar.com | US 740-249-1877 | www.ecolibrumsolar.com



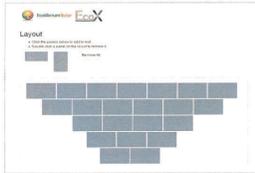
## Aesthetic Design

A wide range of adjustment makes it easy to install a straight, level system. Components are designed to blend into the array, and the aesthetic skirt creates a finished look. Alternatively, a skirt-free option is available to provide a more traditional look.



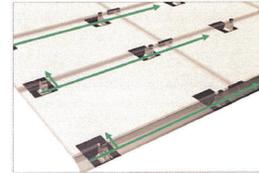
## Cable Management

Whether installing with Microinverters, Power Optimizers, or String Inverters, EcoX provides wire management provisions to both prep the modules, and to route homerun or trunk cables throughout the array.



## Flexible System Design

The EcoX Estimator is a powerful racking system design tool. The user inputs all site conditions and can layout multiple roof surfaces. The EcoX Estimator outputs a site specific design package with engineering specs and bill of materials.



## Single Point Grounding

EcoX and approved modules create a continuously bonded system. The installer can connect a finished array to ground with a single bonding lug.

### Technical Specifications

Materials	Racking components: Aluminum, stainless hardware, dark bronze anodized upper surface, mill finish lower surfaces Flashings: Aluminum, black powder coated finish
Grounding/Bonding Validation	UL2703 - see installation manual for specific module approvals
Fire Resistance Validation	UL2703 - Class A, Type 1 and Type 2 modules
Mechanical Load Validation	UL2703 - see installation manual for specific module approvals
Flashing Validation	ICC-ES AC208/UL441 Rain Test for Roof Flashing
Adjustability	1" vertical range, 3.5" North/South range, connect anywhere in East/West direction
Warranty	15 years

sales@ecolibrumsolar.com | US 740-249-1877 | www.ecolibrumsolar.com



**RETHINK**  
ELECTRIC

850 N Central Ave  
Wood Dale, IL 60191  
www.RethinkElectric.com  
(630) 747-4587

### Client Name & Address

PIVOT ENERGY  
224 N 7TH ST  
ST. LOUIS, MO 63101

### Project Name & Address

EXTRA SPACE STORAGE  
STORE 8132  
200 PARKWAY DR  
LINCOLNSHIRE, IL 60069

### Professional Engineer Stamp



Revision	Description	Date
A.1	INITIAL PLAN SET CREATED	1/14/2019
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---	---	---
---	---	---
---	---	---
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ESS-8132-LINCOLNSHIRE

Designed by: POLINA KOSEVA

Reviewed by: GARRISON RIEGEL

Date: 1/14/2019

Scale: NTS

A.1

DS-05

RACKING DATASHEET

# COMMERCIAL SOLAR MONITORING SOLUTION

## LGate 320

The LGate 320 is a three-phase electronic watt-hour meter for remote monitoring of solar photovoltaic systems. It features a smart communications module to automatically transmit meter data over cellular or Ethernet networks allowing system owners and operators to easily manage distributed solar assets.

The LGate 320 combines a revenue-grade, solid-state power meter with an advanced communications gateway. These components work in conjunction to remotely monitor the performance of commercial solar energy installation regardless of panel or inverter type. The LGate 320 is a one-piece completely under glass meter which installs easily using a standard socket base. Performance data is uploaded in near real-time to the Locus Energy SolarNOC monitoring platforms which provides a suite of tools and analytics for asset managers.

#### DATA COLLECTION

AC energy data is collected by the meter and passed to the communications module. Additional system performance data can be collected directly from meteorological sensors and supported inverters via RS-485 connections. All data is stored in non-volatile memory and then automatically uploaded to the SolarNOC platforms.

#### NETWORK CONNECTIVITY

The communications gateway inside the LGate 320 supports plug and play connectivity through a cellular network connection. Once the unit is installed and powered on, it will immediately begin transmitting data without any configuration. If cellular reception is weak or inconsistent, then communication via Ethernet cable may be used as an alternative option.

#### FEATURES:

- ANSI C12.20 power meter
- RS-485 inputs
- GSM cellular or Ethernet connectivity
- Over the air firmware updates
- Easy, low-cost installation
- Doesn't require entrance into the building
- Plug-and-play activation
- LCD display

Learn more about the LGate 320 at: [www.locusenergy.com](http://www.locusenergy.com)

LGATE 320 | WWW.LOCUSENERGY.COM

#### DIAGRAM – TYPICAL CONFIGURATION

#### DIMENSIONS

#### SPECIFICATIONS

DATALOGGER		POWER	
Processor	ARM9 embedded CPU	Accuracy	ANSI 12.20 (Class 0.2%)
OS	Custom version of Linux 2.6, OTA firmware updates	Voltage Inputs	120 – 480 VAC
Memory	128 MB SDRAM	Max. Current Input	320 A
Display	LCD screen	Service Type	Three phase, Wye or 4-wire Delta
<b>I/O</b>		Socket Type	165
RS485	2 wire and 4 wire	<b>COMPLIANCE</b>	
Modbus		ANSI 12.20 Class 0.2%	
Zigbee		FCC Part 15B	
<b>COMMUNICATIONS</b>		PFCIS	
LAN	RJ45 10/100 Ethernet, full half duplex, auto polarity	AT&T Carrier Compliance	
Cellular	3G GSM	<b>PHYSICAL</b>	
Networking	DHCP or static	Enclosure	NEMA 3R Type
		Weight	2 lbs 2 oz
		Dimensions	6.95" x 6.5" x 7.3"
		Environment	-20 to 50C, all-weather
		Warranty	5 year limited warranty

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**RETHINK ELECTRIC**

850 N Central Ave  
Wood Dale, IL 60191  
www.RethinkElectric.com  
(630) 747-4587

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Client Name & Address

PIVOT ENERGY  
224 N 7TH ST  
ST. LOUIS, MO 63101

---

Project Name & Address

EXTRA SPACE STORAGE  
STORE 8132  
200 PARKWAY DR  
LINCOLNSHIRE, IL 60069

---

Professional Engineer Seal  
MATTHEW JOSEPH BOYCE  
062-070115  
STATE OF ILLINOIS

Revision	Description	Date
A.1	INITIAL PLAN SET CREATED	1/14/2019

Project: ESS-8132-LINCOLNSHIRE

Designed by: POLINA KOSEVA

Checked by: GARRISON RIEGEL

Date: 1/14/2019

Scale: NTS

Sheet: A.1

Sheet Description: DS-06

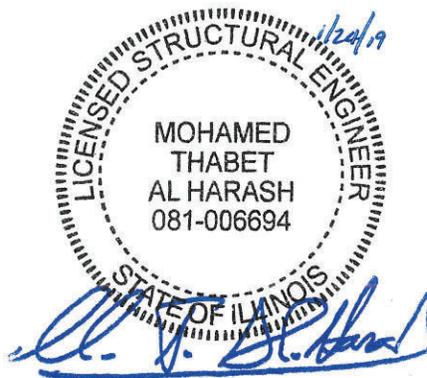
Sheet Title: PV METER DATASHEET



**ENGINEERED SOLUTIONS, LLC**  
DESIGN - ENGINEERING - CONSULTING

**ESS – Lincolnshire**  
200 Parkway Drive  
Lincolnshire, IL 60069

Solar Array  
Structural Calculations



Prepared for:

Mr. Garrison Riegel  
Rethink Electric  
850 N. Central Avenue  
Wood Dale, IL 60191

Project # 19.131  
© Engineered Solutions, LLC.  
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January 2019

Engineered Solutions, LLC  
3368 Carriage Crossing  
Saint Charles, MO 63301



Mr. Garrison Riegel  
Rethink Electric  
850 N. Central Avenue  
Wood Dale, IL 60191

January 24, 2019

**RE: 8132 ESS Lincolnshire – 200 Parkway Drive, Lincolnshire, IL 60069**  
**Project #: 19.131**

Mr. Riegel:

Pursuant to your request, we have reviewed your drawings and the building for the installation of the solar array at the above-referenced address.

Based on our review, we have determined that the existing roof structure is capable of supporting the additional loading of the solar array. The condition and capability of the structure is sufficient for all loading conditions in accordance with the requirements of ASCE 7-05 and IBC 2009.

In addition, the total weight of the proposed arrays in the proposed locations does not cause a greater than 10% increase in lateral seismic forces experienced by any of the lateral load resisting elements that this weight is tributary to, in accordance with the 2009 International Building Code.

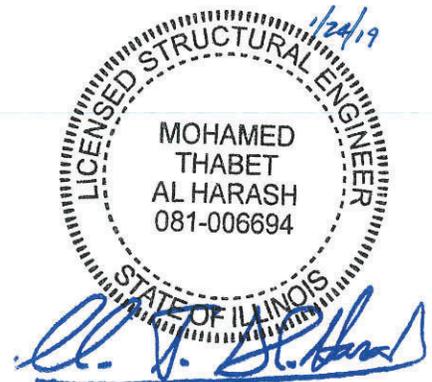
Please feel free to contact me if you have any comments or questions.

Respectfully yours,

*Mohamed T. AL HARASH*

Dr. Mohamed T. AL HARASH, Sc.D., P.E., S.E. - NCEES  
Director of Operations

cc: Matthew Boyce, PE  
Principal





Calculation Sheet

Project #: 19.131  
 Project Name: ESS - Lincolnshire

Date: 1/24/19  
 Calc Type: Gravity

Building Information

Building Length	272 ft
Building Width	30 ft
Building Height	12 ft
Array L	226 ft
Array W	16 ft
Array Unit Weight	2.8 psf
Number of Panels	168 ea

Code Information

S <sub>1</sub>	0.18
S <sub>s</sub>	0.54
Occupancy	II
Site Class	D
ASCE 7-05	

Building Construction Type: Metal Frame

Unit Weight: 40 psf

Increased Wind Loads Due to the addition of Solar Panels (AREA ABOVE PARAPET)

Building Wall Area	3264
Solar Area (surface)	226
Area Increase	< 10% n/a - pitched roof

Gravity loads are checked for the individual load carrying member - critical case. (see attached)



## Calculation Sheet

Project #: 19.131  
 Project Name ESS - Lincolnshire

Date: 1/24/19  
 Calc Type: Seismic

### Seismic Weight

Roof DL	8 psf		
Roof Weight	65.3 k		
Wall Weight	289.9 k		
Total Weight	355.2 k		
Solar Weight	10.1 k		
W = Solar/Total	2.85%	<	10% OK

### Horizontal Seismic Force

$$F_p = \frac{(0.4 \times a_p \times S_{ds} \times W_p) \times (1 + 2 \times z/h)}{(R_p/I_p)} \quad (13.3-1)$$

$$S_{ds} = 2/3 S_{ms} \quad (11.4-3)$$

$$S_{ms} = F_a \times S_s \quad (11.4-1)$$

$$F_a = 1.37 \quad (\text{from Table 11.4-1})$$

$$S_{ms} = 0.7398$$

$$S_{ds} = 0.4932$$

$$R_p = 1.5 \quad (\text{Table 13.5-1})$$

$$I_p = 1 \quad (13.1.3)$$

$$a_p = 1 \quad (\text{Table 13.5-1})$$

$$z/h = 1$$

Difference in height between roof & solar array

$$W_p = 10125 \quad \text{lbs}$$

$$F_p = 3995 \quad \text{lbs}$$

$$F_p = a \times W_p$$

$$a = 39\%$$

$$\text{For one panel, } F_p = 24 \quad \text{lbs}$$

$$\# \text{ of supports} = 4 \quad \text{ea}$$

$$F_p \text{ (per support)} = 6 \quad \text{lbs}$$

The lateral force resisting elements (ext. shear walls) experience an increase of 3995 lbs total ultimate load for the entire array.

These elements experience an increase of 6 lbs locally and are more than adequate.

Project: ESS 8132 LINCOLNSHIRE  
Sheet: 1/1  
Date: 1/24/19 By: MSB



ENGINEERED SOLUTIONS, LLC  
DESIGN - ENGINEERING - CONSULTING

CHECK EXISTING ROOF PURLINS

$$D = 8 \text{ psf}$$

$$\text{SNOW} = 2.5 \text{ psf} \quad \text{ROOF SNOW} = 17.5$$

$$\text{SOLAR} = 3.0 \text{ psf}$$

PURLIN INFO:

$$4 \times 2\frac{1}{2} \times 16 \text{ GA}$$

$$I_x = 1.377$$

$$10' \text{ SPAN}$$

$$S_x = 0.688$$

$$5' \text{ SPACING}$$

FOR Z SECTION

$$\begin{aligned} \text{MALLOW} &= 0.6 \times S_x \times f_y \\ &= 0.6 \times 0.688 \times 55,000/12 \\ &= 1892 \text{ FT-LB} \end{aligned}$$

MAX (+) MOMENT @ MIDSPAN

$$\begin{aligned} M &= wL^2/24 = 28.5 \times 5 \times 10^2/24 \\ &= 594 \text{ FT-LB} \quad \text{OK} \end{aligned}$$

MAX (-) MOMENT @ SUPPORT

$$M = wL^2/12 = 1188 \text{ FT-LB} \quad \text{OK}$$

∴ EXISTING ROOF IS ADEQUATE  
TO SUPPORT PV PANEL ARRAY

February 15, 2019



Village of Lincolnshire  
One Olde Half Day Rd  
Lincolnshire, IL 60069

Subject: Letter of Consent for PV System Installation

To Whom It May Concern,

Let it be known that Extra Space Storage consents to having a photovoltaic solar array installed on the roof top of our facility located at 200 Parkway Dr. Lincolnshire, IL 60069. This letter authorizes Rethink Electric, Inc. to permit the photovoltaic system with the Village of Lincolnshire.

Sincerely,

DocuSigned by:  
*Byron Harris*  
6B68CB220A5E4A0...

Byron Harris

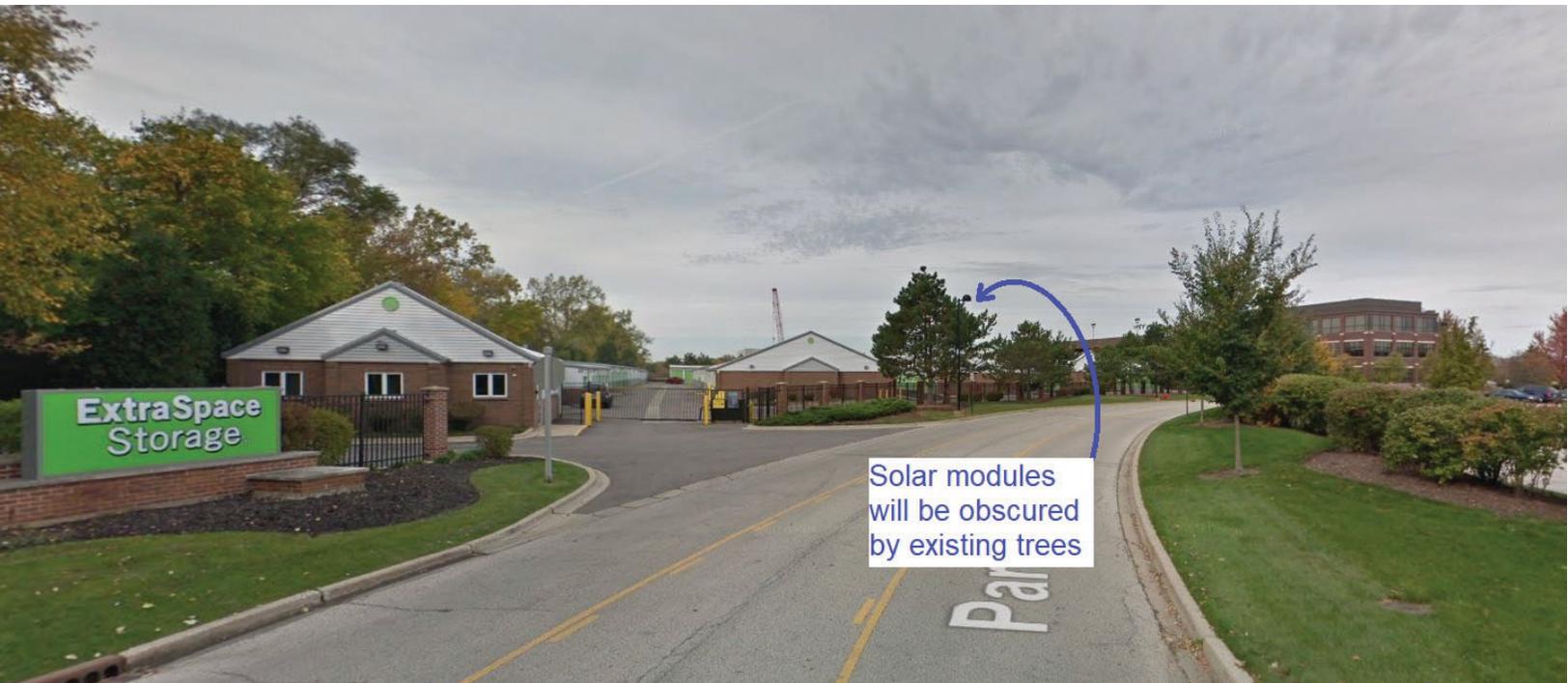
Solar Project Manager

Extra Space Storage









Solar modules  
will be obscured  
by existing trees



Solar modules  
will be obscured  
by existing trees



Solar modules will be located on this building behind the existing trees



Solar modules  
will be obscured  
by existing trees



Solar modules  
will be obscured  
by existing trees



Solar modules will be obscured by existing building facade



Solar modules will be obscured by existing building facade

- b. A wildlife study shall be conducted by a qualified professional not in the employ of the Applicant, such as an ornithologist or wildlife biologist, to determine if there is any potential impact the SWES may present to migratory birds and wildlife species. In cases where the wildlife study indicates that a protected natural resource will be adversely affected by an SWES, the Village shall consult with the Illinois Department of Natural Resources (IDNR), in accordance with Title 17 of the Illinois Administrative Code Part 1075, to determine whether the protective measures outlined in the study are deemed acceptable. A final decision on the application shall not be made until such consultation with IDNR is resolved.
- G. **COMPLIANCE:** Every SWES must maintain compliance with the plans and specifications approved by the Village Board of Trustees. If a SWES becomes non-compliant with approved plans and specifications due to, but not limited to: discoloration, cracking, missing components, rusting, settling, damage or general disrepair; then the owner/operator of the SWES and the owner of the building or lot on which the SWES is located will be jointly and severally responsible for remedying the specific non-conformities. These non-conformities must be remedied within forty-five (45) days after receipt of written notice sent by the Village to the owner/operator of the SWES and the owner of the building or lot. Failure to remedy all of the cited non-conformities, within the forty-five (45) day time period, shall be punishable by a fine not exceeding the amount described per day that the violation continues, pursuant to Chapter 4 of Title 1 of the Village Code.
- H. **ENGINEERING CERTIFICATION:** No SWES may be located within the Village unless the Applicant has provided to the Village the written certification of a professional engineer licensed by the State of Illinois that the structure upon which the facility is located is sufficient from a structure engineering standpoint to bear the load. In instances of free-standing structures, this shall include a certificate that the foundation on which the structure is built, is constructed and engineered to take into account the existing soil conditions. A licensed professional engineer shall also certify that in the event of a fall or collapse, that the facility is designed and manufactured to fall entirely within the boundary lines of the lot on which it is located, and that installation meets or exceeds the maximum construction and installation standards set forth by the manufacturer.

## **6-17-6: SOLAR ENERGY SYSTEMS (SES)**

- A. GENERAL REGULATIONS: A Solar Energy System (SES), as referenced in Section 6-17-2, may be erected or installed only in accordance with this Title 6, Chapter 17 of the Village Code. Any SES shall conform to all Federal laws and regulations concerning its use and operation, and may be installed only in the following zoning districts and standards:
1. Shall be permitted in the R1, R2, R2A, and R3 Zoning Districts. Any attached single-family residential development and mixed-use development which contains residential housing units shall require review by the Architectural Review Board prior to being approved or denied by the Village Board.
  2. Shall be permitted in the R4, R5, R6, B1, B2, E, and O/I Zoning Districts, subject to review by the Architectural Review Board prior to being approved or denied by the Village Board.
- B. PERMITS: No such Solar Energy System (SES), as referenced in Section 6-17-2, shall be erected, constructed, altered or relocated without first obtaining a building permit from the Department of Community Development. An application for a building permit shall be made upon forms provided by the Department of Community Development, signed by the Applicant, and contain or have attached thereto the following information:
1. Name of person, firm, corporation or association constructing and erecting the solar energy system.
  2. Site plan showing the location of the solar energy system upon the lot and copies of the manufacturer's specification for the solar energy system.
  3. Name, address, and telephone number of the applicant, and the name of a responsible party in the case of corporate applications.
  4. Written consent of the owner of the building structure or land on which the solar energy system is to be erected.
  5. Elevation(s) of the existing structural improvements and the proposed solar energy system showing the size and design details.
  6. Four (4) sets of plans and specifications showing the method of construction, location, support, and attachment to the structure.
  7. If required by the Department of Community Development, a copy of stress sheets and calculations prepared by a licensed professional

engineer showing that the solar energy system is designed for the deadload, in the amount required by the manufacturer and all other laws and ordinances of the Village.

8. A line drawing of the electrical components, as supplied by the manufacturer, in sufficient detail to allow for a determination that the manner of installation conforms to the Village Code.
9. Such other information that the Department of Community Development shall require to show full compliance with this and all other ordinances of the Village.

C. PLACEMENT OF SES:

1. Shall be limited to roof-mounted installations on a permitted structure, provided that the installation method shall be compatible and harmonious with the aesthetic qualities of the structure to which the device is attached so as to not abruptly alter the architectural character of the structure.
  - a. Shall be attached directly to the exterior of the roof structure to ensure the lowest profile permissible. All components of the SES shall not extend above the maximum building height permitted by the zoning district and beyond the existing limits of the roof.
  - c. No component of the SES, including mounting racks, shall be permitted to tilt or rotate at a slope greater or less than the roof to which the device is attached.
  - d. Shall be designed and installed to prohibit Sun Reflection towards vehicular traffic and any habitable portion of an adjacent structure. Sun Reflection onto an adjacent roof shall be acceptable.
  - e. Shall occupy not more than fifty-percent (50%) of the outside roof area to which the device is attached. If an SES is installed on multiple roofs on a single structure, the SES shall occupy not more than thirty-percent (30%) of each outside roof area to which the device is attached.
  - f. With the exception of Solar Panels, mounting racks, pipe runs, and electrical wire connections, no portion of an SES shall be installed on the outside of the roof.

- g. No trees or vegetation shall be removed or pruned to reduce or eliminate shading from the sun, unless warranted for good forestry practices, as determined by the Village Forester.

**6-17-7: DECOMMISSIONING AND RESTORATION PLAN:**

- A. **MICRO WIND ENERGY SYSTEMS (MWES) AND SOLAR ENERGY SYSTEMS (SES):** When a MWES or SES is not operated for a continuous period of at least nine (9) months, such Alternative Energy Collection System and all related equipment shall be deemed abandoned by the Village. The owner of such Alternative Energy Collection System shall remove all items within forty-five (45) days following receipt of written notification that removal is required. Such notice shall be sent by registered or certified mail, return receipt requested, by the Village to such owner at the last known address of such owner. A principal structure or lot for sale, lease, or in foreclosure may be exempt, provided that the MWES and/or SES are maintained pursuant to this Title 6, Chapter 17 of the Village Code.
- B. **SMALL WIND ENERGY SYSTEM (SWES)**
  - 1. Prior to receiving a Special Use Permit for the installation of a SWES, the owner and/or operator must include a Decommissioning and Restoration Plan with the application request to ensure such Alternative Energy Collection System and all related equipment is properly decommissioned. The owner of the SWES and the underlying property owner(s) shall be jointly liable for the removal of all equipment associated with the SWES at the end of the Special Use permit period, if any, the useful life of the facility, or when the facility is abandoned or otherwise out of operation for continuous period of at least nine (9) months. The Decommissioning and Restoration Plan shall state how the facility will be decommissioned and how the site will be restored, and shall further provide:
    - a. Provisions for removal of the SWES and all related equipment, including those below the soil surface.
    - b. Provisions for the restoration of the property and improvements upon completion of the decommissioning of the Alternative Energy Collection System and all related equipment.



Spa at Lincolnshire and Crane's Landing Golf Course. The sign size will remain the same. Landscaping will be restored to the originally-approved landscape plan.

One interior directional signs would be updated to include the new Marriott logo. The background will be white and will remain illuminated.

**Interim Chair Orzeske** inquired about the paint finish and durability. **Art Solis, North Shore Signs**, stated the two ground and one directional sign will be repainted white, with life expectancy of the paint to be about 10 years.

**PDM Zozulya** stated the petitioner is before the ARB as this is an amendment to the Master Sign Plan approved in 2011. The Master Sign Plans included variations for sign dimensions, items of information, and an electronic display sign on Milwaukee. While a public hearing is not required, the ARB is required to make a recommendation to the Village Board for final approval.

There was discussion regarding the five remaining directional signs in the complex. **Tonya Zozulya** asked the petitioner if they would like to include those signs in the ARB recommendation and, if so, provide renderings of the color change to be included with the Village Board packet for consideration and final approval. **Brad Lajoie** agreed to include the directional signs for consideration.

**Member Baskin** moved, seconded by **Member McCall**, to recommend approval to the Village Board of the amendment to the Marriott Master Sign Plan amendment to permit revisions to the existing ground monument and directional signs for the Marriott Lincolnshire Resort, located at 10 Marriott Drive, as presented in the petitioner's cover letter and presentation packet, date stamped received March 11, 2019, with approval and inclusion of additional directional signs, consistent in coloring with the proofs submitted, with review by Village Staff and approval by the Village Board.

The motion passed unanimously by voice vote.

### 3.2 Consideration of Roof-Mounted Solar Panel Installation for Building C of Extra Space Storage Facility, 200 Parkway Drive (Rethink Electric LLC)

**Garrison Regal, Vice President of Operations, Rethink Electric LLC** reviewed the proposal to install roof-mounted solar panels on Building C of the storage facility at 200 Parkway Drive, CityPark Center. He presented aerial and ground elevation views of the complex, adding the solar modules will be obscured from view by trees and other buildings in the complex. The solar panels will be a flush-mount, low profile, and installed with non-penetrating methods. The size of each solar panel on this installation will be approximately 6.5' by 3.5'.



Members of the ARB noted this proposal was the first commercial solar project in the Village. **PDM Zozulya** stated Village code requires the ARB to conduct design review with a recommendation to the Village Board. In response to a question on space limitations on the roof, **Tonya Zozulya** stated the 50% requirement takes into consideration the calculation both sides of the roof.

**Member Santosuosso** moved, seconded by **Member McCall**, to approve and recommend approval to the Village Board for the installation of the proposed solar panels on Building C for Extra Space Storage, as presented in the packet submitted by Rethink Electric, LLC, date stamped received March 11, 2019.

Motion passed unanimously by voice vote.

3.3 ~~Approval of Parking Lot Design, Fence Landscaping, and Lighting Plans for Ravinia Plumbing, 575 Bond Street (Ravinia Plumbing)~~

~~**David Ariano, President of Ravinia Plumbing** gave a brief history of Ravinia Plumbing's 98 years in business and current location in Highland Park. They are the contract purchaser for said property and are looking to expand their business and fleet in Lincolnshire.~~

~~**Meghan Michel, P.E. and Project Manager with J. Condon & Associates**, presented the plans for the new parking lot as follows:~~

~~A new parking surface with 85 stalls to be utilized by the service technicians and fleet vehicles;~~

~~A 6' open-style picket fence and landscaping surrounding the new parking surface, and utilizing the existing berm to further enhance the proposed fence and landscape plan.~~

~~The existing 17-stall parking lot would be utilized by office staff; and Utilizing the existing retention basin for increased storm water volume. There would be required changes in the outlet restrictor structures only to accommodate the allowable release rate, and no alteration to the pond is anticipated. The petitioner is working with Lake County Storm Water Management Commission on these plans to get their approval.~~

~~**Member Baskin** asked how many trees would be removed. He also suggested the design of the vinyl fence should complement the building and asked staff if there are similar fences in the industrial park. **PDM Zozulya** stated fences typically are not part of the industrial area unless it is a day care facility. **David Ariano** said the fence and landscape is more for security purposes, and the fence will be installed at grade behind the existing berm. **Member Baskin** commented a cross-section view of the fence/parking lot would be helpful. In regard to tree removal, **Meghan Michel** stated 47 trees are being removed. **PDM Zozulya** added the petitioner's landscape architect calculated 143 caliper inches of trees being removed, to be replaced with 98.5 caliper inches; the negative net difference will be added to the Tree Bank per Village Code.~~

**REQUEST FOR BOARD ACTION  
Committee of the Whole  
April 8, 2019**

**Subject:** Lincolnshire Marriott Resort Ground and Directional Sign Revisions  
– 10 Marriott Drive

**Action Requested:** Consideration of an Ordinance Amending Ordinance #11-3191-13A Related to Marriott’s Lincolnshire Resort Master Sign Plans

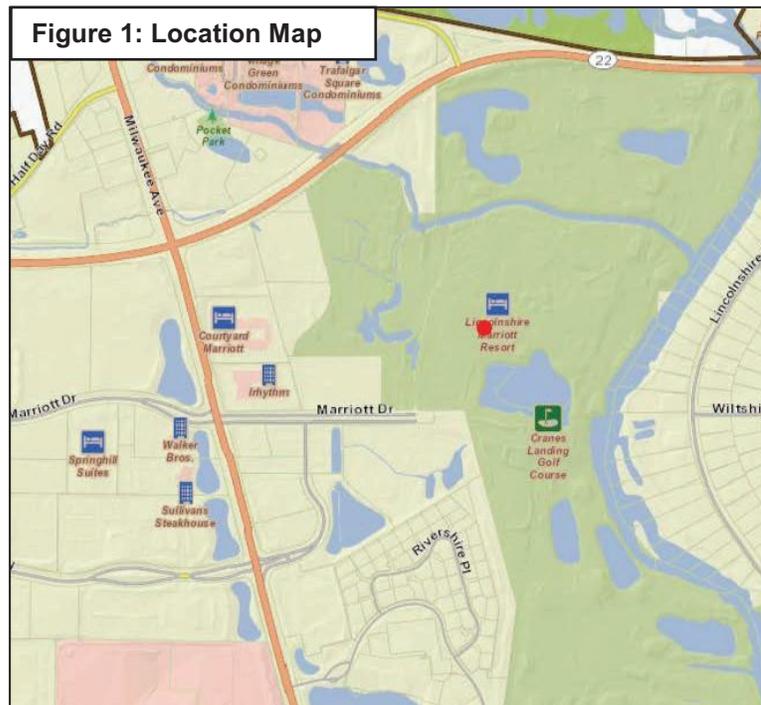
**Petitioner:** Marriott Lincolnshire Resort

**Originated By/Contact:** Tonya Zozulya, Planning & Development Manager

**Advisory Board Review:** Architectural Review Board

**Background:**

- Marriott Lincolnshire Resort (resort) seeks an amendment to its Master Sign Plan to revise existing ground monument and directional signs on the resort property. The property is located at 10 Marriott Drive (see Figure 1 and attached location map in Document 1).
- The 175-acre resort is one of the largest developments within the Village, offering a variety of destination uses, including a hotel, live theater, golf course, restaurants, sports facilities, meeting spaces, and banquet facilities.
- The property was granted its initial Special Use permit in 1973 for a live theater and golf course (Ordinance #73-286-1).
- In 2011, the Village Board approved the resort’s Master Sign Plans (Ordinance #11-3191-13A; see Document 5) to permit two new ground monument signs on Milwaukee Avenue/Marriott Drive and Half Day Road, as well as six directional signs throughout the resort campus. The Master Sign Plans included variations for sign dimensions, items of information, and an electronic display screen for the Milwaukee Avenue ground monument sign.



- Subsequent to initial approval, the Village Board approve three amendments to the Master Sign Plans:
  - 2016: the Village Board approved an amendment to the Master Sign Plans to permit two new Marriott identification wall signs adjacent to the hotel entrance as part of a renovation to the main entrance of the resort (Ordinance #16-3416-143).
  - 2017: the Village Board approved an amendment to the Master Sign Plans to permit an electronic display screen for a new marquee sign for the Marriott Theater (Ordinance #17-3742-170).
  - 2018: the Village Board approved an amendment to the Master Sign Plans to permit a new wall sign and revised directional signs for the College Park Athletic Club (Ordinance #18-3767-195).
- Attached is a draft ordinance, prepared by the Village Attorney, amending Marriott’s Sign Master Plan (see attached Document 2).

**Proposal Summary:**

- Ground Signs – The applicant proposes revisions to the two existing ground signs on Half Day Road and at Milwaukee Avenue/Marriott Drive (see attached Documents 3 and 4).

*Milwaukee Avenue/Marriott Drive*

The 20’ tall by 12’-8” wide ground sign at Milwaukee Avenue/Marriott Drive is proposed to be updated with a new Marriott logo at the top as well as two new panels. The sign cabinet is also proposed to be repainted from cream to white. The number of panels, sign structure/panel dimensions, and lettering/illumination type will not change. The existing and proposed panels are shown below in Table 1.

**Table 1: Marriott Resort Ground Sign Panels – Milwaukee Avenue/Marriott Drive**

Existing Panels (from top to bottom)	Proposed Panels (from top to bottom)
Marriott Theatre	Three Embers
Crane’s Landing Golf Club	Wright’s Brew & Bistro

*Half Day Road*

The petitioner proposes to update the 17’ tall by 12’ wide ground monument sign on Half Day Road with a new Marriott logo and four new tenant panels. The sign cabinet is also proposed to be repainted from cream to white. The number of tenant panels, sign structure/panel dimensions, and lettering/illumination type will not change. The applicant will reinstall previously approved landscaping at the base of the sign. The existing and proposed panels are shown below:

**Table 2: Marriott Resort Ground Sign Panels – Half Day Road**

Existing Panels (from top to bottom)	Proposed Panels (from top to bottom)
Marriott Theatre	Marriott Theatre
Crane’s Landing Golf Club	Three Embers
Season Golf Memberships craneslandinggolf.com	The Spa at Lincolnshire
Left on Milwaukee	Crane’s Landing Golf Course

**Directional Signs** – All six existing directional signs of varying dimensions are proposed to be repainted white from the current tan color, with the lettering color revised from white to black. One directional sign that displays the old Marriott logo will be updated with the current logo. No structural, dimensional, or landscaping alterations are proposed.

**Architectural Review Board Recommendation:**

- On March 19, 2019, the Architectural Review Board (ARB) reviewed the request and unanimously recommended approval, subject to the petitioner refacing five additional directional signs to match the two ground signs and one directional sign submitted to the ARB for review (see attached Document 6).
- The petitioner complied with the ARB recommendation by providing renderings showing the directional signs faces will be painted white, which necessitated the lettering color change from white to black.

**Approval Process:**

Marriott’s Master Sign Plans require ARB review and recommendation and a final approval by the Village Board for the current request. No preliminary evaluation or public hearing is required, per Section 6 of the original Master Sign Plans Ordinance (Ordinance #11-3191-13A). The ordinance provides the Director of Community Development authority to determine if any modifications to the signs alter their architecture, intensity, and purposes. Based on the petitioner’s current request, these proposed modifications do not alter any of the aforementioned sign qualities. The draft ordinance is presented to the Village Board for consideration and approval to memorialize the Village’s approval of the amendment. Previous Master Sign Plans amendments for the Marriott property have been approved via the same process.

**Recommendation:**

Consideration of an amendment to Ordinance #11-3191-13A regarding the ground and directional sign revisions, and placement on the April 22, 2019 Consent Agenda for approval.

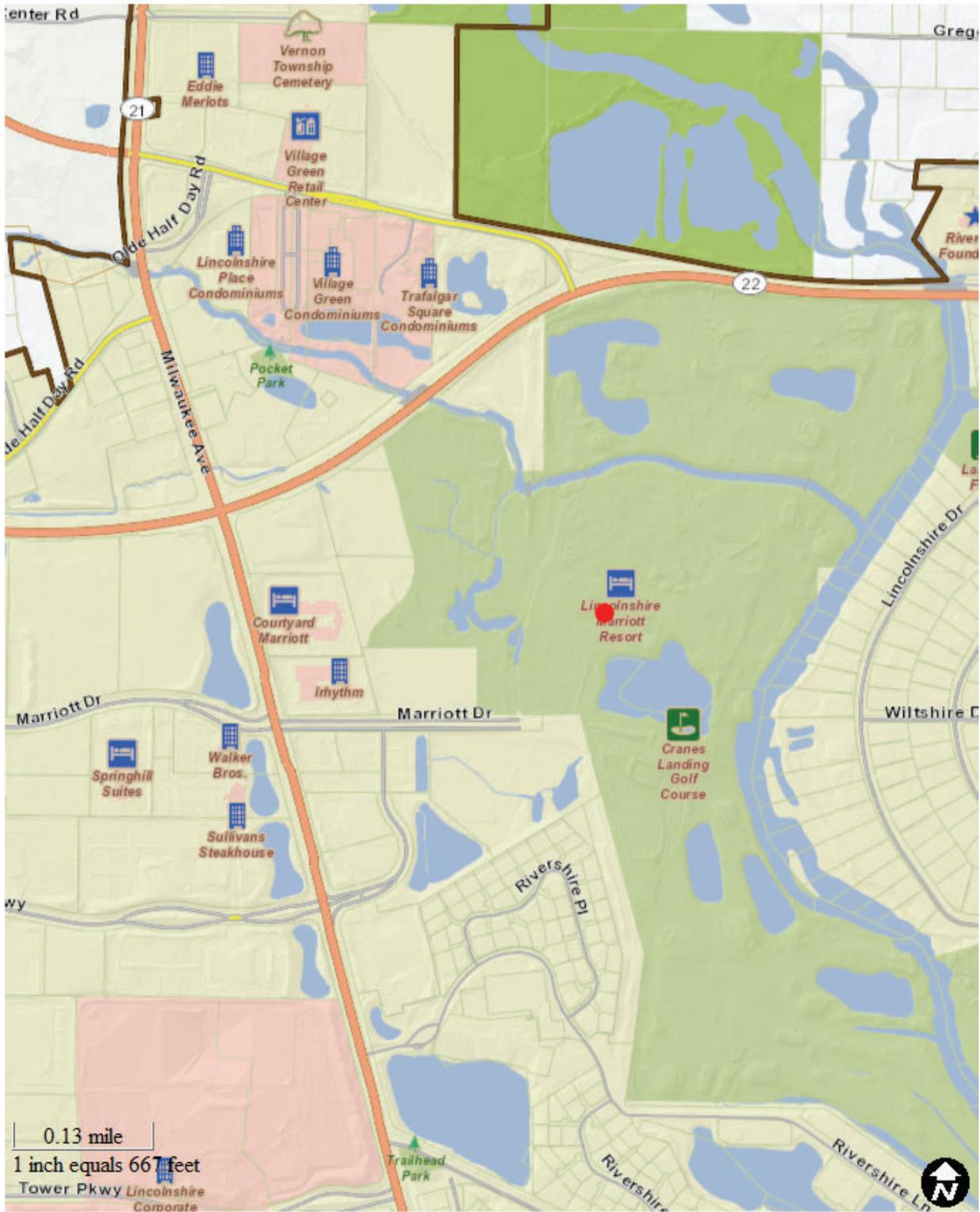
**Reports and Documents Attached:**

- Document 1: Location map, prepared by MGP GIS Consortium.
- Document 2: Petitioner’s cover letter, prepared by Lincolnshire Marriott Resort, dated April 8, 2019.
- Document 3: Ground and directional sign renderings, prepared by North Shore Sign, dated March 7, 13, and 29, 2019.
- Document 4: Marriott’s Master Sign Plan (Ordinance #11-3191-13A).
- Document 5: March 19, 2019 unapproved Architectural Review Board meeting minutes.
- Document 6: Draft ordinance amending the Marriott Resort Master Sign Plan, prepared by the Village Attorney.

<b>Meeting History</b>	
Architectural Review Board:	March 19, 2019
Committee of the Whole (current):	April 8, 2019



### 10 Marriott Drive



Map created on March 12, 2019.  
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Disclaimer: This map is for general information purposes only. Although the information is believed to be generally accurate, errors may exist and the user should independently confirm for accuracy. The map does not constitute a regulatory determination and is not a base for engineering design. A Registered Land Surveyor should be consulted to determine precise location boundaries on the ground.



Mayor Elizabeth Brandt and Village Trustees

April 8, 2019

Dear Mayor Brandt,

I am writing to you today to propose our plans to update our current monument signs at the Marriott Lincolnshire Resort. Our existing signs are not currently displaying the updated amenities within the resort and also the logo on top is not approved brand standard by Marriott International.

Our plans are to switch out the existing panels with the new logoed panels. The monument sign, height, shape, and location will not be altered. Everything will remain the same, just updating the names of the outlets.

The sign on Route 22 currently has four panels of signs. The new sign will also have 4 panels of signs. All 4 current panels will be refaced with new lettering and logos. No existing lettering will remain. The top of the sign will also be refaced with the current Marriott brand standard logo.

The sign on Milwaukee Ave currently has 2 panels. The new sign will also have 2 full panels. Both panels will be refaced with new lettering and logos. No existing lettering will remain. There will be no change to the existing digital display. The top lettering will also change with the new Marriott logo.

From our first submission there was a concern over the Marriott sign panel only on the Route 22 sign. This is correct since the digital display on the Milwaukee sign is mostly theater advertisements and serves as directional guidance for guests to use that entrance.

The landscaping area around the monument sign will not be altered at all. New greenery will be added come spring time 2019. If you have any questions please do not hesitate to contact me.

There are also 5 additional directional signs we spoke about with the ARB committee on March 19<sup>th</sup>. We are currently receiving bids to repaint the signs to have a white background and new black lettering. With exception of the new white background and black lettering there will be no additional work completed to the signs. They will remain in the same areas and landscaping will not be altered.

On March 19<sup>th</sup> at the ARB committee meeting we received a unanimous positive recommendation.

Brad Lajoie

General Manager



RE  
DEC - 1, 2010  
VILLAGES OF LINCOLNSHIRE  
COMMUNITY DEVELOPMENT

Half Day Rd Ground Sign

Milwaukee Ave Ground Sign

10 Marriott Dr, Lincolnshire, IL 60069

1 Directional Sign

2 Directional Sign

3 Directional Sign

4 Directional Sign

5 Directional Sign

6 Directional Sign

[White Box]

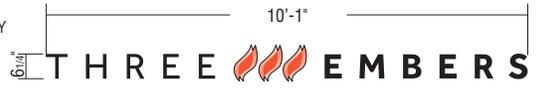


**MARRIOTT DRIVE ENTRANCE LOCATION**  
 REFACE EXISTING D/FACE ILLUMINATED DISPLAY  
 3/8" = 1'-0"  
 FLAT ALUMINUM PANELS  
 WHITE FINISH / ROUTED-OUT  
 BACKED-UP WHITE PLEXIGLAS COPY  
 3/4" THICK CLEAR PUSH-THRU  
 PLEXIGLAS COPY & LOGO  
 TRANSLUCENT RED APPLIED VINYL 1ST SURFACE  
 OPAQUE BLACK APPLIED VINYL 1ST SURFACE COPY  
 REPAINT EXISTING CABINET WHITE

EXISTING MESSAGE CENTER

(2) FLAT ALUMINUM PANELS  
 WHITE FINISH / ROUTED-OUT  
 BACKED-UP WHITE PLEXIGLAS COPY  
 3/4" THICK CLEAR PUSH-THRU  
 PLEXIGLAS COPY & LOGO

"THREE EMBERS" COPY / OPAQUE BLACK  
 VINYL APPLIED TO 1ST SURFACE OF PLEXIGLAS  
 TRANSLUCENT DIGITAL PRINTED "FLAMES"  
 APPLIED TO PLEXIGLAS SURFACE



"WRIGHT'S BREW & BISTRO" COPY / OPAQUE BLACK  
 VINYL APPLIED TO 1ST SURFACE OF PLEXIGLAS



ENLARGED LOGO DETAILS  
 1/2" = 1'-0"

**NORTH SHORE SIGN**  
 1925 Industrial Drive Libertyville, Illinois 60048 847-816-7020  
 "Quality Signage Since 1930"

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REVISED		COMMENTS	
E	3/13/19	CHANGE LAYOUT VIEW	
MARRIOTT LINCOLNSHIRE RESORT LINCOLNSHIRE, IL.			
SCALE	NOTED	DRAW BY:	ART
DATE	6/27/18	REVISED	
SALESPERSON	AS	DRAWING #	11471
		PG.	1



NIGHT VIEW

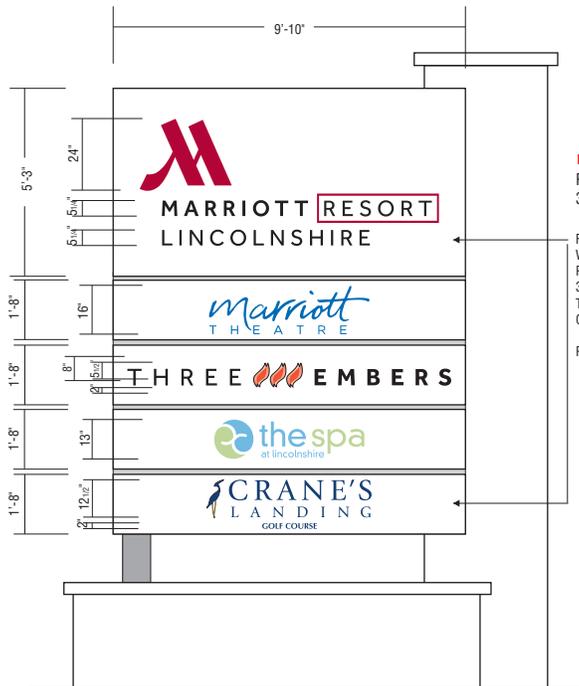


EXISTING SIGN PHOTO

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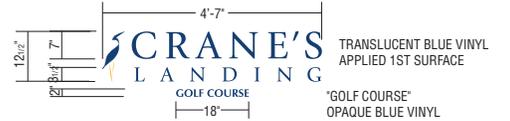
REVISED		COMMENTS	
E	3/13/19	CHANGE LAYOUT VIEW	
MARRIOTT LINCOLNSHIRE RESORT LINCOLNSHIRE, IL.			
SCALE	NOTED		DRAW BY: ART
DATE	6/27/18		REVISED
SALESPERSON	AS		DRAWING # 11471 PG. 2



**ROUTE 22 LOCATION**  
 REFACE EXISTING D/FACE ILLUMINATED DISPLAY  
 3/8" = 1'-0"

FLAT ALUMINUM PANELS  
 WHITE FINISH / ROUTED-OUT BACKED-UP WHITE  
 PLEXIGLAS COPY  
 3/4" THICK CLEAR PUSH-THRU PLEXIGLAS COPY & LOGO  
 TRANSLUCENT RED APPLIED VINYL 1ST SURFACE  
 OPAQUE BLACK APPLIED VINYL 1ST SURFACE COPY

REPAINT EXISTING CABINET WHITE



ENLARGED LOGO DETAILS  
 1/2" = 1'-0"

**NORTH SHORE SIGN**  
 1925 Industrial Drive Libertyville, Illinois 60048 847-816-7020  
 "Quality Signage Since 1930"

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REVISED	COMMENTS
E 3/13/19	CHANGE LAYOUT VIEW

MARRIOTT LINCOLNSHIRE RESORT LINCOLNSHIRE, IL.		DRAW BY: ART
SCALE NOTED		REVISED
DATE 6/27/18		DRAWING # 11471
SALESPERSON AS		PG. 3



NIGHT VIEW



EXISTING SIGN PHOTO




**NORTH SHORE SIGN**  
 1925 Industrial Drive Libertyville, Illinois 60048 847-816-7020  
*"Quality Signage Since 1930"*

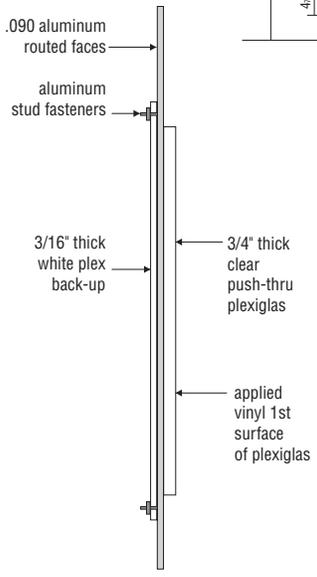
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REVISED		COMMENTS	
E	3/13/19	CHANGE LAYOUT VIEW	
MARRIOTT LINCOLNSHIRE RESORT LINCOLNSHIRE, IL.			
SCALE	NOTED		DRAW BY: ART
DATE	6/27/18		REVISED
SALESPERSON	AS		DRAWING # 11471 PG. 4



**MARRIOTT DRIVE LOCATION**  
 REFACE EXISTING D/FACE ILLUMINATED DISPLAY  
 3/4" = 1'-0"

FLANGED ALUMINUM PANELS 1-1/2" DEEP  
 WHITE FINISH / ROUTED-OUT BACKED-UP WHITE PLEXIGLAS COPY  
 3/4" THICK CLEAR PUSH-THRU PLEXIGLAS COPY & LOGO  
 TRANSLUCENT RED APPLIED VINYL 1ST SURFACE  
 OPAQUE BLACK APPLIED VINYL 1ST SURFACE COPY  
 REPAINT EXISTING CABINET WHITE



SECTIONAL DETAIL VIEW  
 (nts)



NIGHT VIEW

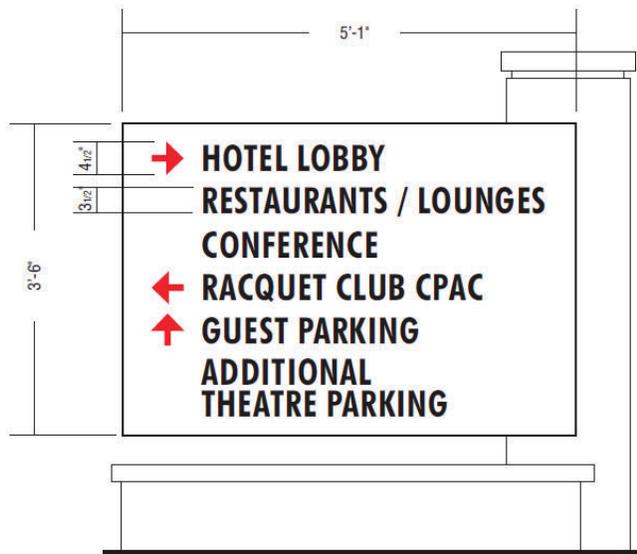


EXISTING SIGN PHOTO

**NORTH SHORE SIGN**  
 1925 Industrial Drive Libertyville, Illinois 60048 847-816-7020  
 "Quality Signage Since 1930"

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REVISED		COMMENTS	
D	3/7/19	CHANGE LAYOUT VIEW	
MARRIOTT LINCOLNSHIRE RESORT LINCOLNSHIRE, IL.			
SCALE	NOTED	DRAW BY: ART	
DATE	6/27/18	REVISED	
SALESPERSON	AS	DRAWING # 11471 PG. 5	



SINGLE FACE FLANGE PANEL  
3/4" = 1'-0"

FLANGED METAL PANEL / 1" BEND BACK  
SATIN WHITE POLYURETHANE FINISH

1/4" THICK FCO PLEXIGLAS BLACK LETTERS  
RED PLEX ARROWS / FLUSH MOUNTED TO  
BKGD. WITH DOUBLE STICK ADHESIVE TAPE  
RE-PAINT EXISTING CABINET SATIN WHITE  
POLYURETHANE FINISH



EXISTING SIGN PHOTO

REFACE & REFINISH EXISTING SINGLE FACE NON-ILLUM DISPLAY



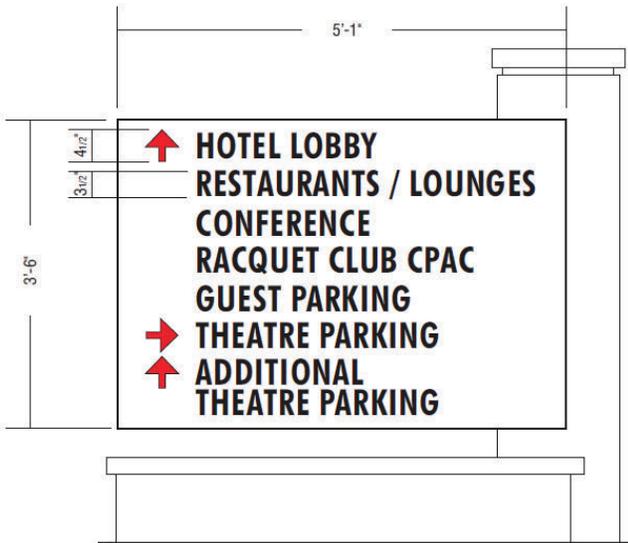
**NORTH SHORE SIGN**  
1925 Industrial Drive Libertyville, Illinois 60048 847-816-7020  
"Quality Signage Since 1930"



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MARRIOTT LINCOLNSHIRE RESORT 10 MARRIOTT DR. LINCOLNSHIRE, IL.		SALES PERSON ART
SCALE	NOTED	DRAWING # 11770 PG. 1
DATE	3/29/19	
DRAWN BY:	AS	



REFACE & REFINISH EXISTING SINGLE FACE NON-ILLUM DISPLAY

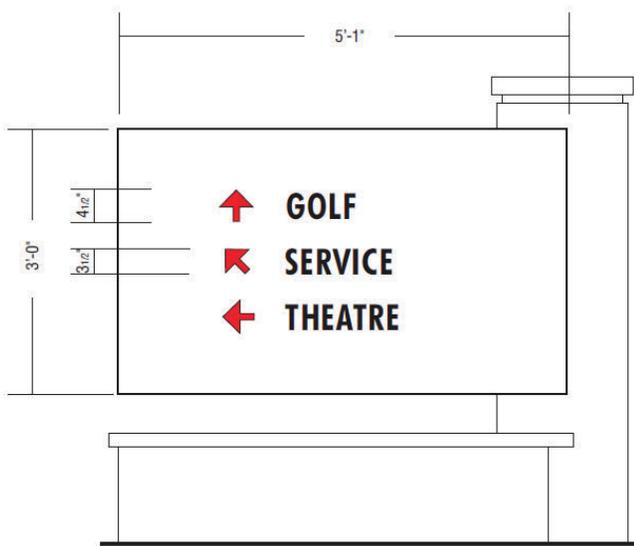
SINGLE FACE FLANGE PANEL  
3/4" = 1'-0"

FLANGED METAL PANEL / 1" BEND BACK  
SATIN WHITE POLYURETHANE FINISH

1/4" THICK FCO PLEXIGLAS BLACK LETTERS  
RED PLEX ARROWS / FLUSH MOUNTED TO  
BKGD. WITH DOUBLE STICK ADHESIVE TAPE  
RE-PAINT EXISTING CABINET SATIN WHITE  
POLYURETHANE FINISH



EXISTING SIGN PHOTO



SINGLE FACE FLANGE PANEL  
3/4" = 1'-0"

FLANGED METAL PANEL / 1" BEND BACK  
SATIN WHITE POLYURETHANE FINISH

1/4" THICK FCO PLEXIGLAS BLACK LETTERS  
RED PLEX ARROWS / FLUSH MOUNTED TO  
BKGD. WITH DOUBLE STICK ADHESIVE TAPE  
RE-PAINT EXISTING CABINET SATIN WHITE  
POLYURETHANE FINISH



EXISTING SIGN PHOTO

REFACE & REFINISH EXISTING SINGLE FACE NON-ILLUM DISPLAY



MARRIOTT LINCOLNSHIRE RESORT  
10 MARRIOTT DR. LINCOLNSHIRE, IL.

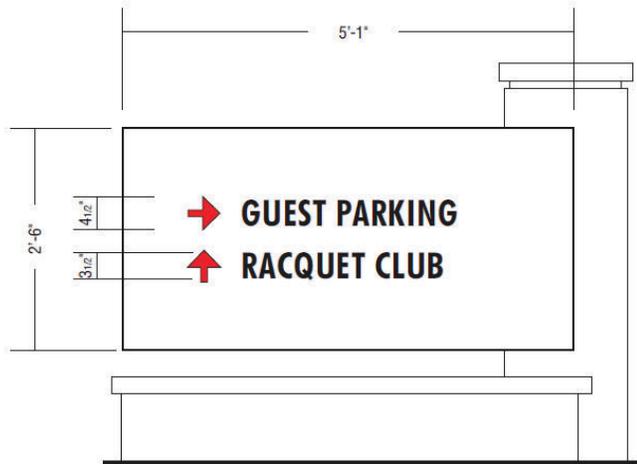
SCALE NOTED

DATE 3/29/19

DRAWN BY: AS

SALESPERSON  
ART

DRAWING # 11770  
PG. 3



REFACE & REFINISH EXISTING SINGLE FACE NON-ILLUM DISPLAY



EXISTING SIGN PHOTO

SINGLE FACE FLANGE PANEL  
3/4" = 1'-0"

FLANGED METAL PANEL / 1" BEND BACK  
SATIN WHITE POLYURETHANE FINISH

1/4" THICK FCO PLEXIGLAS BLACK LETTERS  
RED PLEX ARROWS / FLUSH MOUNTED TO  
BKGD. WITH DOUBLE STICK ADHESIVE TAPE  
RE-PAINT EXISTING CABINET SATIN WHITE  
POLYURETHANE FINISH

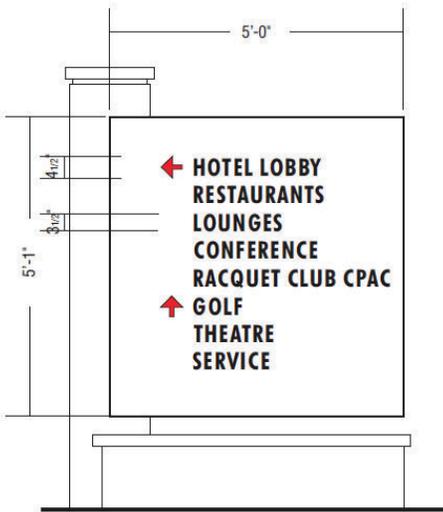
**NORTH SHORE SIGN**  
1925 Industrial Drive Libertyville, Illinois 60048 847-816-7020  
"Quality Signage Since 1930"



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<b>MARRIOTT LINCOLNSHIRE RESORT</b> 10 MARRIOTT DR. LINCOLNSHIRE, IL.		SALESPERSON <b>ART</b>
SCALE	NOTED	DRAWING # <b>11770</b> PG. 4
DATE	3/29/19	
DRAWBY:	AS	



REFACE D/FACE ILLUMINATED DISPLAY  
3/4" = 1'-0"

FABRICATED ALUMINUM FACES  
SATIN WHITE POLYURETHANE FINISH  
ROUTED-OUT / BACKED-UP WHITE PLEXIGLAS  
COPY & ARROWS  
1/2" THICK CLEAR PUSH-THRU PLEXIGLAS

ARROWS - RED PERFORATED APPLIED VINYL  
COPY - BLACK PERFORATED APPLIED VINYL

REPAINT EXISTING CABINET / SATIN WHITE FINISH



EXISTING SIGN PHOTO



REFACE & REFINISH EXISTING SINGLE FACE ILLUMINATED DISPLAY

**NORTH SHORE SIGN**  
1925 Industrial Drive Libertyville, Illinois 60048 847-816-7020  
"Quality Signage Since 1930"



Colors depicted on this drawing are printed simulations to assist in visualizing the design. They do not accurately reflect the actual colors specified.

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MARRIOTT LINCOLNSHIRE RESORT 10 MARRIOTT DR. LINCOLNSHIRE, IL.		SALES PERSON ART
SCALE	NOTED	DRAWING # 11770 PG. 5
DATE	3/29/19	
DRAWN BY	AS	

STATE OF ILLINOIS )  
 ) SS.  
COUNTY OF LAKE )

CLERK'S CERTIFICATE

I, **BARBARA MASTANDREA**, do hereby certify that I am the duly appointed and qualified Village Clerk for the Village of Lincolnshire, Lake County, Illinois.

I do further certify that the above and attached is a true and correct copy of an Ordinance entitled:

**AN ORDINANCE  
GRANTING VARIANCES FROM  
TITLE 12 OF THE VILLAGE CODE  
RELATED TO MARRIOTT'S LINCOLNSHIRE RESORT  
MASTER SIGN PLANS  
(10 Marriott Drive)**

I do further certify that the aforesaid Ordinance was entrusted to my care and custody, that the same is duly spread upon the record of proceedings of said Village, and that I am the custodian of all Village records, including the journal of proceedings, ordinances, and resolutions of said Village.

**IN WITNESS WHEREOF**, I have hereunto set my hand and seal this 28th day of February, 2011.

  
Village Clerk  
Village of Lincolnshire  
Lake County

Prepared by and Mail to:  
Village of Lincolnshire  
One Olde Half Day Road  
Lincolnshire, IL 60069

**ORDINANCE NO. 11-3191-13A**

**AN ORDINANCE  
GRANTING VARIANCES FROM  
TITLE 12 OF THE VILLAGE CODE  
RELATED TO MARRIOTT'S LINCOLNSHIRE RESORT  
MASTER SIGN PLANS  
(10 Marriott Drive)**

**WHEREAS**, application has been made by White Way Sign, Inc., as authorized by Strategic Hotel Capital, Inc. d/b/a Marriott's Lincolnshire Resort ("Owner")(collectively, the "Petitioner"), for approval of a Master Sign Plan for the replacement of the existing resort signage at Marriott's Lincolnshire Resort, with variations to Sections 12-9-1(A)(1), 12-9-1(A)(13), 12-8-1(H)(1), 12-11-1(T) , and 12-9-1(G)(1) of the Lincolnshire Sign Control Ordinance (collectively, the "Variances"), to permit the installation of two monument ground signs and six directional signs on property commonly known as 10 Marriott Drive, Lincolnshire, Illinois (the "Subject Property"); and

**WHEREAS**, a public hearing was duly advertised on October 28, 2010, in the Daily Herald and was convened by the Architectural Review Board on November 16, 2010, and continued until finally adjourned on January 18, 2011, on which date the Architectural Review Board voted in favor of recommending approval of the Petitioner's application for said Variances; and

**WHEREAS**, the Architectural Review Board has heretofore submitted to the Mayor and Board of Trustees of the Village of Lincolnshire, Lake County, Illinois, its findings of fact and recommendations related thereto, including that the Variance satisfies the standards to qualify for a sign variance set forth in Section 12-17-1 of the Village Code, attached hereto as Exhibit D;

**WHEREAS**, the Corporate Authorities of the Village of Lincolnshire, Lake County, Illinois, have duly considered said finding and recommendations of said Architectural Review Board; and

**WHEREAS**, the Corporate Authorities desire to address in this ordinance only that part of the Petitioner's application relating to the Marriott Drive Monument Sign and the internal Directional Signs, and wish to save that part of the Petitioner's application related to the Illinois Route 22 Monument Sign for a second and separate ordinance to be adopted hereafter.

**NOW, THEREFORE, BE IT ORDAINED** by the Mayor and Board of Trustees of the Village of Lincolnshire, Lake County, Illinois, in exercise of its home rule authority, as follows:

**SECTION 1: FINDINGS:**

- A. The findings and recommendations of the Architectural Review Board of the Village of Lincolnshire, Lake County, Illinois, are herein incorporated by reference as the findings of this Board to the same effect as if fully recited herein at length. All references in the findings and recommendations are made the references of the Mayor and Board of Trustees of the Village of Lincolnshire.
- B. The Mayor and Board of Trustees hereby further find and declare that the Marriott's Lincolnshire Resort is a unique development within the Village insofar as it provides a singular mix of products and services which contribute to the welfare of the Village and, as a result of such characteristics, its special contributions to the Village make it uniquely eligible for special relief from the strict application of the Sign Code, including the opportunity to use Electronic Signs to advertise the variety of products and services available at the resort.

**SECTION 2:** That the property which is the subject of this Ordinance is legally described as set forth in **Exhibit A**, attached hereto and incorporated as though fully set forth herein.

**SECTION 3:** Subject to the conditions described in Section 4 below, variances from Sections 12-9-1(A)(1), 12-9-1(A)(13), 12-8-1(H)(1), 12-11-1(T) , and 12-9-1(G)(1) are hereby granted and issued to the Subject Property at 10 Marriott Drive, as herein more specifically described and as depicted on the sign plans attached hereto in **Exhibit B**, for the purpose of permitting the following relief:

**Monument Ground Sign (Marriott Drive)**

1. A variation to Section 12-9-1(A)(1), Ground Signs, to increase the height of a monument sign to 20'-6", rather than the code permitted 6' maximum sign height.
2. A variation to Section 12-9-1(A)(1), Ground Signs, to increase the length of a monument sign to 12'-8", rather than the code permitted 10' maximum sign length.
3. A variation to Section 12-9-1(A)(1), Ground Signs, to increase the sign area of a monument sign to 260 square feet, rather than the code permitted 60-square foot maximum sign area.
4. A variation to Section 12-9-1(A)(13), Ground Signs, to locate the copy area of a monument sign less than six inches from the perimeter of the sign face, rather than the code permitted six inch minimum separation from the perimeter of the sign face.
5. A variation to Section 12-8-1(H)(1), Items of Information, to increase the items of information on a single sign face to four, rather than the code limitation of not more than two items of information.
6. A variation to Section 12-11-1(T), Prohibited Signs, to permit the use of an electronic display screen, not permitted.

**Directional Signs**

1. Variations to Section 12-9-1(G)(1), Directional Signs, to increase the height of Directional Sign 1 to 6'-8", Directional Sign 2 to 7'-8", Directional Sign 3 to 5'-8", Directional Sign 4 to 6'-2", Directional Sign 5 to 5'-8", and Directional Sign 6 to 4'-8", rather than the code permitted 4' maximum sign height.
2. Variations to Section 12-9-1(G)(1), Directional Signs, to increase the sign area of Directional Sign 1 to 20 square feet, Directional Sign 2 to 25 square feet, Directional Sign 3 to 15 square feet, Directional Sign 4

to 17.5 square feet, Directional Sign 5 to 17.5 square feet, and Directional Sign 6 to 12.5 square feet, rather than the code permitted three-square foot maximum sign area.

**SECTION 4:** The variances herein described shall not become effective unless and until the Petitioner causes the Master Sign Plan to comply with these conditions:

- A. The Marriott Drive Monument Sign location shall be no closer west (towards the Marriott Drive and Milwaukee Avenue intersection) than the existing resort sign.
- B. A minimum time limit/interval of no less than 10 seconds between images or text shall be required for the electronic display screen.
- C. The electronic display screen shall not display any (i) advertising related to the price, rate or cost for any merchandise, services, or activities, and (ii) personal messages directed at or for the benefit of individuals or groups.
- D. The content displayed on the electronic display screen and the transition between messages and/or images shall not involve any flashing, blinking, scrolling, rotation, animation or similar effects.
- E. The brightness level of the electronic display screen shall be limited to 5,000 nits during the day and 1,000 nits at night, with the L.E.D. panel equipped with a light sensor to automatically adjust brightness based on ambient light levels, provided that the brightness level of the electronic (L.E.D.) sign components shall be subject to the final acceptance of the Community Development Department.
- F. The landscape plans shall be revised to add more grasses and evergreens, where appropriate, and shall be determined in the field by Staff and the Petitioner to achieve four-season interest and verticality.
- G. The Marriott Lincolnshire Resort commitments, as contained in the letter dated December 1, 2010.

**SECTION 5:** Review.

The Village Board shall review the criteria in which the electronic display screen displays content (as described in Section 4 above), within 120 days from the date of this Ordinance, which may be extended by resolution of the Village Board. At such time, the Village Board may without further public hearing amend this ordinance to adjust the

means by which content may be displayed on the electronic display screen of the Marriott Drive Monument Sign.

- A. The Village Board expressly reserves and retains jurisdiction of Petitioner's application for the purpose of (i) granting, denying or granting with conditions that portion of the application related to the proposed Monument Sign located on Illinois Route 22; and (ii) amending the authority hereby granted as described in paragraph A above; each or all without need for further hearing.

**SECTION 6:** In the event the Petitioner, or its successor and assigns, elects to maintain the signs described herein in a manner providing for the same intensity and purposes approved by this Ordinance, any future sign face changes may be made only with the recommendation of the Architectural Review Board and approval of the Village Board. The decision whether future modifications alter the architecture or preserve or expand the intensity and purposes for which the approved signs are used shall be made in the sole discretion of the Director of Community Development.

**SECTION 7:** The specific terms and conditions of this Ordinance shall prevail against other existing ordinances of the Village to the extent that there might be any conflict. Except for the foregoing limitation, the development of the Subject Property is subject to all terms and conditions of applicable ordinances and regulations of the Village of Lincolnshire.

**SECTION 8:** The following exhibits shall be attached to and made a part of this Ordinance and, except as expressly modified by this Ordinance, all covenants, standards, requirements, designs or specifications in such exhibits shall be binding on the Petitioner:

- A. Sign Packet, prepared by White Way Sign, Inc., date stamp received December 1, 2010, attached hereto in **Exhibit B**;
- B. Owner's Letter, dated December 1, 2010, regarding Electronic Sign (L.E.D.) brightness and limiting off-site advertising, attached hereto in **Exhibit C**.

**SECTION 9:** Any person violating the terms and conditions of this Ordinance shall be subject to a penalty not exceeding Five Hundred Dollars (\$500.00) with each and every day that the violation of the Ordinance is allowed to remain in effect being deemed a complete and separate offense. In addition, the appropriate authorities of the Village may take such other action as they deem proper to enforce the terms and conditions of this Ordinance, including, without limitation, an action in equity to compel compliance with its terms. Any person violating the terms of this Ordinance shall be subject, in addition to the foregoing penalties, to the payment of court costs and reasonable attorneys' fees. This section shall not apply to the Village of Lincolnshire, its officials, agents or employees.

**SECTION 10:** The premises shall be made available for inspection by any department of the Village at all reasonable times for compliance with this Ordinance and any other laws or regulations.

**SECTION 11:** This Ordinance shall be in full force and effect from and after its passage, approval and publication in pamphlet form as provided by law. Provided, however, that this Ordinance shall not take effect until a true and correct copy of this Ordinance is executed by the Owner of the Subject Property or such other parties in interest consenting to and agreeing to be bound by the terms and conditions contained within this Ordinance. Such execution shall take place within sixty (60) days after the passage and approval of this Ordinance or within such extension of time as may be granted by the Corporate Authorities by motion.

**PASSED** this 28th day of February, 2011, by the Corporate Authorities of the Village of Lincolnshire on a roll call vote as follows:

**AYES:** Brandt, Feldman, McDonough, Saltiel, Servi

**NAYS:** None

**ABSTAIN:** None

**ABSENT:** Walder

**APPROVED** this 28<sup>th</sup> day of February, 2011.

  
\_\_\_\_\_  
Brett Blomberg, Mayor

**ATTEST:**

  
\_\_\_\_\_  
Barbara Mastandrea, Village Clerk

ACKNOWLEDGED and ACCEPTED  
this 30<sup>th</sup> day of March, 2011.

**STRATEGIC HOTEL CAPITAL, INC.  
D/B/A MARRIOTT LINCOLNSHIRE  
RESORT**

A handwritten signature in black ink, consisting of several loops and a long horizontal stroke extending to the right.

By:

*General Manager*

Its:



**UNAPPROVED** Minutes of the regularly scheduled **ARCHITECTURAL REVIEW BOARD** held on Tuesday, March 19, 2019 in the Public Meeting Room of the Village Hall, One Olde Half Day Road, Lincolnshire, IL

**PRESENT:** Members Orzeske, McCall, Baskin and Santosuosso

**ABSENT:** Chair Kennerley and Member Tapia; Trustee-Liaison Hancock,

**ALSO PRESENT:** Ben Gilbertson, Assistant Village Manager/Director of Community and Economic Development (AVM/CED)  
Tonya Zozulya, Planning and Development Manager (PDM)

## CALL TO ORDER

### 1.0 ROLL CALL

The roll was called by **Tonya Zozulya, PDM** and Member Orzeske declared a quorum to be present.

**Member Baskin** moved and **Member Santosuosso** seconded the motion to appoint **Member Orzeske** Interim Chair. Motion passed unanimously by voice vote.

### 2.0 APPROVAL OF MINUTES

2.1 Approval of the Minutes of the Architectural Review Board held on Tuesday, February 19, 2019.

**Member Baskin** moved and **Member Santosuosso** seconded the motion to approve the minutes as written for the February 19, 2019 Architectural Review Board. The motion passed unanimously by voice vote.

### 3.0 ITEMS OF GENERAL BUSINESS

3.1 Consideration of an Amendment to Existing Master Sign Plans (Ordinance #11-3191- 13A) to Revise Existing Ground Monument and Directional Signs for the Marriott Lincolnshire Resort, 10 Marriott Drive (Marriott Lincolnshire Resort)

**Brad Lajoie**, General Manager of the Marriott Lincolnshire Resort, reviewed the proposed revisions to the two existing monument signs on Half Day Road and on Milwaukee Avenue/Marriott Drive. He outlined the following proposed changes:

The entrance ground sign on Marriott Drive will include the new Marriott logo and two revised panels to display Three Embers and Wright's Brew and Bistro. No changes are proposed for the either the electronic portion of this sign, or its size.

The Half Day Road/Route 22 ground sign will include the new Marriott logo as well as four revised panels to display Marriott Theatre, Three Embers, the



Spa at Lincolnshire and Crane's Landing Golf Course. The sign size will remain the same. Landscaping will be restored to the originally-approved landscape plan.

One interior directional signs would be updated to include the new Marriott logo. The background will be white and will remain illuminated.

**Interim Chair Orzeske** inquired about the paint finish and durability. **Art Solis, North Shore Signs**, stated the two ground and one directional sign will be repainted white, with life expectancy of the paint to be about 10 years.

**PDM Zozulya** stated the petitioner is before the ARB as this is an amendment to the Master Sign Plan approved in 2011. The Master Sign Plans included variations for sign dimensions, items of information, and an electronic display sign on Milwaukee. While a public hearing is not required, the ARB is required to make a recommendation to the Village Board for final approval.

There was discussion regarding the five remaining directional signs in the complex. **Tonya Zozulya** asked the petitioner if they would like to include those signs in the ARB recommendation and, if so, provide renderings of the color change to be included with the Village Board packet for consideration and final approval. **Brad Lajoie** agreed to include the directional signs for consideration.

**Member Baskin** moved, seconded by **Member McCall**, to recommend approval to the Village Board of the amendment to the Marriott Master Sign Plan amendment to permit revisions to the existing ground monument and directional signs for the Marriott Lincolnshire Resort, located at 10 Marriott Drive, as presented in the petitioner's cover letter and presentation packet, date stamped received March 11, 2019, with approval and inclusion of additional directional signs, consistent in coloring with the proofs submitted, with review by Village Staff and approval by the Village Board.

The motion passed unanimously by voice vote.

3.2 Consideration of Roof-Mounted Solar Panel Installation for Building C of Extra Space Storage Facility, 200 Parkway Drive (Rethink Electric LLC)

**Garrison Regal, Vice President of Operations, Rethink Electric LLC** reviewed the proposal to install roof-mounted solar panels on Building C of the storage facility at 200 Parkway Drive, CityPark Center. He presented aerial and ground elevation views of the complex, adding the solar modules will be obscured from view by trees and other buildings in the complex. The solar panels will be a flush-mount, low profile, and installed with non-penetrating methods. The size of each solar panel on this installation will be approximately 6.5' by 3.5'.

ORDINANCE NO. \_\_\_\_\_

**AN ORDINANCE  
AMENDING ORDINANCE NO. 11-3191-13A  
RELATED TO MARRIOTT'S LINCOLNSHIRE RESORT  
MASTER SIGN PLANS  
(10 Marriott Drive)**

**WHEREAS**, the Corporate Authorities adopted Ordinance No. 11-3191-13A on February 28, 2011, which granted approval of a Master Sign Plan on property commonly known as 10 Marriott Drive, Lincolnshire, Illinois (the "Subject Property");

**WHEREAS**, application has been made by [name of applicant], as authorized by [name of owner] ("Owner")(collectively, the "Petitioner"), to amend such Master Sign Plan to permit the Petitioner to (a) reface two existing ground monument signs to replace tenant names, and (b) change the color of one of the existing directional signs (the "Application"); and

**WHEREAS**, a public meeting was convened by the Architectural Review Board on March 19, 2019, on which date the Architectural Review Board voted in favor of recommending approval of the Petitioner's Application; and

**WHEREAS**, the Architectural Review Board has heretofore submitted to the Mayor and Board of Trustees of the Village of Lincolnshire, Lake County, Illinois, its findings of fact and recommendations related to the Application;

**WHEREAS**, the Corporate Authorities have duly considered said finding and recommendations of said Architectural Review Board; and

**NOW, THEREFORE, BE IT ORDAINED** by the Mayor and Board of Trustees of the Village of Lincolnshire, Lake County, Illinois, in exercise of its home rule authority, as follows:

**SECTION 1:** FINDINGS: The findings and recommendations of the Architectural Review Board of the Village of Lincolnshire, Lake County, Illinois, are herein incorporated by reference as the findings of this Board to the same effect as if fully recited herein at length. All references in the findings and recommendations are made the references of the Mayor and Board of Trustees of the Village of

Lincolnshire. All of the foregoing recitals are found to be true, complete and accurate and are incorporated as though fully restated herein.

**SECTION 2:** That the property which is the subject of this Ordinance is legally described as set forth in **Exhibit A**, attached hereto and incorporated as though fully set forth herein.

**SECTION 3:** Subject to the conditions described in Section 4 below, Exhibit B of Ordinance No. 11-3191-13A is amended only to the extent necessary to permit the Petitioner to (a) reface two existing ground monument signs to replace tenant names and change the color of the sign cabinet, and (b) change the color of the cabinet and lettering of six existing directional signs, as more specifically described in **Exhibit B**.

**SECTION 4:** The relief herein described shall not be implemented in a manner inconsistent with the following plans and conditions:

- A. Color ground and directional sign renderings prepared by North Shore Sign, dated 3/7/19, 3/13/19 and 3/29/19.

**SECTION 5:** Affirmation and Ratification. The Village expressly ratifies and reaffirms the terms and conditions of Ordinance No. 11-3191-13A, it being the intent of the Mayor and Board of Trustees for this Ordinance to be supplemental to and cumulative with such Ordinance. Any violation or breach of Ordinance No. 11-3191-13A shall be deemed a breach of this Ordinance the same as though all of the terms and conditions thereof were fully recited herein.

**SECTION 6:** In the event the Petitioner, or its successor and assigns, elects to maintain the signs described herein in a manner providing for the same architecture, intensity and purposes approved by this Ordinance, any future sign face changes may be made only with the approval of the Zoning Administrator. The decision whether future modifications alter the architecture or preserve or expand the intensity and purposes for which the approved signs are used shall be made in the sole discretion of the Zoning Administrator.

**SECTION 7:** The specific terms and conditions of this Ordinance shall prevail against other existing ordinances of the Village to the extent that there might be any conflict. Except for the foregoing limitation, the development of the Subject Property is subject to all terms and conditions of applicable ordinances and regulations of the Village of Lincolnshire.

**SECTION 8:** Any person violating the terms and conditions of this Ordinance shall be subject to a penalty not exceeding Five Hundred Dollars (\$500.00) with each and every day that the violation of the Ordinance is allowed to remain in effect being deemed a complete and separate offense. In addition, the appropriate authorities of the Village may take such other action as they deem proper to enforce the terms and conditions of this Ordinance, including, without limitation, an action in equity to compel compliance with its terms. Any person violating the terms of this Ordinance shall be subject, in addition to the foregoing penalties, to the payment of court costs and reasonable attorneys' fees. This section shall not apply to the Village of Lincolnshire, its officials, agents or employees.

**SECTION 9:** The premises shall be made available for inspection by any department of the Village at all reasonable times for compliance with this Ordinance and any other laws or regulations.

**SECTION 10:** This Ordinance shall be in full force and effect from and after its passage, approval and publication in pamphlet form as provided by law. Provided, however, that this Ordinance shall not take effect until a true and correct copy of this Ordinance is executed by the Owner of the Subject Property or such other parties in interest consenting to and agreeing to be bound by the terms and conditions contained within this Ordinance. Such execution shall take place within sixty (60) days after the passage and approval of this Ordinance or within such extension of time as may be granted by the Corporate Authorities by motion.

**PASSED** this \_\_\_\_\_ day of \_\_\_\_\_, 2019, by the Corporate Authorities of the Village of Lincolnshire on a roll call vote as follows:

**AYES:**

**NAYS:**

**ABSTAIN:**

**ABSENT:**

**APPROVED** this \_\_\_\_<sup>th</sup> day of \_\_\_\_\_, 2019.

\_\_\_\_\_  
Elizabeth J. Brandt, Mayor

**ATTEST:**

\_\_\_\_\_  
Barbara Mastandrea, Village Clerk

Published by me in pamphlet form  
this \_\_\_\_<sup>th</sup> day of \_\_\_\_\_, 2019.

ACKNOWLEDGED and ACCEPTED  
this \_\_\_\_ day of \_\_\_\_\_, 2019.

**OWNER**

By: \_\_\_\_\_

Its:

**EXHIBIT A**

**LEGAL DESCRIPTION OF THE SUBJECT PROPERTY**

**PARCEL 1:** THAT PART OF THE SOUTHWEST QUARTER OF SECTION 14, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING SOUTHERLY OF THE CENTER LINE OF STATE ROUTE 22 AND WESTERLY OF THE CENTER LINE OF THE DES PLAINES RIVER IN LAKE COUNTY, ILLINOIS.

AND

**PARCEL 2:** THAT PART OF THE WEST HALF OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS FOLLOWS: BEGINNING AT THE NORTHWEST CORNER OF THE NORTHWEST QUARTER OF SAID SECTION 23; THENCE SOUTH ALONG THE WEST LINE THEREOF, 603.05 FEET TO THE CENTER LINE OF INDIAN CREEK, AS CENTER LINE WAS LOCATED ON AUGUST 26, 1943; THENCE EASTERLY AND SOUTHERLY ALONG THE CENTER LINE AND THE CENTER LINE EXTENDED ON INDIAN CREEK TO THE CENTER LINE OF THE DES PLAINES RIVER, AS SAID CENTER LINE WAS LOCATED ON AUGUST 26, 1948; THENCE NORTHEASTERLY ALONG SAID CENTER LINE OF THE DES PLAINES RIVER TO A POINT ON THE EAST LINE OF THE WEST HALF OF THE NORTHWEST QUARTER OF SAID SECTION 23, 228.6 FEET SOUTHERLY OF THE NORTHEAST CORNER OF SAID WEST HALF OF THE NORTHWEST QUARTER; THENCE NORTHERLY ALONG SAID EAST LINE, 228.60 FEET TO THE NORTHEAST CORNER OF SAID WEST HALF OF THE NORTHWEST QUARTER, THENCE WESTERLY ALONG THE NORTH LINE OF SAID WEST HALF OF THE NORTHWEST QUARTER, 1338.85 FEET TO THE PLACE OF BEGINNING, IN LAKE COUNTY, ILLINOIS.

AND

**PARCEL 3:** THAT PART OF THE WEST HALF OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS FOLLOWS: BEGINNING AT A POINT WHICH IS SOUTH 72° 40' EAST 501.1 FEET FROM A POINT ON THE WEST LINE OF SAID WEST HALF OF THE NORTHWEST QUARTER OF SECTION 23, WHICH IS 783.9 FEET SOUTH FROM THE NORTHWEST CORNER OF SAID SECTION 23, THENCE SOUTH 23° EAST 654.1 FEET TO A POINT WHICH IS 733.9 FEET EAST OF THE WEST LINE AND 1515.7 FEET SOUTH OF THE NORTH LINE OF SAID WEST HALF OF THE NORTHWEST QUARTER; THENCE NORTH 67° EAST 410 FEET, MORE OR LESS, TO THE CENTER OF THE DES PLAINES RIVER, AS SAID CENTER LINE WAS LOCATED ON SEPTEMBER 20, 1934; THENCE NORTHERLY ALONG THE CENTER OF SAID RIVER, 660 FEET, MORE OR LESS, TO THE CENTER LINE EXTENDED OF INDIAN CREEK; THENCE WESTERLY ALONG THE CENTER OF SAID CREEK, 433 FEET, MORE OR LESS, TO A POINT WHICH IS 542.8 FEET EAST FROM THE WEST LINE AND 750.3 FEET SOUTH FROM THE NORTH LINE OF SAID WEST HALF OF THE NORTHWEST QUARTER; THENCE SOUTH 19° 20' WEST 205 FEET TO THE PLACE OF BEGINNING IN LAKE COUNTY, ILLINOIS.

AND

**PARCEL 4:** THAT PART OF THE WEST HALF OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS FOLLOWS: BEGINNING AT A POINT ON THE WEST LINE OF SAID WEST HALF, 764.05 FEET SOUTH OF THE NORTHWEST CORNER THEREOF, THENCE NORTH 89° 25' 30" EAST, 167.60 FEET; THENCE SOUTHEASTERLY ALONG A CURVE TANGENT TO THE LAST DESCRIBED COURSE HAVING A RADIUS OF 275.45 FEET AND CONCAVE, SOUTHWESTERLY, A DISTANCE OF 238.06 FEET, CHORD MEASURE; THENCE SOUTH 39° 22' EAST 14.15 FEET, THENCE SOUTHEASTERLY ALONG A CURVE TANGENT TO THE LAST DESCRIBED COURSE HAVING A RADIUS OF 114.06 FEET AND CONCAVE NORTHEASTERLY, A DISTANCE OF 100.90 FEET, MORE OR LESS, CHORD MEASURE TO THE NORTHWESTERLY LINE OF PREMISES CONVEYED BY WARRANTY DEED DATED MAY 20, 1946 AND RECORDED MAY 27, 1946 AS DOCUMENT NUMBER 590919, FROM GENE T. DYER AND HIS WIFE, TO DOROTHY T. JONES; THENCE NORTH 19° 20' EAST ALONG SAID NORTHWESTERLY LINE, 186.50 FEET, MORE OR LESS, TO THE CENTER LINE OF INDIAN CREEK AS SAID CENTER LINE WAS LOCATED ON AUGUST 26, 1948; THENCE WESTERLY AND NORTHERLY ALONG SAID CENTER LINE TO A POINT ON THE WEST LINE OF THE WEST HALF OF THE NORTHWEST QUARTER, 603.05 FEET SOUTH OF THE NORTHWEST CORNER THEREOF; THENCE SOUTH ALONG SAID WEST LINE, 161.0 FEET TO THE PLACE OF BEGINNING, IN LAKE COUNTY, ILLINOIS.

**AND**

**PARCEL 5:** THAT PART OF THE WEST HALF OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS FOLLOWS: BEGINNING AT A POINT ON THE WEST LINE OF SAID WEST HALF, 764.05 FEET SOUTH OF THE NORTHWEST CORNER THEREOF; THENCE NORTH 89° 25' 30" EAST 167.60 FEET; THENCE SOUTHEASTERLY ALONG A CURVE HAVING A RADIUS OF 275.45 FEET AND CONCAVE SOUTHWESTERLY, A DISTANCE OF 238.06 FEET; CHORD MEASURE; THENCE SOUTH 39° 22' EAST 14.15 FEET; THENCE SOUTHEASTERLY ALONG A CURVE HAVING A RADIUS OF 114.06 FEET AND CONCAVE NORTHEASTERLY A DISTANCE OF 100.90 FEET, MORE OR LESS, CHORD MEASURE, TO THE NORTHWESTERLY LINE OF PREMISES CONVEYED BY WARRANTY DEED DATED MAY 20, 1946 AND RECORDED MAY 27, 1946, AD DOCUMENT NUMBER 590919, FROM GENE T. DYER AND WIFE, TO DOROTHY T. JONES; THENCE SOUTH 19° 20' WEST ALONG THE NORTHWESTERLY LINE OF SAID DOROTHY T. JONES PROPERTY, 18.55 FEET TO A POINT WHICH IS SOUTH 72° 40' EAST 501.10 FEET FROM A POINT ON THE WEST LINE OF SAID WEST HALF OF THE NORTHWEST QUARTER, WHICH IS 783.90 FEET SOUTH FROM THE NORTHWEST CORNER OF SAID SECTION 23; THENCE SOUTH 23° EAST 654.10 FEET TO A POINT WHICH IS 733.90 FEET EAST OF THE WEST LINE AND 1515.70 FEET SOUTH OF THE NORTH LINE OF SAID WEST HALF OF THE NORTHWEST QUARTER; THENCE NORTH 67° EAST 410.0 FEET, MORE OR LESS, TO THE CENTER OF THE DES PLAINES RIVER AS CENTER WAS LOCATED ON SEPTEMBER 20, 1964; THENCE SOUTHEASTERLY ALONG THE CENTER OF SAID DES PLAINES RIVER TO A POINT ON THE EAST LINE OF THE WEST HALF OF THE NORTHWEST QUARTER OF SAID SECTION 23, 999.0 FEET NORTH OF THE SOUTHEAST CORNER THEREOF; THENCE SOUTH ALONG SAID EAST LINE 715.20 FEET OF THE NORTH LINE OF THE SOUTH 4.30 CHAINS (283.80 FEET) THEREOF; THENCE WEST ALONG SAID NORTH LINE OF THE SOUTH 4.30 CHAINS, 1331.0 FEET TO THE WEST LINE OF THE WEST HALF OF THE NORTHWEST QUARTER OF SAID SECTION 23 AND THENCE NORTH ALONG THE WEST LINE OF THE

WEST HALF OF THE NORTHWEST QUARTER OF SAID SECTION 23, 1605.75 FEET TO THE PLACE OF BEGINNING EXCEPTING THEREFROM THAT PORTION LYING SOUTHWESTERLY OF A LINE 1450.00 FEET NORTHEASTERLY OF AND PARALLEL TO THE CENTER LINE OF MILWAUKEE AVENUE, ALL IN LAKE COUNTY, ILLINOIS.

AND

**PARCEL 6:** LOT "A" IN LINCOLNSHIRE UNIT NO. 6, BEING A SUBDIVISION IN SECTION 14 AND 23, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED JUNE 6, 1956 AD DOCUMENT 992285, IN BOOK 34 OF PLATS, PAGE 70, IN LAKE COUNTY, ILLINOIS.

AND

**PARCEL 7:** THAT PART OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS FOLLOWS: BEGINNING AT THE SOUTHWEST CORNER OF SAID NORTHWEST QUARTER; THENCE NORTHERLY ALONG THE WEST LINE OF SAID NORTHWEST QUARTER, A DISTANCE OF 283.8 FEET; THENCE EASTERLY PARALLEL WITH THE SOUTH LINE OF SAID NORTHWEST QUARTER, A DISTANCE OF 1331.25 FEET TO THE EAST LINE OF THE WEST HALF OF SAID NORTHWEST QUARTER, THENCE NORTHERLY ALONG THE EAST LINE OF THE WEST HALF OF SAID NORTHWEST QUARTER, A DISTANCE OF 541.2 FEET; THENCE EASTERLY PARALLEL WITH THE SOUTH LINE OF SAID NORTHWEST QUARTER; A DISTANCE OF 493.0 FEET TO THE CENTER LINE OF THE DES PLAINES RIVER, THENCE SOUTHERLY ALONG THE CENTER LINE OF THE DES PLAINES RIVER, A DISTANCE OF 929.0 FEET TO THE SOUTH LINE OF SAID NORTHWEST QUARTER; THENCE WESTERLY ALONG THE SOUTH LINE OF SAID NORTHWEST QUARTER, A DISTANCE OF 2205.0 FEET TO THE PLACE OF BEGINNING EXCEPTING THEREFROM THAT PORTION LYING IN THE WEST HALF OF SAID NORTHWEST QUARTER, ALL IN LAKE COUNTY, ILLINOIS.

AND

**PARCEL 8:** THAT PART OF THE SOUTH HALF OF SECTION 23, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS FOLLOWS, TO WIT: COMMENCING AT THE WEST QUARTER SECTION CORNER OF SAID SECTION 23; THENCE SOUTH ALONG THE WEST LINE OF SAID SECTION, 528.7 FEET TO THE CENTER OF THE NORTH MILL ROAD (SO CALLED); THENCE SOUTH 64 1/4 DEGREES EAST ALONG THE CENTER OF SAID ROAD, 2470.2 FEET TO AN IRON BAR, THENCE SOUTH 24 1/4 DEGREES WEST ALONG THE CENTER OF THE WEST RIVER ROAD (SO CALLED), 70 FEET; THENCE SOUTH 71 DEGREES EAST 604.5 FEET TO AN IRON BAR IN THE CENTER OF THE ROAD TO LAKE FOREST, WHICH SAID BAR IS IN A LINE WITH THE CENTER OF SAID NORTH MILL ROAD CONTINUED; THENCE NORTH 64 1/2 DEGREES EAST ALONG THE CENTER OF SAID LAKE FOREST ROAD, 440.9 FEET TO A STAKE; THENCE NORTH 48 1/2 DEGREES EAST ALONG THE CENTER OF SAID LAST NAMED ROAD, 483.6 FEET, THENCE NORTH 7 1/2 DEGREES EAST ALONG THE CENTER OF SAID LAST NAMED ROAD, 462.1 FEET, THENCE EAST ON A LINE PARALLEL WITH THE EAST AND WEST QUARTER SECTION LINE, 1725 FEET TO THE EAST LINE OF SAID SECTION; THENCE NORTH ALONG THE EAST LINE OF SAID SECTION, 861.9 FEET TO THE EAST QUARTER SECTION CORNER OF SAID SECTION;

THENCE WEST ALONG THE EAST AND WEST QUARTER SECTION LINE TO THE PLACE OF BEGINNING EXCEPTING THEREFROM THAT PART THEREOF, DESCRIBED AS FOLLOWS, TO-WIT: COMMENCING AT THE NORTHEAST CORNER OF THE SOUTHEAST QUARTER OF SAID SECTION 23, THENCE SOUTH ON THE EAST LINE OF SAID SECTION, 861.9 FEET; THENCE WEST ON A LINE PARALLEL WITH THE EAST AND WEST QUARTER SECTION LINE, 1729.2 FEET, THENCE SOUTH 6° 36' WEST 462.1 FEET; THENCE SOUTH 48 1/2° WEST 108 FEET, THENCE NORTH 4° 14' WEST 180.8 FEET TO THE CENTER OF THE DES PLAINES RIVER; THENCE NORTH 43° 14' WEST ALONG THE CENTER OF THE DES PLAINES RIVER, 54 FEET; THENCE NORTH 7° 5' WEST, 1179.5 FEET TO THE NORTH LINE OF THE SAID SOUTHEAST QUARTER OF SAID SECTION 23; THENCE EAST ALONG THE NORTH LINE OF THE SAID SOUTHEAST QUARTER OF SAID SECTION TO THE PLACE OF BEGINNING.

AND ALSO EXCEPTING THEREFROM THAT PART DESCRIBED AS FOLLOWS: BEGINNING AT A POINT ON THE CENTER LINE OF NORTH MILL ROAD (SO CALLED) 2393.02 FEET SOUTHEASTERLY, AS MEASURED ALONG SAID CENTER LINE OF THE INTERSECTION OF SAID CENTER LINE AND THE WEST LINE OF SAID SECTION 23; THENCE NORTHWESTERLY ALONG SAID CENTER LINE, A DISTANCE OF 1427.0 FEET; THENCE NORTHEASTERLY AT RIGHT ANGLES TO SAID CENTER LINE, A DISTANCE OF 550.0 FEET; THENCE SOUTHEASTERLY ALONG A LINE FORMING AN ANGLE OF 83 DEGREES TO THE RIGHT WITH THE LAST DESCRIBED COURSE EXTENDED, A DISTANCE OF 767.0 FEET, THENCE SOUTHEASTERLY ALONG A LINE FORMING AN ANGLE OF 40 DEGREES TO THE RIGHT WITH THE LAST DESCRIBED COURSE EXTENDED, A DISTANCE OF 190.0 FEET; THENCE NORTHEASTERLY ALONG A LINE FORMING AN ANGLE OF 86 DEGREES TO THE LEFT WITH THE LAST DESCRIBED COURSE EXTENDED, A DISTANCE OF 675.25 FEET, THENCE SOUTHWESTERLY 963.40 FEET, MORE OR LESS, TO THE PLACE OF BEGINNING.

ALSO EXCEPTING THEREFROM THAT PART OF THE SOUTH HALF OF SECTION 23, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING EASTERLY OF THE CENTER LINE OF THE DES PLAINES RIVER.

ALSO EXCEPTING THEREFROM THAT PART OF SAID SOUTH HALF DESCRIBED AS FOLLOWS: COMMENCING AT A POINT ON THE CENTER LINE OF NORTH MILL ROAD, 2393.02 FEET SOUTHEASTERLY, AS MEASURED ALONG SAID CENTER LINE OF THE INTERSECTION OF SAID CENTER LINE AND THE WEST LINE OF SAID SECTION 23; THENCE NORTHWESTERLY ALONG SAID CENTER LINE A DISTANCE OF 1427.0 FEET TO THE POINT OF BEGINNING; THENCE NORTHEASTERLY AT RIGHT ANGLES TO SAID CENTER LINE A DISTANCE OF 550.0 FEET; THENCE SOUTHEASTERLY ALONG A LINE FORMING AN ANGLE OF 88 DEGREES TO THE RIGHT WITH THE LAST DESCRIBED COURSE EXTENDED, A DISTANCE OF 767.0 FEET; THENCE SOUTHEASTERLY ALONG A LINE FORMING AN ANGLE OF 40 DEGREES TO THE RIGHT WITH THE LAST DESCRIBED COURSE EXTENDED, A DISTANCE OF 190.0 FEET; THENCE NORTHEASTERLY ALONG A LINE FORMING AN ANGLE OF 86 DEGREES TO THE LEFT WITH THE LAST DESCRIBED COURSE EXTENDED, A DISTANCE OF 591.3 FEET; THENCE NORTHWESTERLY A DISTANCE OF 1323.1 FEET, MORE OR LESS, TO A POINT ON THE NORTH LINE OF THE SOUTH HALF OF SAID SECTION 23; THENCE WESTERLY ALONG SAID NORTH LINE, A DISTANCE OF 1430.4 FEET TO THE NORTHWEST CORNER OF SAID SOUTH HALF; THENCE SOUTHERLY ALONG THE WEST LINE OF SAID SOUTH HALF 528.7 FEET TO ITS INTERSECTION WITH THE CENTER LINE OF THE NORTH MILL ROAD, THENCE SOUTHEASTERLY ALONG SAID CENTER LINE, A

DISTANCE OF 966.02 FEET, MORE OR LESS, TO THE POINT OF BEGINNING, ALL IN LAKE COUNTY, ILLINOIS.

AND

**PARCEL 9:** THAT PART OF THE NORTHEAST QUARTER OF SECTION 22, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS FOLLOWS: BEGINNING AT THE NORTHEAST CORNER OF SAID SECTION 22; THENCE SOUTH ALONG THE EAST LINE OF SAID SECTION 22, A DISTANCE OF 764.05 FEET TO A POINT, SAID POINT FURTHER DESCRIBED AS NORTH 89° 25' 30" EAST A DISTANCE OF 1313.40 FEET FROM A POINT ON THE CENTER LINE OF MILWAUKEE AVENUE, WHICH IS SOUTHEASTERLY 813.60 FEET ALONG SAID CENTER LINE FROM THE INTERSECTION OF SAID CENTER LINE AND THE NORTH LINE OF SAID SECTION 22; THENCE SOUTH 89° 25' 30" WEST, A DISTANCE OF 101.8 FEET; THENCE NORTH 5° 5' 20" WEST A DISTANCE OF 262.00 FEET, MORE OR LESS, TO A POINT ON THE CENTER LINE OF INDIAN CREEK AS LOCATED ON AUGUST 26, 1948, SAID POINT BEING FURTHER DESCRIBED AS BEARING NORTH 51° 41' 30" EAST ALONG SAID INDIAN CREEK CENTER LINE, A DISTANCE OF 159.61 FEET FROM A POINT ON THE EAST LINE OF SAID SECTION 22, 603.05 FEET SOUTH OF THE NORTHEAST CORNER OF SAID SECTION 22; THENCE NORTHEASTERLY A DISTANCE OF 519.38 FEET, MORE OR LESS, TO THE POINT OF BEGINNING IN LAKE COUNTY, ILLINOIS.

AND

**PARCEL 10:** THE EAST 466.00 FEET OF THE SOUTH 283.80 FEET OF THE WEST HALF OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 43 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN IN LAKE COUNTY, ILLINOIS.

AND

**PARCEL 11:** THAT PART OF THE WEST HALF OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS FOLLOWS: BEGINNING AT THE INTERSECTION OF THE NORTH LINE OF THE SOUTH 4.30 CHAINS (283.8 FEET) AND A LINE 1450.0 FEET NORTHEASTERLY OF AN PARALLEL TO THE CENTER LINE OF MILWAUKEE AVENUE; THENCE NORTHWESTERLY ALONG SAID PARALLEL LINE 454.4 FEET; THENCE SOUTHWESTERLY 439.6 FEET TO A POINT ON THE NORTH LINE OF THE SOUTH 4.30 CHAINS (283.8 FEET) OF SAID WEST HALF, SAID POINT BEING 448.8 FEET EAST OF THE WEST LINE OF SAID WEST HALF AS MEASURED ALONG SAID NORTH LINE; THENCE EASTERLY ALONG SAID NORTH LINE 171.0 FEET, MORE OR LESS, TO THE POINT OF BEGINNING , IN LAKE COUNTY, ILLINOIS.

AND

**PARCEL 12:** THAT PART OF THE SOUTH HALF OF SECTION 23, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, BEING A STRIP OF LAND 25.0 FEET IN WIDTH MORE PARTICULARLY DESCRIBED AS FOLLOWS: COMMENDING AT A POINT ON THE CENTER LINE OF NORTH MILL ROAD, 2393.02 FEET SOUTHEASTERLY, AS MEASURED ALONG SAID CENTER LINE OF THE INTERSECTION OF SAID CENTER LINE AND THE WEST LINE OF SAID SECTION 23; THENCE NORTHWESTERLY ALONG SAID

CENTER LINE A DISTANCE OF 1427.0 FEET; THENCE NORTHEASTERLY AT RIGHT ANGLES TO SAID CENTER LINE A DISTANCE OF 550.0 FEET; THENCE SOUTHEASTERLY ALONG A LINE FORMING AN ANGLE OF 83 DEGREES TO THE RIGHT WITH THE LAST DESCRIBED COURSE EXTENDED, A DISTANCE OF 767.0 FEET; THENCE SOUTHEASTERLY ALONG A LINE FORMING AN ANGLE OF 40 DEGREES TO THE RIGHT WITH THE LAST DESCRIBED COURSE EXTENDED, A DISTANCE OF 190.0 FEET; THENCE NORTHEASTERLY ALONG A LINE FORMING AN ANGLE OF 86 DEGREES TO THE LEFT WITH THE LAST DESCRIBED COURSE EXTENDED, A DISTANCE OF 561.49 FEET TO THE POINT OF BEGINNING; THENCE CONTINUING NORTHEASTERLY ALONG THE LAST DESCRIBED COURSE 29.81 FEET; THENCE NORTHWESTERLY A DISTANCE OF 1232.1 FEET, MORE OR LESS, TO A POINT ON THE NORTH LINE OF THE SOUTH HALF OF SAID SECTION 23; THENCE WESTERLY ALONG SAID NORTH LINE TO A DISTANCE OF 1016.0 FEET, MORE OR LESS, TO A POINT ON SAID NORTH LINE 414.4 FEET EAST OF THE NORTHWEST CORNER OF SAID SOUTH HALF, AS MEASURED ALONG SAID NORTH LINE, THENCE SOUTHERLY AT RIGHT ANGLES TO THE LAST DESCRIBED COURSE 25.0 FEET; THENCE EASTERLY PARALLEL TO THE NORTH LINE OF SAID SOUTH HALF 1008.4 FEET; THENCE SOUTHEASTERLY 1208.3 FEET TO THE POINT OF BEGINNING, IN LAKE COUNTY, ILLINOIS.

AND

**PARCEL 13:** THE SOUTH 4.30 CHAINS (283.8 FEET) OF THE WEST HALF OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, EXCEPTING THEREFROM THE EAST 466.0 FEET AND THAT PORTION LYING WEST OF THE FOLLOWING DESCRIBED LINE: BEGINNING AT A POINT ON THE NORTH LINE OF THE SOUTH 4.30 CHAINS (263.8 FEET) OF SAID WEST HALF, 44.8 FEET EAST OF THE WEST LINE OF SAID WEST HALF AS MEASURED ALONG SAID NORTH LINE, THENCE SOUTHWESTERLY 285.4 FEET MORE OR LESS TO A POINT ON THE SOUTH LINE OF THE SOUTH 4.30 CHAINS (263.8 FEET) OF SAID WEST HALF, 414.4 FEET EAST OF THE WEST LINE OF SAID WEST HALF, AS MEASURED ALONG SAID SOUTH LINE, IN LAKE COUNTY, ILLINOIS.

**EXHIBIT B**

**GROUND AND DIRECTIONAL SIGN PLANS**

**REQUEST FOR BOARD ACTION  
Committee of the Whole  
April 8, 2019**

**Subject:** Draft Plan for the Prairie View Metra Station Area Plan and the Existing Boundary and Planning Agreement between the Village of Lincolnshire and Village of Buffalo Grove

**Action Requested:** Discussion and Direction Regarding the Village of Buffalo Grove's Draft Plan for the Prairie View Metra Station Area Plan as it Relates to the Existing Boundary and Planning Agreement between the Village of Lincolnshire and Village of Buffalo Grove

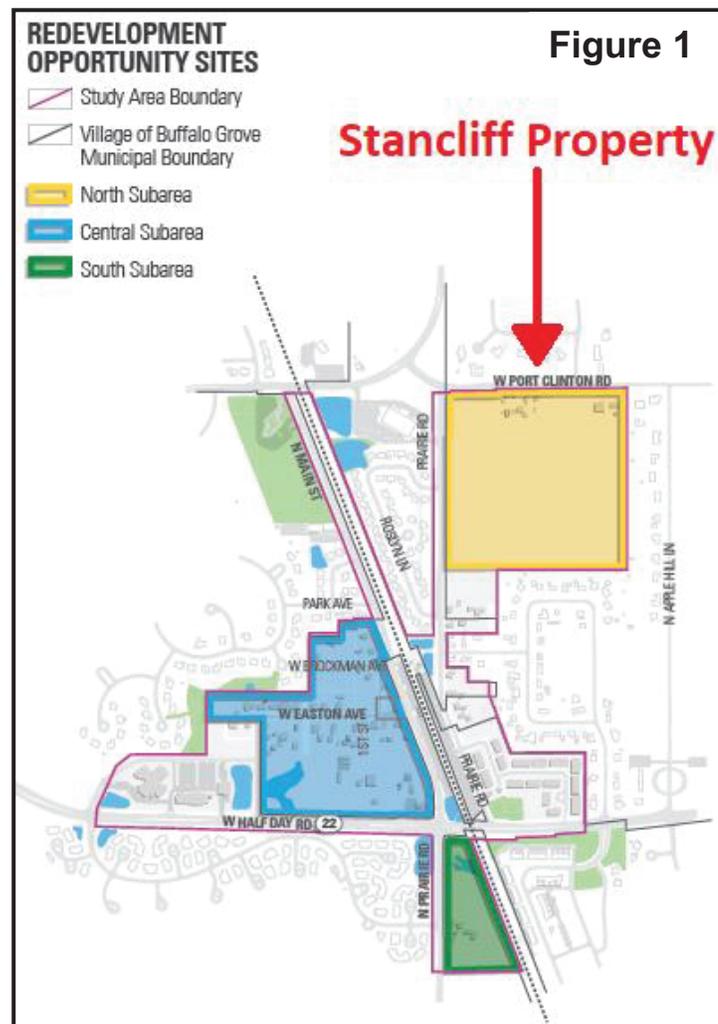
**Originated By/Contact:** Ben Gilbertson, Assistant Village Manager/Community & Economic Development Director

**Referred To:** Village Board

**Background:**

In April 2018, the Village of Buffalo Grove began work on an update to its comprehensive plan for the Prairie View Metra Station Area (Prairie View Plan). The project area encompasses approximately 128 acres, portions of which are in Buffalo Grove's jurisdiction as well as unincorporated Vernon Township. The plan is scheduled to be presented and approved by the Buffalo Grove Village Board on April 15<sup>th</sup>. Prior to the plan's approval, Buffalo Grove is soliciting public comment.

In 2005, the "Boundary and Planning Agreement between the Village of Lincolnshire and Village of Buffalo Grove" (Boundary Agreement) was approved by both Village Boards. This intergovernmental agreement was intended to define logical municipal boundaries and guide development of undeveloped and underdeveloped property within the combined planning jurisdictions of both Villages consistent with their respective Comprehensive Plans. The Prairie View Plan contemplates development on the 40-acre, unincorporated Stancliff Property



(see Figure 1). The Stancliff Property is also subject to land use restrictions delineated in the Boundary Agreement. Based upon the Boundary Agreement, the Stancliff Property is eventually to be annexed by Buffalo Grove at a future date.

Given the interplay between the Prairie View Plan and Boundary Agreement, staff is requesting feedback from the Village Board regarding the Prairie View Plan recommendations for the Stancliff Property. In turn, this may impact any potential changes to the Boundary Agreement as well as the Prairie View Plan itself.

**Summary – Draft Plan for the Prairie View Metra Station Area Plan:**

The Plan’s main objective is to create a “cohesive vision” across three Buffalo Grove planning subareas in an effort to leverage the Prairie View Metra Station as an amenity for future residents.

- **North Subarea:** The North Subarea consists of the 40-acre Stancliff Property. The Plan recommends a mix of compact single-family detached and attached housing, as well as incorporating sustainable strategies. This is the only subarea governed by the Boundary Agreement.
- **Central Subarea:** The Central Subarea contains unincorporated properties west of the Prairie View Metra Station and north of Half Day Road. This subarea calls for a variety of housing choices and commercial activities.
- **South Subarea:** The South Subarea proposes a corner mixed-use building with gateway signage and plaza at the southwest of Half Day Road and North Prairie Road. Stacked townhomes and stormwater detention areas are located throughout this subarea.

Three types of housing are recommended across the three subareas, totaling 658 units. All of the proposed housing units would be within the Lincolnshire-Prairie View School District 103 (D103) and the Adlai E. Stevenson High School District 125 (D125) school district boundaries (see Documents 4 and 5, respectively). Of the 658 units, the Stancliff Property calls for 131 units (approximately 20% of all units). See Table 1 below for additional details.

**Table 1: Prairie View Metra Plan – Housing Type and Unit Recommendations by Subarea**

	North Subarea Stancliff Property	Central Subarea Prairie View Station	South Subarea (Half Day Rd. / Prairie Rd. Corner)	TOTAL
Townhome/ Rowhome	58	87	64	209
Single-Family Detached	73	15	39	127
Multi-Family	-	332	-	322
<b>TOTAL</b>	<b>131</b> (3.3 units/acre)	<b>434</b> (4.6 units/acre)	<b>103</b> (9.5 units/acre)	<b>658</b> (4.6 units/acre)

On several occasions within the proposed Prairie View Plan, the Village of Buffalo Grove acknowledges the Boundary Agreement as well as the need for Buffalo Grove to revisit it in concert with the Village of Lincolnshire to “reflect the vision set forth in the Plan.” Revisiting the Boundary Agreement is necessary because the Stancliff Property units/acre recommendation (3.3) exceeds the agreed-upon units/acre in the Boundary Agreement (2.0). At 40 acres, this is a difference of 51 units. The Plan is silent with respect to projected household size and student generation. Nonetheless, public engagement sessions found some concerns with the impact of the development on Stevenson High School, as well as some desire to keep single-family housing on the Stancliff Property.

**Summary – Boundary Agreement and Amendment Process:**

The Boundary Agreement was approved by both Villages in 2005. The Boundary Agreement identifies the most logical municipal boundaries for Lincolnshire and Buffalo Grove; clarifies the appropriate land uses; and designates the desired densities for a variety of unincorporated areas within the municipalities' overlapping planning jurisdiction areas. The Boundary Agreement also ensures that local residents and officials – not real estate developers – determine the most desirable development scenarios for undeveloped and underdeveloped properties within the Boundary Agreement map area. Furthermore, the Boundary Agreement marked the successful conclusion of a long-term goal of the Mayor and Board of Trustees to secure the annexation of desirable properties and ensure Lincolnshire's vision for appropriate growth and development is realized. Since adoption of the Boundary Agreement, several properties have been annexed by Lincolnshire that are subject to the Boundary Agreement, including the 34-acre Boznos/Par King property, the 20-acre Lincolnshire Trails property, and the four-acre former Cubby Bear (now Loft 21) property.

It is important to reiterate that the Prairie View Plan is an update to Buffalo Grove's Comprehensive Plan. Comprehensive Plans for all communities are not "legally binding", but rather offer recommendations for future land use to help guide cohesive development. As such, were the Buffalo Grove Village Board to adopt the Prairie View Plan with the 3.3 units/acre recommendation for the Stancliff Property, this would not supersede the 2.0 units/acre recommendation in the Boundary Agreement. This process has been confirmed by Village Attorney Simon.

For the density to be permitted to increase on the Stancliff Property, the Boundary Agreement must be amended and approved by both Village Boards. Neither Village may directly or indirectly seek any modification of the Boundary Agreement through court action. The Boundary Agreement stands as approved until amended or changed by the mutual agreement of both Village Boards. Otherwise, the Boundary Agreement is in full force and effect through May 23, 2025. Either Village may terminate the agreement by serving written notice not less than 180 days prior to the expiration of the agreement. If no written notice is delivered, the Boundary Agreement automatically renews for successive five-year terms.

**Recommendation:**

Staff recommends the Lincolnshire Village Board leverage the Prairie View Plan recommendations to engage the Village of Buffalo Grove to holistically review the Boundary Agreement in its entirety. Although the Lincolnshire Village Board has been prudent with respect to the impact of development on D103 and D125 student populations, the 51-unit difference between the Boundary Agreement and Prairie View Plan recommendations should not deter the Village Board from identifying other areas of the Boundary Agreement that could better serve the broader interests and initiatives of Lincolnshire.

**Reports and Documents Attached:**

- Document 1: Village of Buffalo Grove's Draft Plan for the Prairie View Metra Station Area Plan
- Document 2: Executed 2005 Boundary and Planning Agreement between the Village of Lincolnshire and Village of Buffalo Grove
- Document 3: Boundary Agreement Map, prepared by MGP
- Document 4: Adlai E. Stevenson School District 125 – Boundary Map
- Document 5: Lincolnshire-Prairie View School District 103 – Boundary Map

<b>Meeting History</b>	
Committee of the Whole	April 8, 2019



# Draft Plan

Village of Buffalo Grove  
Prairie View Metra Station Area Plan

February 2019



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# Acknowledgments

## Steering Committee

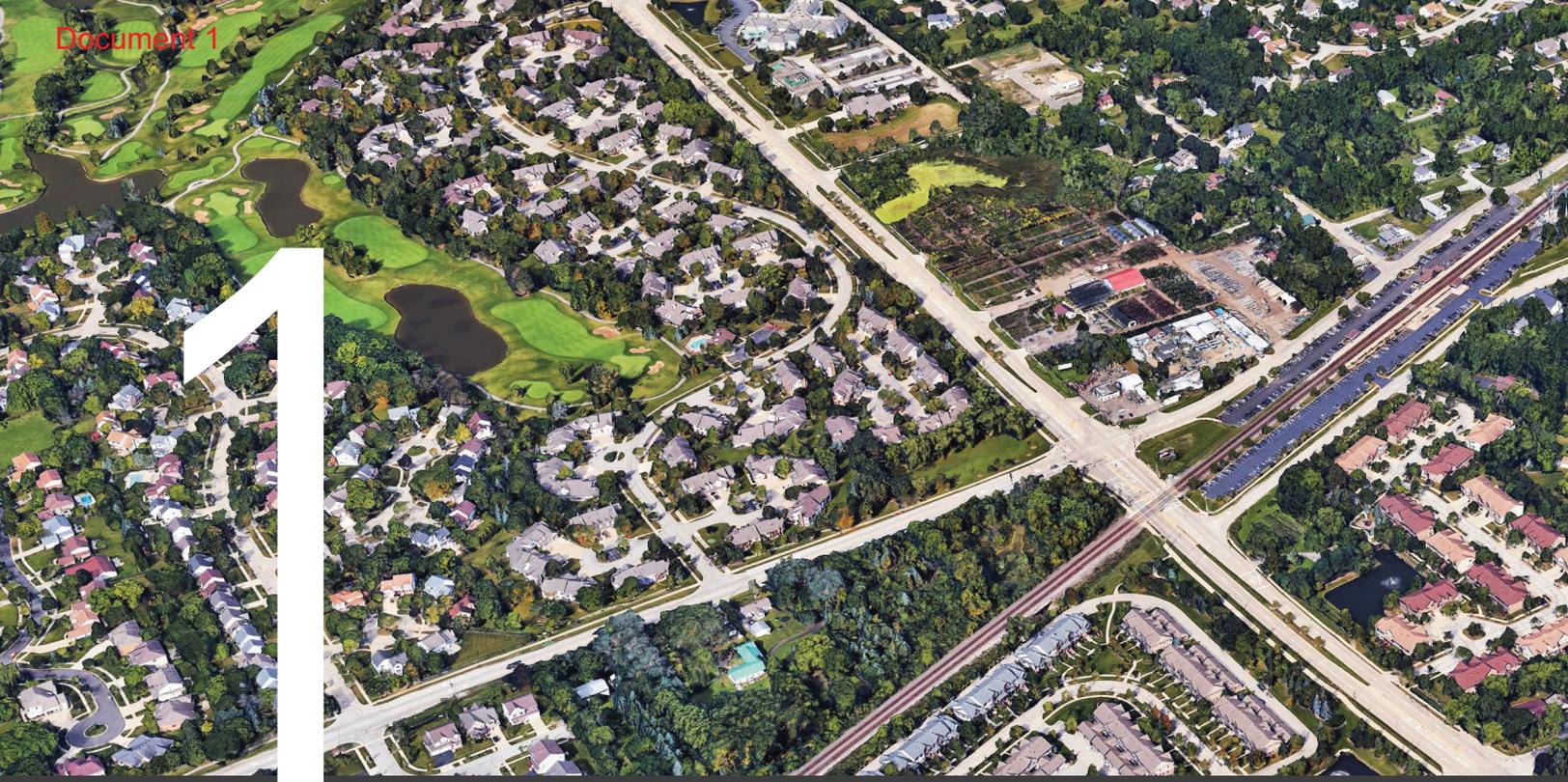
Eric Smith, Trustee, Village of Buffalo Grove Board  
Daniel Didech/Jonathan Altenberg, Supervisor, Vernon Township  
Patrick Palbicke, Assistant Superintendent for Business at  
Lincolnshire Prairie View District #103  
Allison Buchwach, Senior Planner, Metra  
Kyle Olson, Resident, Village of Buffalo Grove  
Frank Cesario, Chair, Village of Buffalo Grove  
Planning and Zoning Commission  
Bill Hoffman, Resident and business owner,  
Village of Buffalo Grove  
Jim Kedroski, Local builder and resident,  
Village of Buffalo Grove

## Village of Buffalo Grove

Dane Bragg, Village Manager  
Jenny Maltas, Deputy Village Manager  
Chris Stilling, Community Development Director  
Nicole Woods, Principal Planner  
Rati Akash, Village Planner  
Darren Monico, Village Engineer

## Consultant Team

RATIO Kimley»Horn

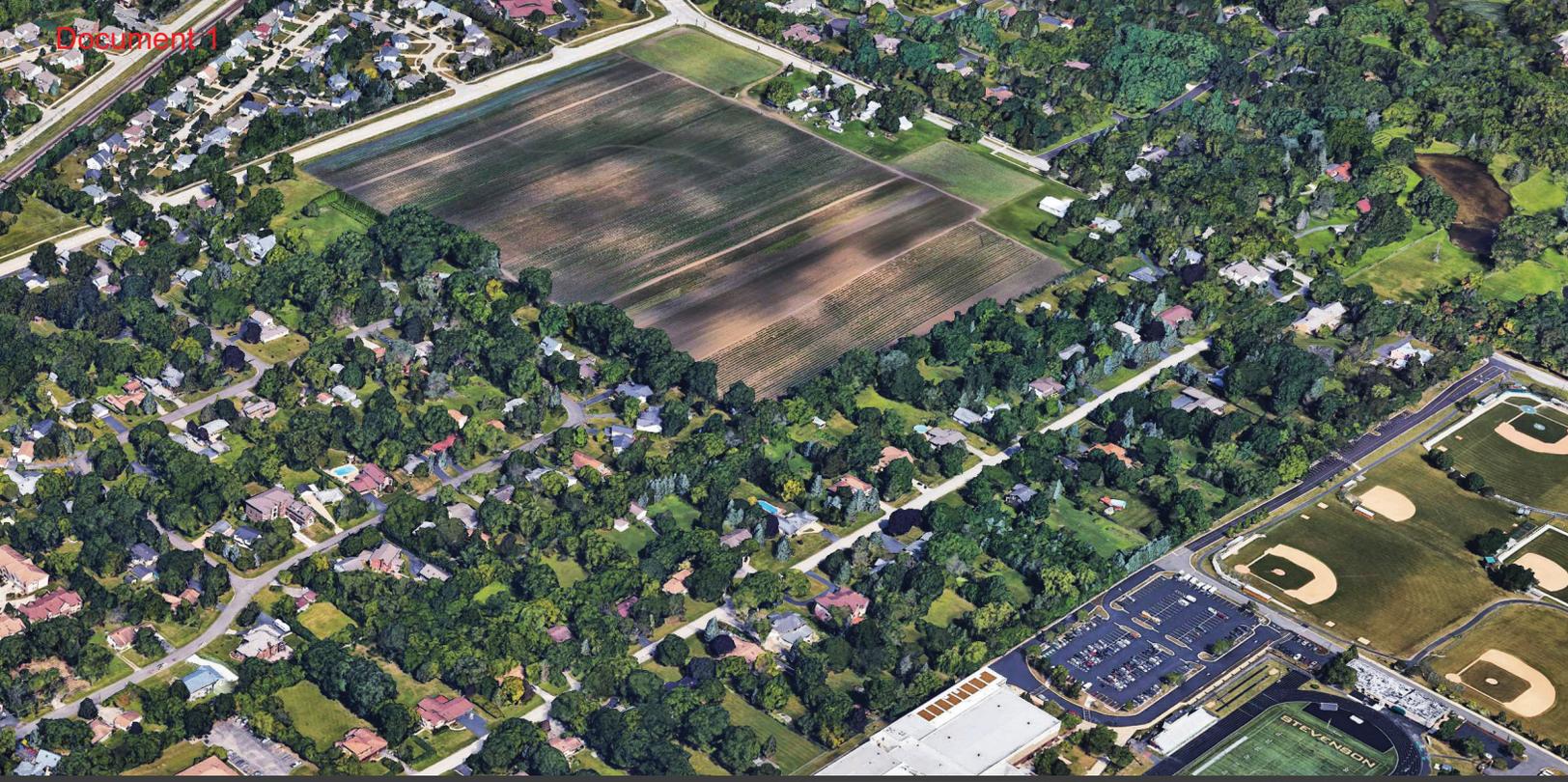


# Introduction

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The Prairie View Metra Station Area is an asset to the Village of Buffalo Grove. A cohesive vision for this area can unlock its potential by providing a framework for future development. The Station Area is primarily made up of residential, commercial, retail, and institutional uses. There are underutilized parcels and unincorporated land that have both economic development and quality of life potential. A diversity of tenancies ranging from scrap metal to landscape supplies, from healthcare to residential, from school and church facilities to an active retail center. The immediate station area is surrounded by residential development made up of primarily single-family homes with adjacent local small businesses. An historic mixed-use hamlet adds character and context to the study area. Improving connectivity between the Prairie View Metra Station, businesses along Half Day Road, adjacent residential properties and commercial businesses that make up the Station Area can attract more investment.

The planning process focused on making an authentic destination in Buffalo Grove by defining the critical elements that make up the Station Area and then drawing upon those elements to make a cohesive and legible environment that is attractive to investors and developers. By leveraging the strengths of the Station Area – proximity to regional transportation and roadway systems, connectivity to regional trail systems, access to natural resource amenities such as the nearby Forest Preserve District lands, established neighborhoods with historical characters, and a variety of retail, civic, commercial, religious and institutional businesses – the Prairie View Metra Station Area can fulfill its potential.



Local and regional plans served as a framework and guidance to the development of the Prairie View Station Area Plan. These plans include:

- Buffalo Grove Lake Cook Corridor Market Study and Plan (2018)
- Buffalo Grove Economic Development Strategic Plan (2015)
- Buffalo Grove Annexation Strategy and Plan (2016)
- Buffalo Grove 2018-2023 Strategic Plan (2018)
- Buffalo Grove Homes for a Changing Region (2013)
- Buffalo Grove Comprehensive Plan (2009)
- Buffalo Grove Transit Station Areas Plan(2007)
- Lake County Strategic Plan (2017)
- Lake County Comprehensive Economic Development Strategy (2013)
- CMAP Go To 2040 (2010)/On To 2050 (2018)

The vision and planning framework for the Prairie View Station Area integrate future land use, economic development and multimodal transportation planning strategies. This plan serves as the Village’s official policy and development guide for the area and will assist the Village in making informed decisions that best serve residents needs in the community over the next 15 to 20 years.

The Plan was created to achieve the following objectives:

- **Objective 1:** Provide a Framework for the Community’s Aspirations
- **Objective 2:** Integrate Previous Plans to Strengthen Community Identity
- **Objective 3:** Support Activation of the Station Area
- **Objective 4:** Build Consensus through Outreach with Key Stakeholders

## HOW TO USE THIS PLAN

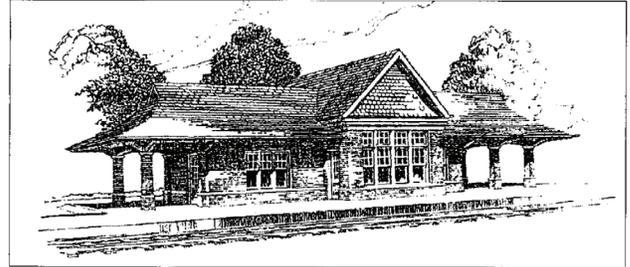
The Prairie View Metra Station Area Plan establishes policies that will guide the physical development of the Prairie View Metra Station Area. These policies are vital in supporting the Village’s staff, elected and appointed officials, and the public in their ability to review and evaluate future projects, development, and land annexation. The Plan can serve as a marketing tool to highlight the Village and the Prairie View Metra Station Area’s unique assets and economic advantages within the Chicagoland region. It can also be used to support applications for grants and to demonstrate that future projects align with the Village’s larger, holistic strategy and developmental vision rather than stand-alone efforts.

# HISTORY OF PRAIRIE VIEW

The original homes of Prairie View were mostly established in a piecemeal fashion along Easton Avenue during the late 19th century and early 1980s. At that time, Prairie View had an old train station that was named after the "Soo Line," a passenger rail line that served Prairie View residents but was discontinued in the early 1960s. This structure was torn down and was replaced with the present Prairie View Metra Station in 1996. At that time, the Prairie View Metra Station did not look "new," as its architecture was intended to have the same architectural style as the residences in the Prairie View community.

Prairie View was also home to the Vernon American Legion Post 1247 (presently at 16595 Easton Avenue). However, aging veterans and declining membership prompted the relocation of this service. Instead of leaving this historic building to neglect, local architects rehabilitated the building into a modern office building.

Over time, the area in and around Prairie View was slowly annexed by adjacent municipalities, including the Village of Buffalo Grove and Lincolnshire. This trend has resulted in northern Prairie View being landlocked by incorporated lands. As a result, newer and more contemporary residential developments are being developed. There is also a boundary agreement between the Village of Buffalo Grove and Lincolnshire that affects 40 acres of land near Prairie Road.



The new station, to be completed before the August 19 start of commuter service, doesn't look new at all, which is the idea.

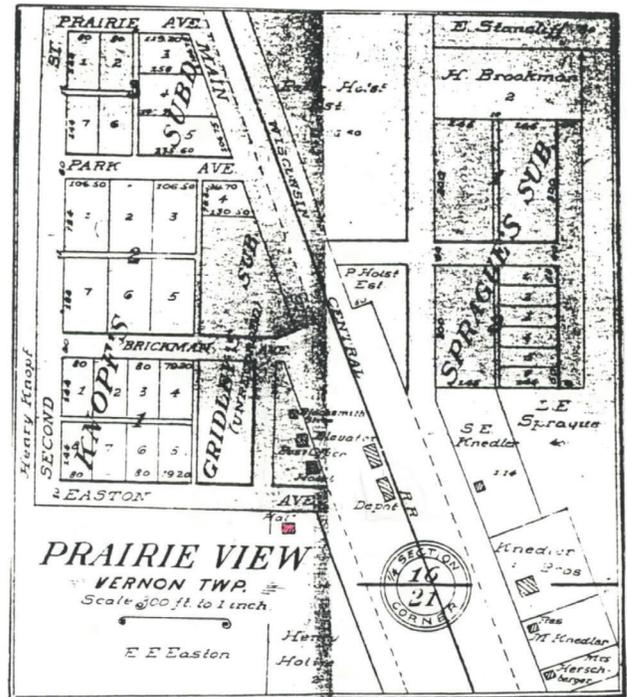


The old station, on the same site, was torn down several years ago after Soo Line passenger service was discontinued in the early 1960s.

Past news article showing the current and previous train station in Prairie View.



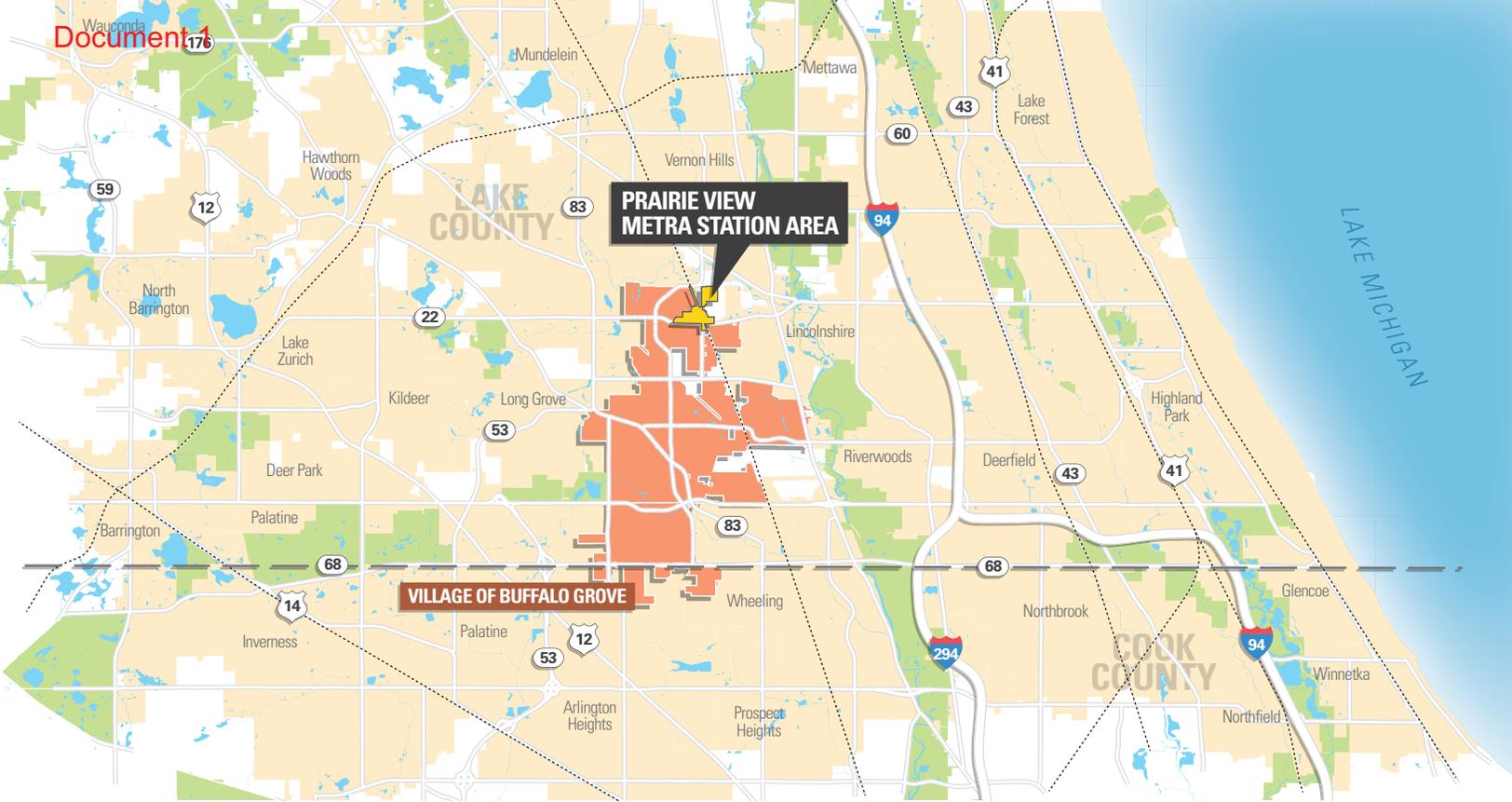
Past news article showing a rendering of the rehabilitated Vernon Legion Post 1247 building, which currently houses office and wellness uses.



A map showing Prairie View's parcels and development in the 19th Century.



Aerial view of Prairie View area in 1994. | Credit: Google Earth



## STUDY AREA & LOCAL CONTEXT

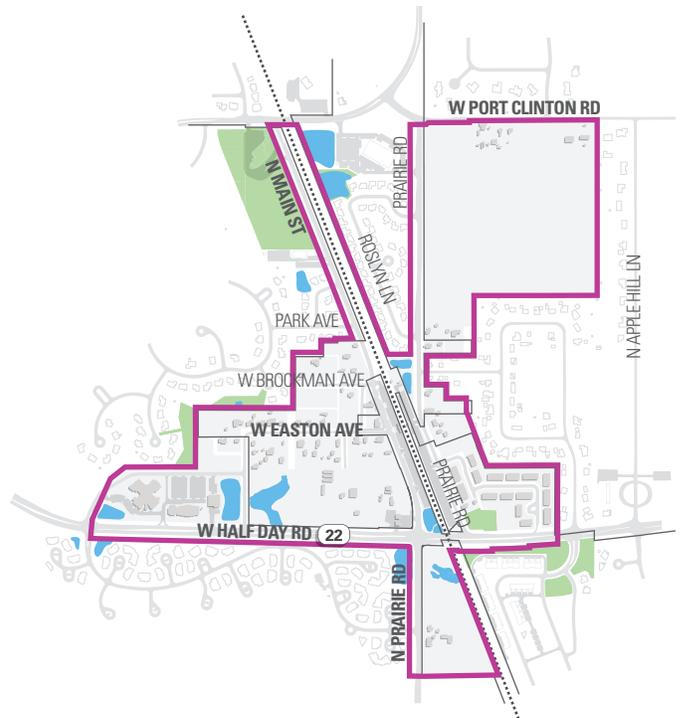
The Prairie View Metra Station Area roughly covers 128 acres and is located in the northern part of Buffalo Grove, centered around the Prairie View Metra Station. This encompasses land on both east and west sides of Main Street that includes Noah’s Landing, Prairie Landing, Easton Station residential developments; Woodlands Shopping Center; Sunrise Retirement facility; and other businesses, and homes in the unincorporated Vernon Township. The Stancliff property, a vacant 40 acre parcel just southeast of Port Clinton and Prairie Roads, marks the northern edges of the study area while the vacant triangle parcel southeast of Half Day and Prairie Roads denotes the southern edge.

The Prairie View Metra Station area is one of two Metra stations that is located within the Village of Buffalo Grove. It has access to Half Day Road, which is a critical east-west roadway that connects residents to the regional transportation network. Buffalo Grove Road is located one-half mile west of the station, which provides residents access Interstate 94, Milwaukee Avenue (US Highway 45), and County Highway 83.

The Station Area is located near prominent destinations and community facilities. These include the Arboretum Club, the Adlai E. Stevenson High School to the east, and Sullivan Woods and Peterson Woods to the north.

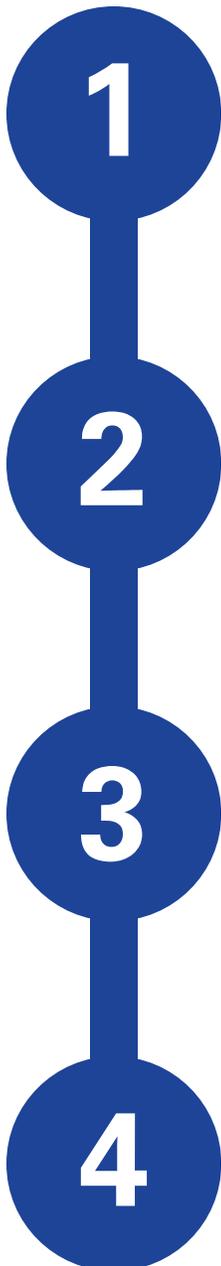
### STUDY AREA

-  Study Area Boundary
-  Village of Buffalo Grove Municipal Boundary



## PROJECT SCHEDULE

The planning process had four stages that created a station area plan for Prairie View.



APR - MAY 2018

### **Project Initiation**

At the onset of the project, logistics regarding schedule, milestones, data source availability, and critical issues affecting the project were established. Additionally, the consultant team, along with the Village Staff, hosted the first public workshop and an online survey to gain input from residents and stakeholders regarding the Prairie View Metra Station Area Plan. Interviews were also conducted with key stakeholders.

MAY - JUL 2018

### **Data Collection, Land Use & Market Assessment**

During this stage, the consultant team assessed and evaluated physical conditions of the Prairie View Metra Station Area context. Land use and zoning, current and ongoing planning activities, open space, key urban design features, utilities, strategic properties, area assets, and environmental considerations as well as vehicular, bicycle, and pedestrian systems, and traffic patterns were investigated.

AUG- FEB 2019

### **Draft Station Area Plan**

In this phase, specific recommendations and strategies for the Prairie View Metra Station are developed that incorporate best practices for transit supportive development including mixed use, increased densities, and balanced parking requirements to create an economical and vibrant environment. The Draft Plan “tests” the proposed location of recommended development sites identified in the market assessment with the vision and character goals of Buffalo Grove.

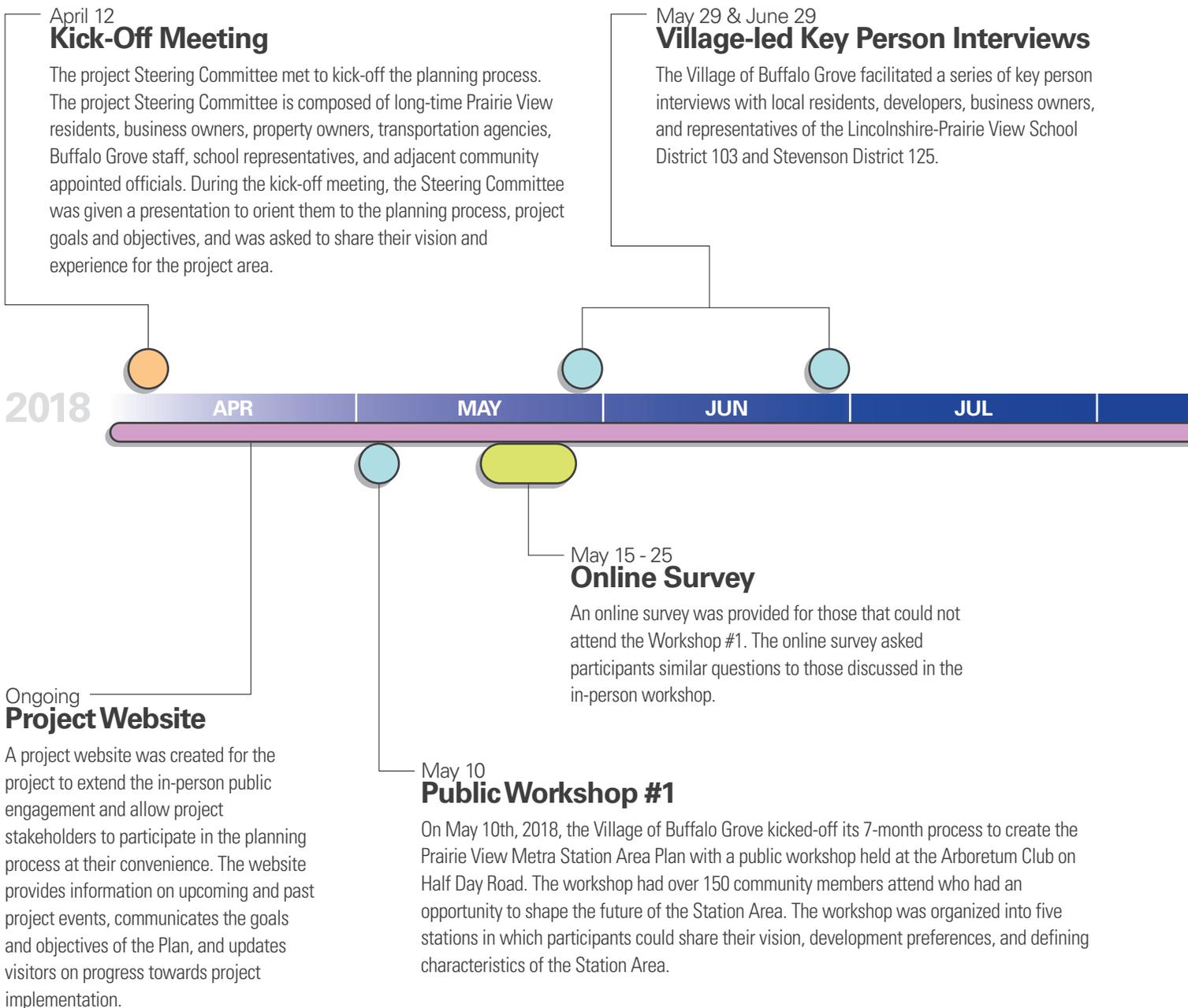
FEB- APRIL 2019

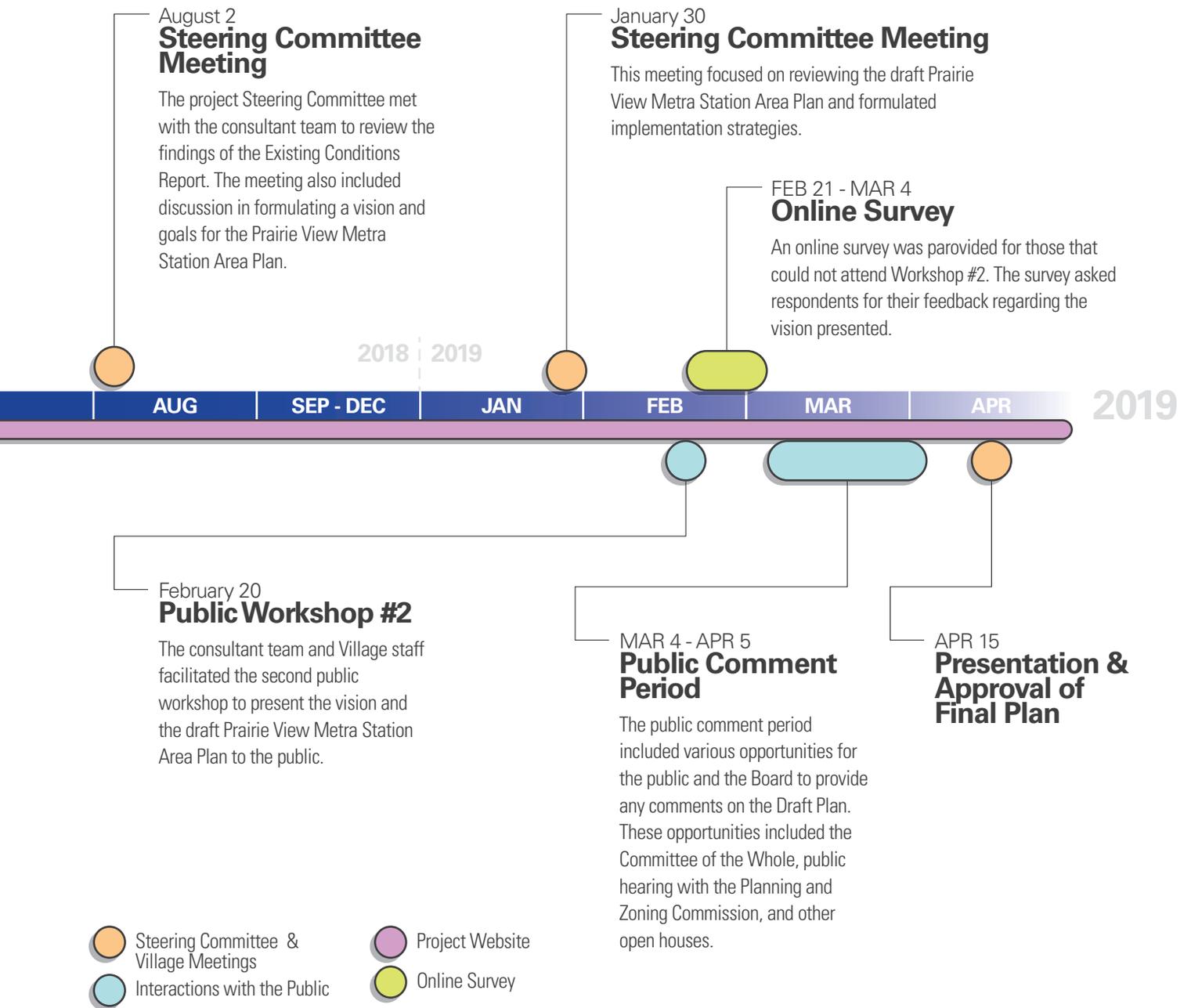
### **Final Station Area Plan**

The Final Plan illustrates the refinement of the project vision, development goals, transportation, market analysis, and implementation. The information gathered in Stages 1-3 is compiled and used in the Final Prairie View Station Area Plan.

# PUBLIC ENGAGEMENT

To create the Prairie View Station Area Plan, the voices of the community needed to be heard. The public engagement process provided a variety of methods to solicit input into the planning process by understanding the public's needs, preferences, priorities, and vision for the Prairie View Metra Station Area.







## SUMMARY OF EXISTING CONDITIONS

### Land Use Patterns

The Prairie View Metra Station can be characterized by three distinct zones. Land uses within these areas vary slightly, however, there are notable spatial patterns.

#### Zone 1

Zone 1 is the area fronting W Half Day Road and west of N Main Street, in addition to the site at the southeast corner of W Half Day Road and N Prairie Road. The portion west of W Easton Ave consists of: a fire station, a medical office building, and a senior-living facility are within the Village limits. The rest of Zone 1 is within unincorporated Lake County. This area includes: a plant nursery, a scrap metal business, and single-family detached housing, which is in the southeastern portion of Zone 1. This area is currently unincorporated and is not connected to the Village's utility infrastructure and public services.

#### Zone 2

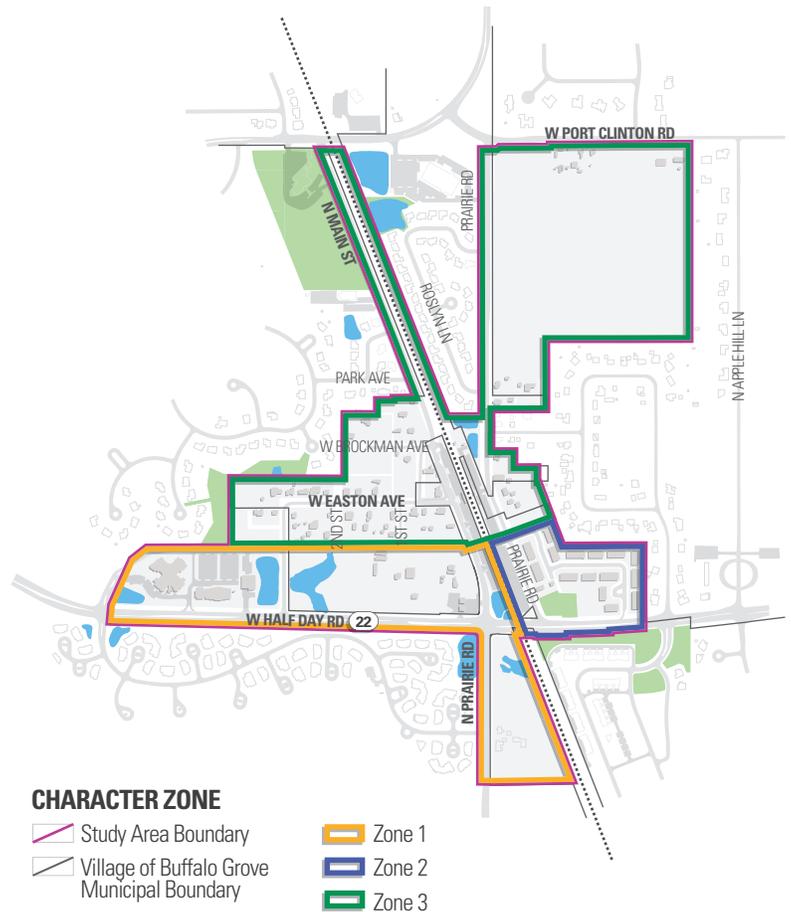
The second zone primarily consists of residential development east of Prairie Road and north of W Half Day Road. There is a residential subdivision that consists of townhomes. Prairie Landing Townhomes, which consists of the lots at the northeast corner of W Half Day Road and Prairie Road, is to be constructed as townhomes.



### Zone 3

The third zone consists of parcels on both sides of the Metra rail tracks and north of Zone 1 and 2. The west side of the tracks contains small-scale businesses along N Main Street, in addition to pockets of vacant lots. These businesses provide services and limited commercial offerings. Further west are mostly single-family detached homes within unincorporated Lake County. The Easton Station Townhomes, within the Village limits, are recent examples of more modern housing typologies coming into the community. They offer different housing types and amenities and are geared towards the changing Village demographic, in contrast to older and more suburban-style housing subdivisions, such as Noah’s Landing.

To the east of the rail tracks, the land use is mostly comprised of single-family detached housing. Some of these parcels are within unincorporated Lake County. To the north, the 40-acre Stancliff Property remains mostly agricultural. This property is under a boundary agreement between the Village of Buffalo Grove and the Village of Lincolnshire.

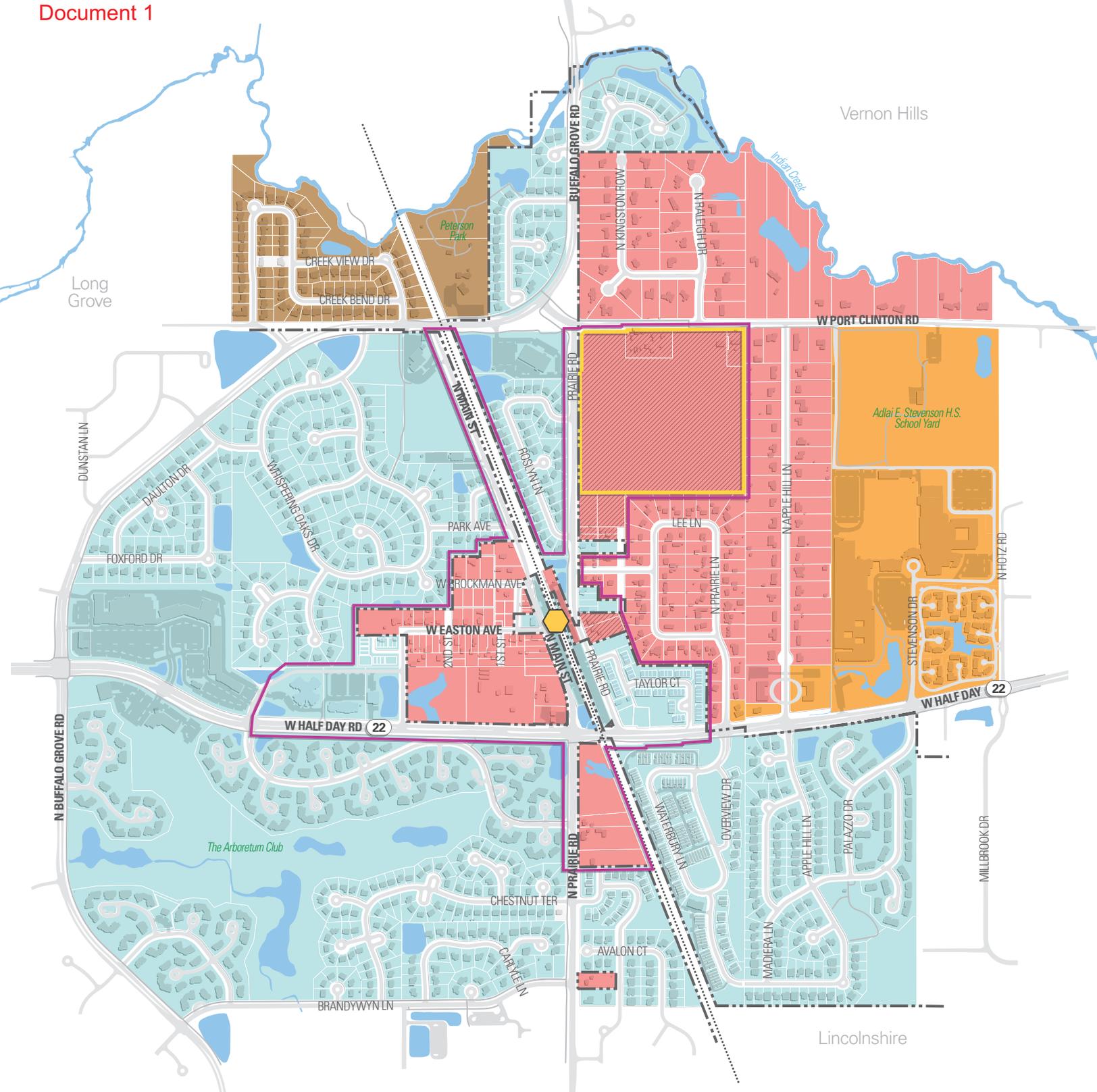




## Land Jurisdiction

Land within the Prairie View Metra Station Area includes parcels that are incorporated into the Village of Buffalo Grove as well as unincorporated parcels. Incorporated parcels are serviced by the Village's utility infrastructure and public service departments. The remaining parcels, including the Stancliff Property are unincorporated and are under the jurisdictional authority of Lake County. There are observable physical differences between unincorporated and incorporated areas including lack of sidewalks, lack of utility infrastructure, and fewer streetlights.

Some of the unincorporated area within the Prairie View Metra Station Area, including the 40-acre Stancliff Property, is subject to the 2005 boundary agreement between the Village of Buffalo Grove and the Village of Lincolnshire. This agreement identifies future boundary lines as well as future land uses.



## JURISDICTIONAL BOUNDARY

### Legend

-  Study Area Boundary
-  Village of Buffalo Grove Municipal Boundary
-  Future annexation into Village of Buffalo Grove via Boundary Agreement
-  Stancliff Farm (40 acres)
-  Village of Buffalo Grove
-  Village of Vernon Hills
-  Village of Lincolnshire
-  Unincorporated Lake County
-  Prairie View Metra Station





The American Foursquare  
@ Zone 5

## Architectural Typologies & Character

The overall development of the Prairie View Metra Station Area presents a cohesive and comprehensive cross section of Post-War suburban residential development in the Midwestern United States.

Five Primary Typologies were observed within the six area zones defined on the adjacent map:

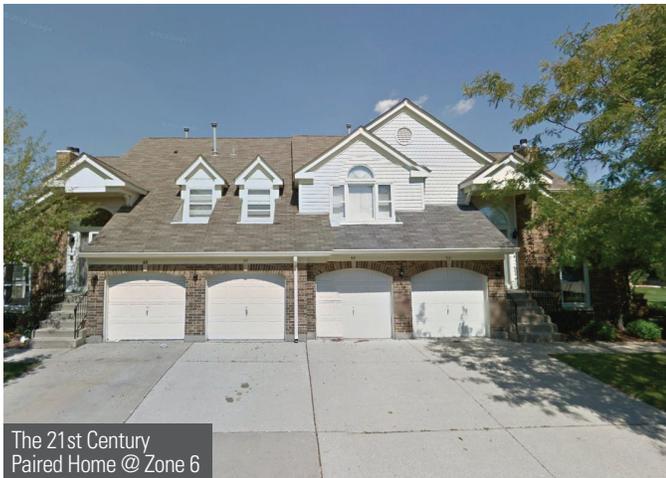
- The American Foursquare (Early 1900s)
- The Post-War Building Boom (1940s - 1970s)
- The Late 20th Century Single Family Home (1980s-2000s)
- The 21st Century Paired Home (1990s-2000s)
- The New Urbanist Townhome (2010s)



The Post-War Building Boom  
@ Zone 4



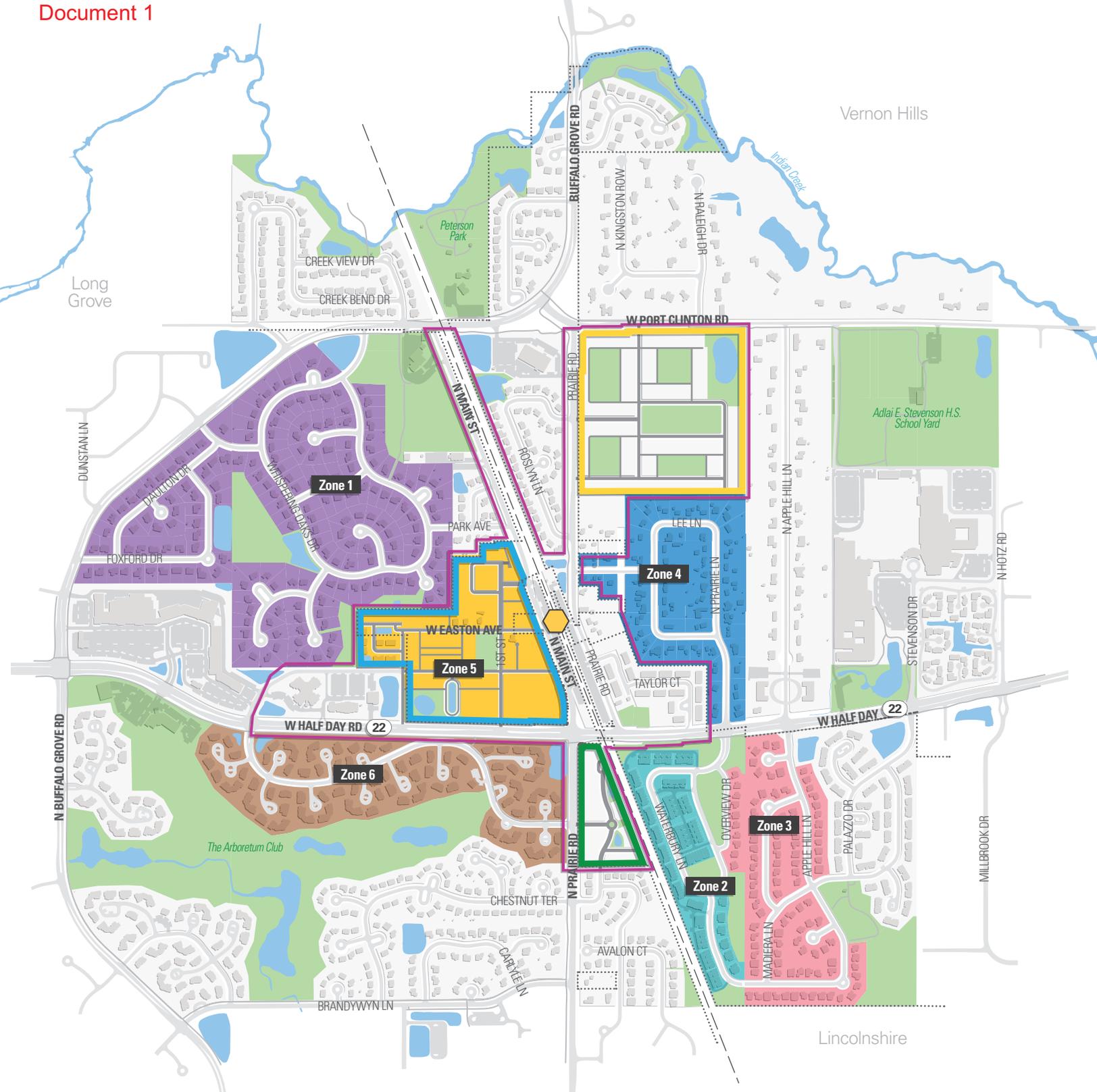
The Late 20th Century Single Family Home  
@ Zone 1 & 3



The 21st Century Paired Home  
@ Zone 6



The New Urbanist Townhome  
@ Zone 2



# ARCHITECTURAL TYPOLOGY ZONES

## Legend

- Study Area Boundary
- Village of Buffalo Grove Municipal Boundary
- Proposed Roadway Alignment
- Prairie View Metra Station
- North Subarea
- Central Subarea
- South Subarea
- Parks/Open Space





## Transportation

### Roadways and Vehicular Traffic

The Prairie View Metra station is located approximately 3.5 miles west of Interstate 94. East-west access to the study area is provided by Half Day Road (IL Route 22). Located approximately one mile east of the study area, Milwaukee Avenue (IL Route 21) provides north-south access to the study area.

Throughout the study area, the street system is predominantly a network of local and collector roadways with arterial roadways around the perimeter.

### Transit

The Prairie View station serves as a key activity center for the study area. Located along the Metra North Central Service (NCS) Line, commuter rail service is provided on track owned by Canadian National Railway Company (CN). The tracks also support active freight. According to data provided by the Illinois Commerce Commission, an average of 42 daily trains, including 22 passenger trains, is reported at the at-grade crossing on Buffalo Grove Road. In addition to the Buffalo Grove Road crossing, an additional at-grade crossing is located within the study area at Half Day Road. A dedicated pedestrian crossing is also provided near the Prairie View station building.

The NCS Line provides north-south commuter access between Chicago Union Station and the Village of Antioch. Monday through Friday, Metra operates a total of nine inbound trains from Prairie View to Union Station, and a total of ten outbound trains from Union Station to Prairie View. These trains also provide access between Prairie View and the O'Hare Transfer station. Weekend Metra service is not provided along the NCS Line.

Prairie View is the second busiest station on the NCS Line with a total of 388 boardings (all trains). With a total of 590 boardings (all trains), Buffalo Grove is the busiest station along the NCS Line. The Prairie View Metra Station's parking lot has a capacity for 431 spaces and it has an observed utilization of approximately 73%. A good portion of the commuter parking space are utilized by Stevenson High School students.

Approximately 20 percent of commuters are dropped-off at the station. Vehicle pick-up/drop-off activity currently occurs in the commuter parking lots on both the east and west sides of the train tracks. The Prairie View station also has a significant portion of the commuters who walk to/from the station. It is important to note that approximately 20 percent of commuters using the Prairie View station walk, which is about double the percentage of commuters walking to both the Vernon Hills and Buffalo Grove stations.

Fixed route bus service to the Prairie View Metra station is not currently provided.

### Pedestrian and Bicycle Network

In general, pedestrian connectivity in the study area is discontinuous with sidewalk provided along some segments. Although the study area maintains a pedestrian-scale development pattern, infrastructure to support pedestrian activity is limited and in some cases, the built environment discourages pedestrians.

The existing bicycle network provides connectivity between adjacent residential subdivisions, commercial areas (e.g., Woodland Commons), Prairie View Metra Station, area parks and recreational and institutional uses such as local parks, schools, religious facilities, and township offices.



## Market and Economic Assessment

The Prairie View Metra Station Area has a significant demographic and market base. The population approximately a quarter of a mile from the Prairie View Metra Station, which encompasses the study area, has approximately 504 people and 200 households in 2018.

The Prairie View Metra Station Area's residential market is strong as families with children are attracted to the area's high quality public school district. Single-family homes, condominiums, and townhomes are being quickly absorbed into the market. The Prairie View Metra Station Area's is positioned well to offer housing typologies that are appealing to the empty-nester and millennial demographics. These demographics value walkability (especially near commercial options), open space, condominium/townhome housing products that offer little to no maintenance. Access to the Metra train station will also be one of the greatest amenities the Prairie View Metra Station area offers.

The Prairie View submarket is positioned between two relatively strong retail areas. To the west of Prairie View is the Woodland Commons shopping center and to the east is the Milwaukee Road Corridor. There are also other numerous retail and shopping choices within a few miles of the Prairie View station.

Based on market analyses, the area may be able to support approximately 40,000 to 50,000 square feet of retail oriented towards commuter and residential needs as well as destination businesses.

With the local resident household's higher proclivity for health and fitness activities, personal health focused businesses may find a strong niche serving highly educated commuters and nearby residents. Personal health care services such as: hair/nail salon, day spa, fitness center, and specialty fitness. Specialty foods represent another potential retail market. Drinking/food establishments that complement other health and wellness businesses are also viable. All retail should contribute to the identity and character of the area.



# Principles & Concepts

Developing a *community-driven* vision was a fundamental objective of the Plan. Consequently, the project team gathered the main findings from all the project’s outreach activities and distilled them into the plan’s principles. These principles guided the vision’s direction and development.

## 7 Principles



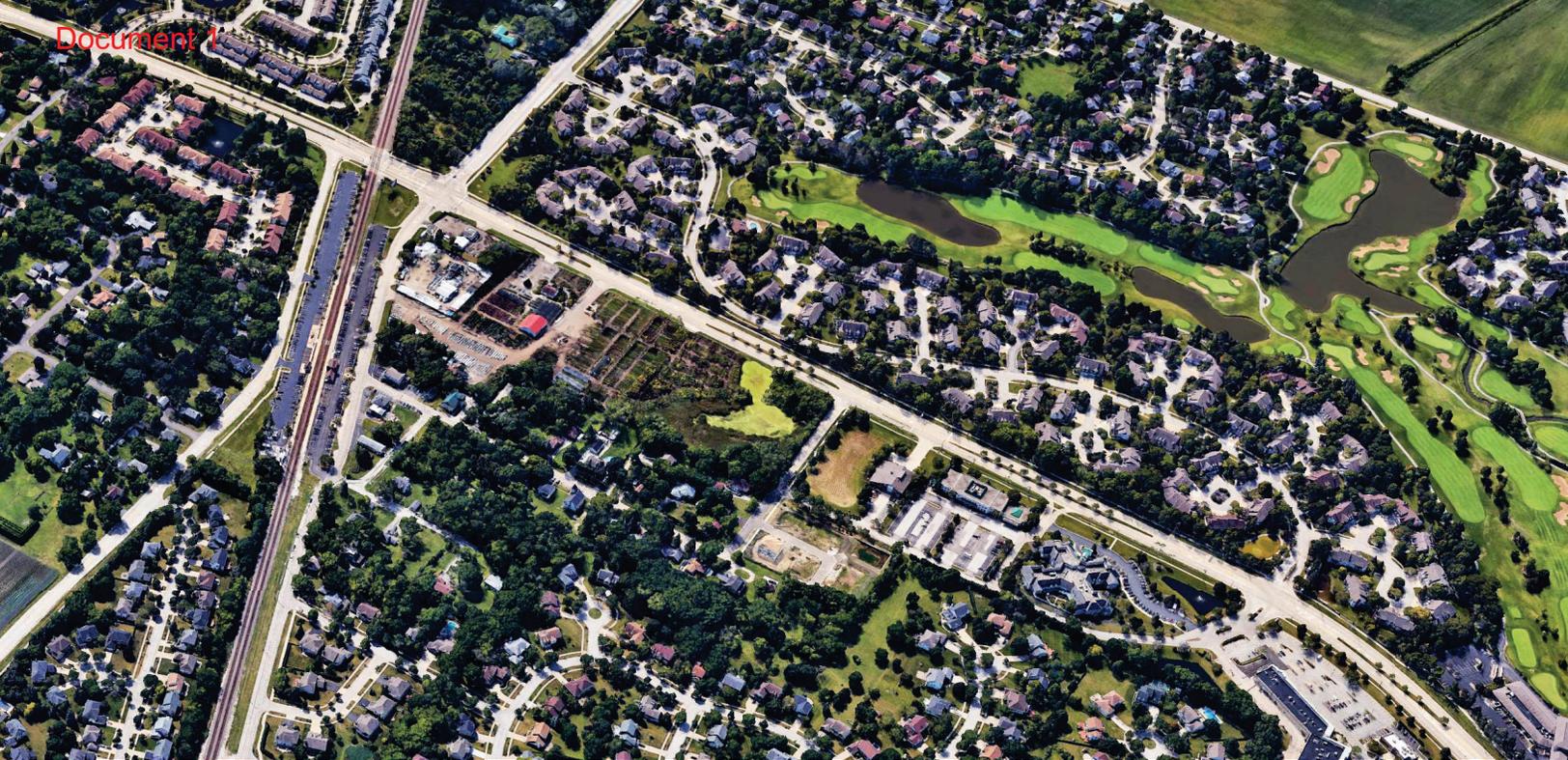
### Principle 1

**Preservation and celebration of area’s historical character, charm and small town feel.** The Prairie View area’s buildings and architecture provide a distinctive character and charm. The Plan should look to enhance and celebrate this character.



### Principle 2

**Create a Destination and establish an identity.** The Plan should look to establish the Prairie View Area as a destination and help it achieve a sense of identity. The place-making elements should be authentic and unique to the area, but support and complement the area’s context and strengths.



**Principle 3**

**Incorporate and leverage the area’s greatest assets.** The neighborhood has access to various amenities, assets, and resources including the Prairie View Train Station, Vernon Township, and Stevenson High School. The plan should look to build off these assets as they provide additional means of vibrancy and character.



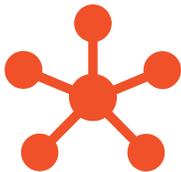
**Principle 4**

**Provide residential options that accommodate the changing demographics.** The Plan should look to provide residential options that can accommodate the needs of the area’s emerging demographics which include millennials, young families, baby boomers, and empty nesters.



**Principle 5**

**Look to add neighborhood and convenience commercial establishments.** The Plan should include neighborhood and convenience commercial development that can be supported in today’s retail market as well and leverage the area’s pedestrian traffic.



**Principle 6**

**Enhance the connectivity of the area – roads, train, bike paths, and pedestrian. Emphasis on walkability.** The Plan should look to expand the neighborhood’s connectivity to, from, and within the neighborhood. This accessibility should be examined for all modes of transportation but there should be an emphasis on walkability.



**Principle 7**

**Incorporate open space and environmental sustainability.** The Plan should look to preserve and celebrate the natural environment in the area by incorporating open space and other sustainability initiatives.

## KEY PLAN CONCEPTS

The following highlights potential concepts the Village of Buffalo Grove may consider in implementing the goals and strategies of the Prairie View Metra Station Area Plan. They can also serve as discussion points between the Village and stakeholders in regards to the Plan's goals and strategies.

### Unique Businesses in Prairie View

#### Changing Trends and Retailers' Health Clinics

Consumer's behavior in approaching their health and wellness has shifted over the years. In the past, this mostly involved people having regularly scheduled doctor's visit and exercise and a mindful diet. With the proliferation of easily accessible information, consumers are beginning to engage more in "self-care" to meet their health goals in a more personal and meaningful ways, in addition to avoiding chronic diseases, that annual check-ups could not. Additionally, the growth of sales and manufacturing has been sluggish at best over the past few years. According to Nielsen's Health and Wellness 2020 Report, the sales are forecasted to grow by only 0.9 percent. These trends present an opportunity for retailers to impact consumers' lives by supporting consumer's medical needs. The Prairie View Metra Station Area currently has an underutilized medical office space, which presents an opportunity to attract contemporary medical and wellness-focused tenants.

In recent years, many retailers have prioritized healthy foods in the center of their store to emphasize the notion of healthy eating and established health clinics to provide consumers' guidance at a more personal level. In fact, Accenture projected there would be an increase of at least 2,800 in-store retail clinics in the U.S. by the end of 2017. This would equate to about 47 percent increase from 2014. This demonstrates that there is a growing acceptance among consumers in regard to the clinics' ease of access and convenience.

*Source: <https://www.nielsen.com/us/en/insights/reports/2017/capitalizing-on-health-and-wellness-trends.html>*

#### Local food Incubator

The concept of food incubator began in the 1960s as a mean to spur economic development in the U.S. Food incubator take in specialty food entrepreneurs and provide them a shared and low-cost commercial kitchen space, in addition to education, for them to make a profit in producing, selling, packaging, and marketing their food products. These spaces provide a stepping stone for food entrepreneurs in establishing and selling their brand and food to the larger market. They also provide a supportive environment where each entrepreneur learns the business insight they need on accounting, sales, and marketing.

*Source: <https://www.eater.com/2016/2/26/11110808/food-incubator-accelerator-small-business>*

## Shop Local

In many cities, success and vitality are integrally linked to the economic health of local businesses which are the foundation of the local economy and serve the needs of residents and visitors alike. Because local owners live in the community they do business in, they are free to make decisions based on local needs rather than corporate policies handed down from decision-makers far removed. Additionally, studies have shown that independent, local owners spend more money at other local businesses creating a “multiplier effect” - money recirculates within the community rather than “leaking out” to remote corporate headquarters or a centralized purchasing department. Listed below are reasons why locally-owned businesses are good for a city’s long-term viability.

**Buy Local - Support Yourself:** Several studies have shown that when you buy from an independent, locally owned business, rather than a nationally owned businesses, significantly more of your money is used to make purchases from other local businesses and service providers, and farms. “For every \$100 spent in locally owned independent stores, \$68 returns to the community through taxes, payroll, and other expenditures. If you spend that in a national chain, only \$43 stays here. Spend it online and nothing comes home.”

**Support Community Groups:** Non-profit organizations receive an average 250% more support from smaller locally owned businesses than they do from national businesses.

**Keep The Community Unique:** Where you shop, where you eat and have fun - all of it makes your community home. The one-of-a-kind businesses are an integral part of the distinctive character of Burlington. Tourism also increases as visitors seek out destinations that offer them the sense of being someplace, not just anyplace.

**Reduce Environmental Impact:** Locally-owned businesses can make more local purchases requiring less transportation and contributing less to sprawl, congestion, habitat loss and pollution.

**Create More Good Jobs:** Small locally-owned businesses are the largest employers nationally offering jobs to residents.

**Get Better Service:** Locally-owned businesses often hire people with a better understanding of the products they are selling and take more time to get to know customers.

**Invest In Community:** Locally-owned businesses have owners and employees who live in the community, are less likely to leave, and are vested in the community’s future.

**Put Your Taxes To Good Use:** Local businesses in town centers require comparatively little infrastructure investment and make more efficient use of public services as compared to nationally owned stores entering the community.

**Buy What You Want, Not What Someone Wants You To Buy:** A multitude of small businesses, each selecting products based not on a national sales plan but on their own interests and the needs of their local customers, guarantees a much broader range of product choices.

**Encourage Local Prosperity:** A growing body of economic research shows that in an increasingly homogenized world, entrepreneurs and skilled workers are more likely to invest and settle in communities that preserve their one-of-a-kind businesses and distinctive character.

*Source: <http://www.sbnportland.org/>*

## Non-Traditional Housing

### Aging In Place

The US Census estimates that between 2005 and 2020:

- Population aged between 50 and 64 will increase by 21 percent.
- Population aged 65 or older will increase by 33 percent.
- Population aged under 50 will increase by only 4 percent.

This can be attributed to the 72 million baby boomers going through their life cycle in conjunction with a profound increase in life expectancies. In Buffalo Grove, about 23 percent of the Village's population is aged between 50 and 64 years old in 2016. This indicates that there would be a significant increase in population aged 65 and above in the coming years. These trends demonstrate that aging in place is becoming a more critical issue for the Village to tackle as its population ages. It is important for the Village to allow innovative land uses to allow its elderly population to independently live in their homes and community safely and comfortably, regardless of their socio-economic background.

### Multi-Generational Housing

The US Census showed that 18.1% of the U.S. population (57 million Americans) lived in multi-generational households in 2012. This is double the number of such household in 1980. This growth spiked during the Great Recession of 2007-2009 and continued to grow, despite at a slower pace, during the post-recession period. Racial and ethnic minorities are more likely to live in a multi-generational family, which has been increasing since the 1970s. The Pew Research Center founded that Asians are most likely of all minority groups to live in multi-generational arrangements. In 2016, the Asian population consists of 19 percent of the total population in the Village of Buffalo Grove.

There are various reasons for this phenomenon. Young adults are marrying in later stages and staying in school longer. This contributes to an increasing share of young adults living with their parents. Declining employment and wages may also undermine adults' ability to live independently. Many existing residential units in the Village of Buffalo Grove are designed for single-generation living, and this issue came up in previous Steering Committee meetings and the public workshop. There is an interest to promote new housing types that are conducive to multi-generational living. The following are design and housing examples that promote such concepts.

Source: Pew Research Center | <http://www.pewsocialtrends.org/2014/07/17/in-post-recession-era-young-adults-drive-continuing-rise-in-multi-generational-living/>



Example of an in-law unit that is above a detached garage.

Source: <https://www.theplancollection.com/garage-apartments-house-plans>

### Design Features of a Multi-Generational Housing

- **Multiple Masters:** Multi-generational homes may require a second master bedroom, preferably on the first-floor, to provide better access and privacy for older parents. This also give them a sense of privacy in a shared home.
- **Suite Addition:** One of the best ways to make multi-generational housing work is to construct an in-law suite/apartment with a separate kitchenette and entrance (see **In-Law Units / Carriage House** for greater detail). The feasibility this feature will depend on zoning restrictions.
- **Versatile Living Room:** Multi-generational housing can either feature segregated living room or a large space with appropriate amenities to enable different generations to coexist. An example is to install shelving/storage units in the living room to help keep the space free from toys and clutter, which can pose a safety issue to seniors.
- **Flex Space:** Rooms should be designed to be adaptable for various uses. For example, an underutilized living room can be repurposed into a home office, then an in-law suite, then a space for a young adult who decided to move home, and then an entertainment space.
- **Universal Design / Open Access:** This works in tandem with flexible spaces to create environment that all persons can use. Examples of universal design include wide-enough hallways to accommodate a wheelchair and zero-entry threshold to accommodate people with physical disabilities.

Source: <https://www.bsbdesign.com/five-essentials-multigenerational-home-design-interior-designer-cook/> & <http://www.morseconstructions.com/blog/key-features-of-multi-generational-homes>



In-law units can be designed to conform to existing development's character.  
Source: <http://www.verseksmsek.com/>

### In-Law Units / Carriage Housing

Also known as granny flats, in-law units are apartments that are an accessory to a primary residence. They have their own entrance, kitchen, bedroom, and living area. In-law units are typically situated above the garage of the primary residence or as a separate structure in the primary residence's rear lawn. The unit is not rented out. Rather, it is occupied by the primary residence's guest or relative.

In-Law units could provide benefits to the community, homeowners, families, and elderly alike. Having in-law units increases the number of persons per households in existing residential neighborhoods. This reduces the need for municipalities to extend utilities and services. This option helps municipalities increase their diversity of housing options at moderate pricing points without requiring local funding. Having in-law units enables the elderly population to independently remain in the community and to have continued access to nearby community amenities.

Currently, municipalities may have regulations pertaining to what certain relatives and guests can live there and whether rents may be charged. The Village of Buffalo Grove's Zoning Ordinance currently does not permit residential uses on accessory dwelling units (see 17.12.005 - Accessory building or structure in the Village's Municipal Code). The Village should evaluate its permitting and regulatory process to see how its neighborhoods can accommodate in-law units in a fashion that they would contribute to the neighborhood's sense of place.



An example photo of a parklet.  
Source: Flickr / Steve Chou



An example photo of a pop-up gathering space.  
Source: Flickr / Kent State University

## Temporary Art Installations

Pilot art installation programs enhance the public realm, create interim uses for vacant lots and underutilized space, and transform urban space into art. Temporary installations may include parklets and plazas, window clings in vacant storefronts, public sculpture, or community-led and built gardens. The act of improvising and proactively reinvigorating the public realm, is also known as a Pop-up or Tactical Urbanism. These DIY sessions are intended to be place-based and neighborhood specific. Temporary art installations are relatively low-budget and are not generally capital intensive. They are opportunities for community members to participate in the improvement of their neighborhood. Based on the character of Prairie View, events and programs can be created in coordination with property and business owners to ensure they gain greater exposure from the events. Opportunities for these urban interventions include:

- Window decals/window clings
- Temporary stages
- Urban gardens
- Parklets/Plazas
- Food Trucks
- Music
- Art Walk
- Temporary art installations



Source: <http://api.ning.com>

## Urban Tree Canopy

Urban Tree Canopy (UTC) is defined as the layer of leaves, branches, and stems of trees that cover an area when viewed from above. It is useful in absorbing rainfall that would otherwise run off on a paved surface, picking up pollutants along the way, and be transported to the local storm drainage system. UTC provides many benefits to the Prairie View community. This includes: reducing urban heat island effect, increasing property values and energy savings, reducing air pollution, and promoting aesthetic benefits. Enhancing UTC in a community, such as street tree planting, is a crucial first step in seeking improvements to green infrastructure.

A concentration of street trees can contribute to a greater concentration of UTC in Prairie View. Street trees are often thought of as a liability to maintenance. However, a Modesto, California study revealed that the asphalt on streets shaded by large canopy tree lasts longer than that on unshaded streets, reducing maintenance costs by sixty percent over thirty years.<sup>1</sup>

<sup>1</sup> McPherson, E. Gregory, and Jules Muchnick. (2005). *Effects of Street Tree Shade on Asphalt Concrete Pavement Performance*. *Journal of Arboriculture*, 31(6), 303-310.

## Energy Savings for Homeowners

### Illinois Homeowner Performance (IHP) with ENERGY STAR

This program aims to assist Illinois homeowners and property owners to reduce their energy costs and improve their homes' comfort, safety, durability, and property value. This is achieved when homeowners work with IHP-qualified contractors to adopt a "whole-home" approach when improving the home's energy efficiency. A "whole-home" approach is defined as accounting for the entire home and all its systems when evaluating their performance and prescribing remedies.

After completing the improvements, the homeowner receives a Certification of Completion that documents the improvement and investment the homeowner has made. Illinois's multiple listing services (MLS) and the Midwest Real Estate Data (MRED) recognize this program, which enables homeowners to display their certification when they list their home on the market. Similar certification programs in other regions have shown that homeowners with similar certifications were able to sell their homes faster and/or an increased price point.

#### Eligibility

- Single-family home, townhome, or individual condo in a building of 4 or fewer units with heating and/or cooling equipment specific to the individual unit
- A building of 4 or fewer units with shared heating and/or cooling equipment may also be eligible if the upgrade addresses the building as a whole\*
- The building's original construction must be completed prior to audit (that is, IHP is only applicable to existing homes, not new construction)
- The home must be located within ComEd, Nicor Gas, Peoples Gas, and North Shore Gas utilities.

Source: <http://www.illinoishomeperformance.org/what-is-ihp>

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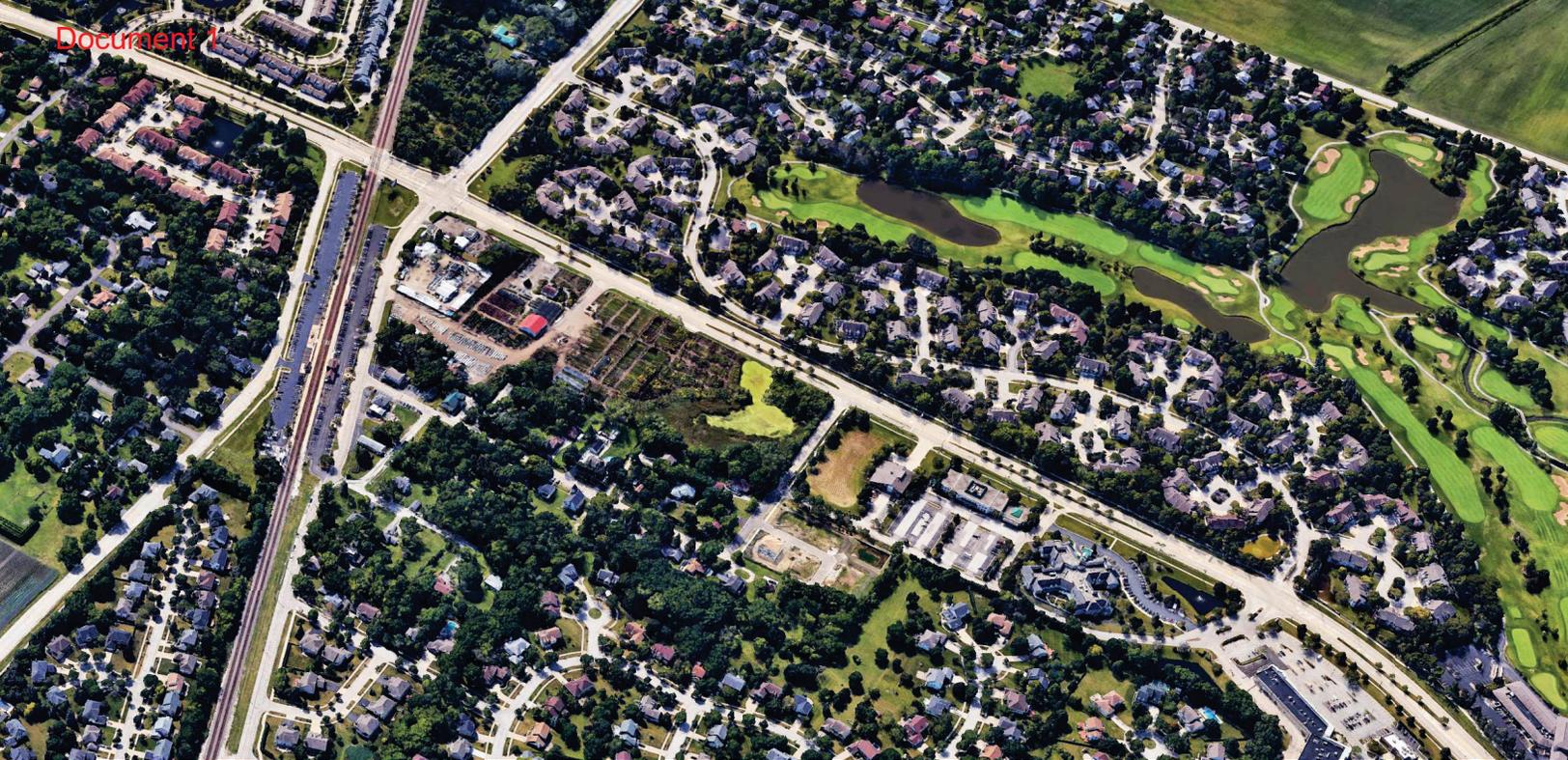


# Land Use & Development

## LAND USE

The Land Use and Development Chapter is intended to guide the Village's land use and development decisions to be consistent with the Village's long-term vision and strategic goals. The land use plan will designate desired land uses within the Prairie View Metra Station Area, including the three subareas. These subareas are situated within unincorporated Lake County parcels, including the 40-acre Stancliff Property.

To promote cohesive and legible development of the Prairie View Metra Station Area these parcels will require incorporation into municipal boundaries. This is critical to ensure redevelopment within the subareas and the Station Area fall under a comprehensive vision and have similar access to the Village' infrastructure and services. The following describes the three subareas the Plan has designated.



## North Subarea

The North Subarea consists of the 40-acre Stancliff Property. It is envisioned that future development in this area accommodate compact single-family detached and attached housing and incorporate sustainable strategies.

## Central Subarea

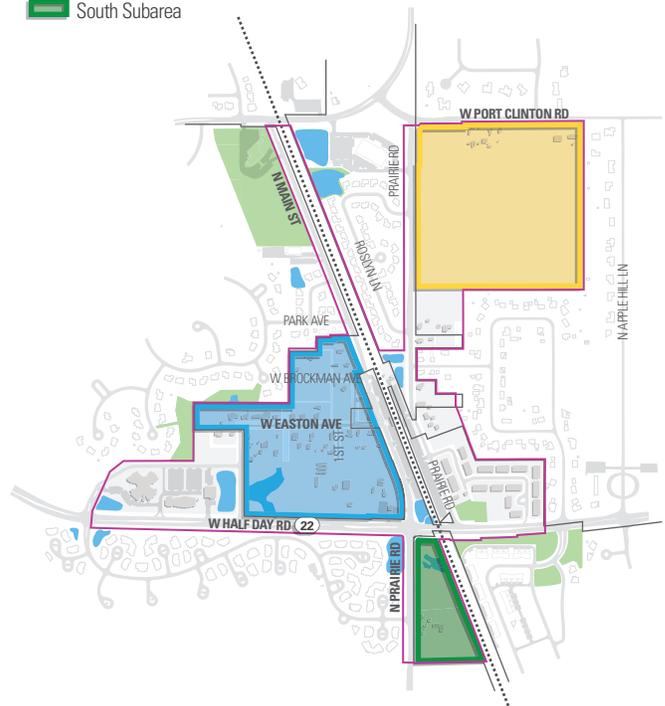
This subarea contains unincorporated Lake County Parcels west of the Prairie View Metra Station and north of Half Day Rd. There will be a variety of housing choices within this subarea, and commercial activities are hosted within mixed-use buildings fronting N Main Street. Streetscape improvements, with corner plazas at Easton Ave. and N Main St. will welcome Metra Commuters into the Station Area.

## South Subarea

The South Subarea will accommodate a corner mixed-use building with gateway signage and plaza at the southwest of Half Day Rd. and N Prairie Rd. Stacked townhomes and stormwater detention areas are located throughout this subarea. The roadway entryway into the South Subarea will align with the existing Willow Parkway.

### REDEVELOPMENT OPPORTUNITY SITES

-  Study Area Boundary
-  Village of Buffalo Grove Municipal Boundary
-  North Subarea
-  Central Subarea
-  South Subarea



## Land Use Designations

### Single-Family Detached

This land use designation consists of single-family detached homes. This is the predominant existing land use surrounding the Prairie View Metra Station Area. The Plan calls for this use to be concentrated at Brockman Avenue and at the North Subarea. The character of single-family detached neighborhoods includes a variety of architectural style, including the American Foursquare and the Late 20th Century Single-Family Homes, on suburban street networks.

### Single-Family Attached

Single-family attached residential typically consists of townhomes. These units are horizontally connected but have separate entrances from the public street and sidewalk. This land use is prevalent within and around the North (including Stancliff Property), Central (including Brockman Avenue), and South Subarea. The Plan calls for a planned expansion of the newly-constructed Easton Station townhomes on Easton Avenue. The existing townhomes at Waterbury Place is a good model for future single-family attached developments.

### Multi-Family Residential

Multi-family residential areas include apartment/condominium buildings where units share a common entrance or hallway. The future land use plan envisions an additional multi-family building along W Half Day Road and mixed-use buildings with upper-floor multifamily units along N Main Street.

### Commercial

Commercial includes general retail, dining, and services that are tailored to the local customer base. The plan suggests incorporating all future commercial space within mixed-use buildings.

### Office

The office building at 150 W Half Day Road currently houses medical uses. To help bolster occupancy, the building should look into accommodating non-traditional medical uses, such as wellness and health businesses.

### Mixed-Use

The intersection of W Half Day Road and N Main Street and N Main Street across the Prairie View Metra Station are intended to be an attractive, pedestrian-scaled, and mixed-use environment. Desired ground-floor land uses include retail, restaurants, personal services, offices, and wellness and health establishments. Upper-floor uses can consist of multi-family units. A 4-acre mixed-use building is located at the northwest corner of N Main Street and W Half Day Road.

### Education

Stevenson High School is located east of the Prairie View Metra Station and can be accessed from W Half Day Road and Stevenson Drive. Being a reputable high school, it has attracted many families to move into the Village of Buffalo Grove. There are also opportunities to improve and increase connections between neighborhoods around the Prairie View Metra Station Area and the Stevenson High School.

### Religious

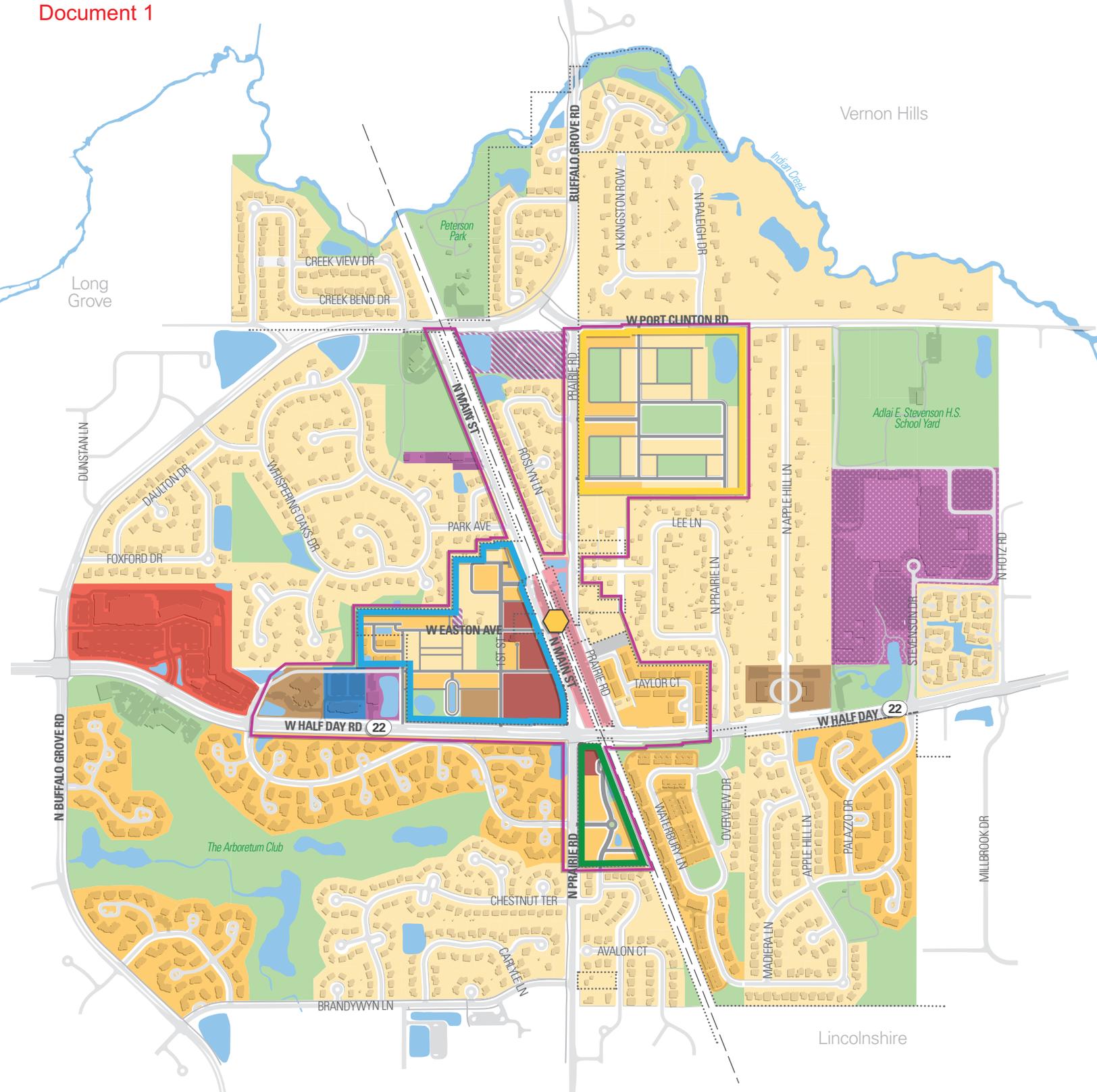
The Westminster Reformed Presbyterian Church (16670 W Easton Avenue) is the only religious land use within the Prairie View Metra Station Area. It provides important functions that serve nearby residents and features a distinct architectural character that is desired in future developments in Prairie View.

### Government

The Village's Fire Station 27 is a government land use located at the northwest corner of W Half Day Road and Easton Avenue. The fire station is shown to remain.

### Parks / Open Space

This land use designation includes an athletic field operated by the Vernon Township and future open space, which accommodate both active and passive recreation. These spaces also include tot lots, central green space with year-round programming, and stormwater detention area. Where possible, connections to parks and open space should be strengthened through the development of pedestrian and bike trails that provide safe passage into the surrounding neighborhoods and commercial areas.



## FUTURE LAND USE

### Legend

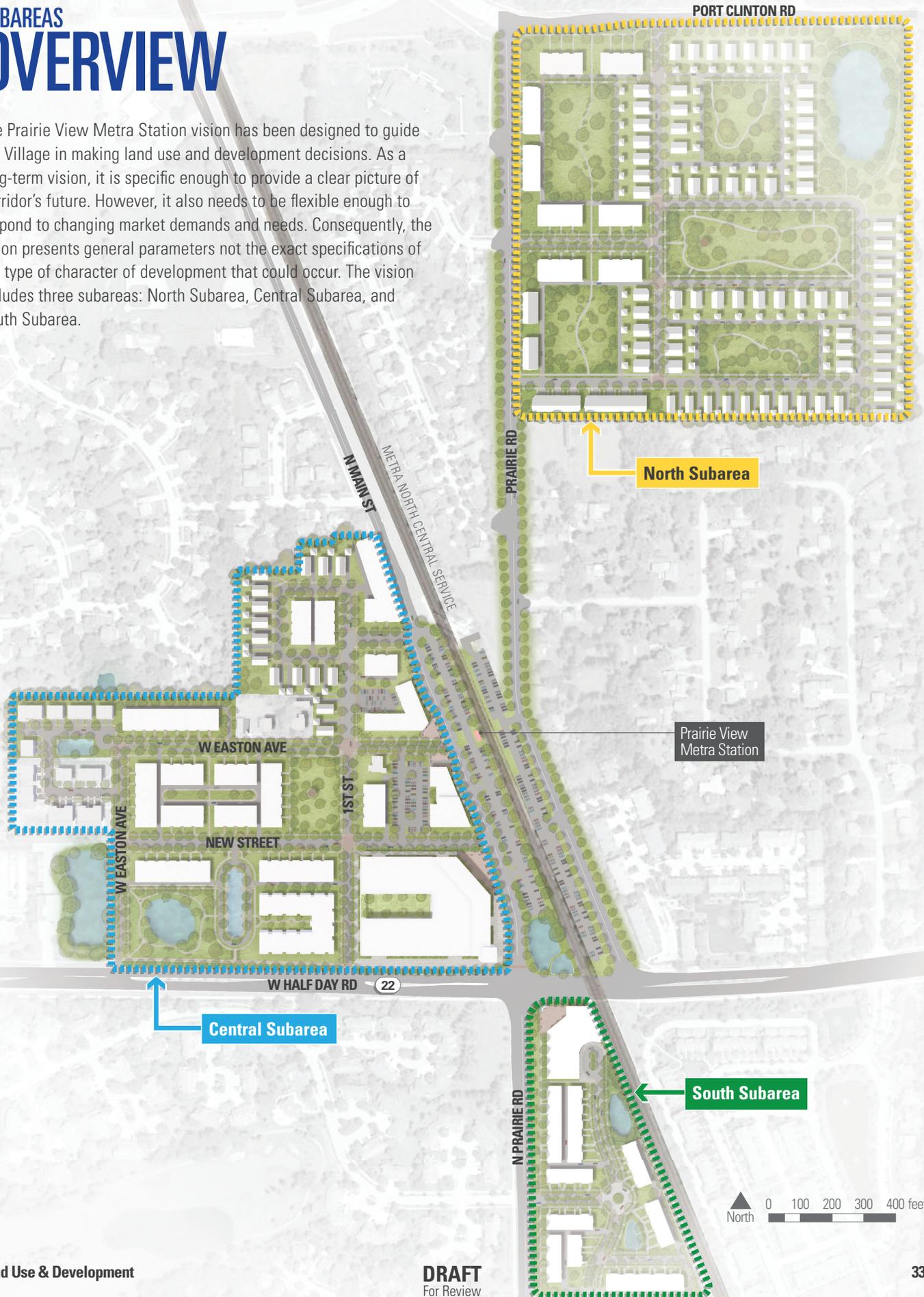
- Study Area Boundary
- Village of Buffalo Grove Municipal Boundary
- Proposed Roadway Alignment
- Prairie View Metra Station
- North Subarea
- Central Subarea
- South Subarea
- Single-Family Detached
- Single-Family Attached (ex: townhomes/rowhomes)
- Multi-Family (ex: condominiums)
- Commercial
- Mixed-use
- Office
- Education
- Government
- Religious
- Parks/Open Space
- Transportation



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# SUBAREAS OVERVIEW

The Prairie View Metra Station vision has been designed to guide the Village in making land use and development decisions. As a long-term vision, it is specific enough to provide a clear picture of Corridor's future. However, it also needs to be flexible enough to respond to changing market demands and needs. Consequently, the vision presents general parameters not the exact specifications of the type of character of development that could occur. The vision includes three subareas: North Subarea, Central Subarea, and South Subarea.



# NORTH SUBAREA SITE PLAN

The 40-acre former Stancliff property is located in the north section of the Prairie View Station Area. Residential units are clustered around green space which is connected throughout the whole development. The detention pond in the north east corner of the site is an amenity and includes native plantings and a walking trail. The development is organized around a large, central park that can be programmed with seasonal events for residents.

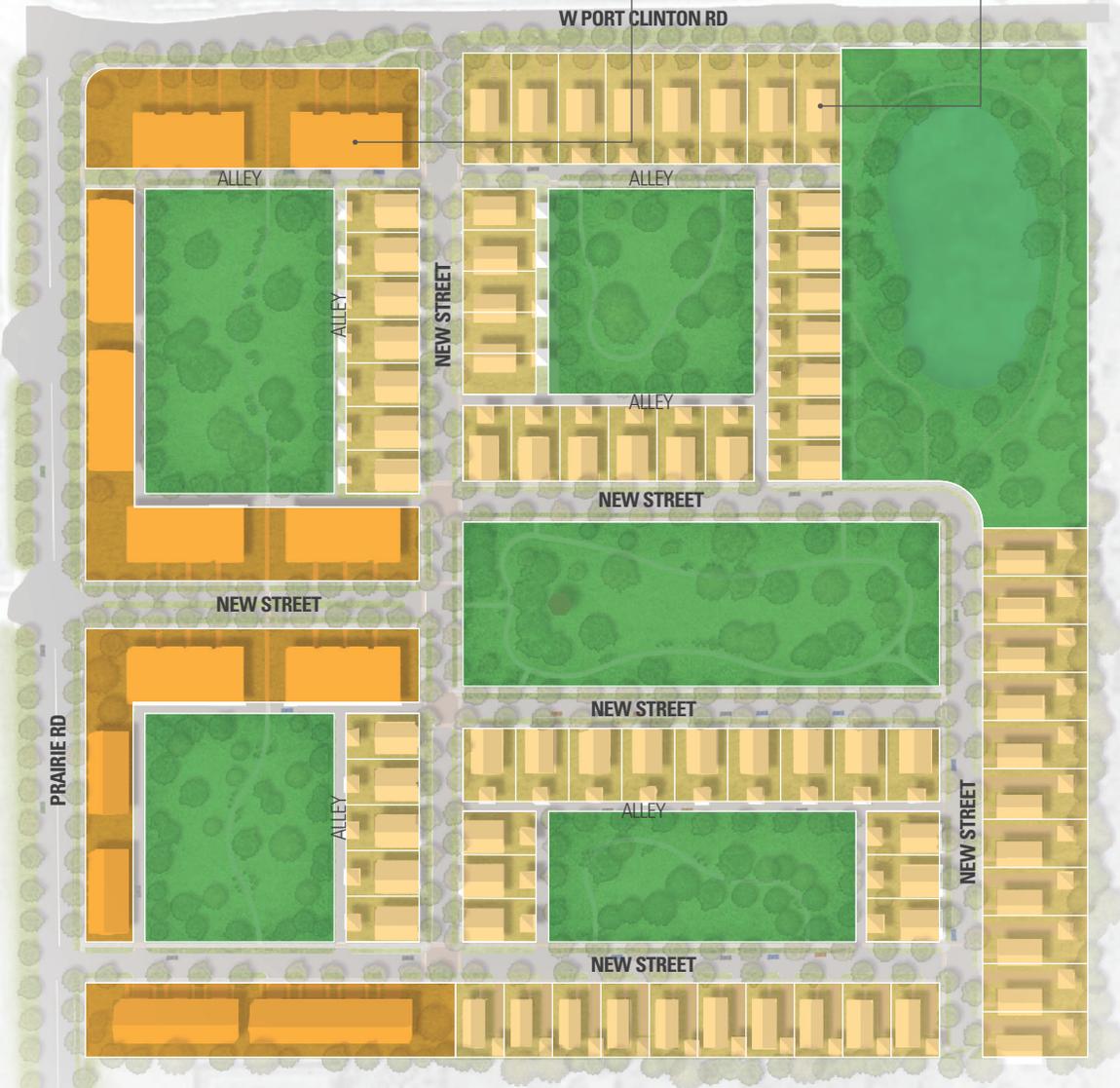
**Total Housing Units: 131 units**

- Townhome/Rowhome: 58 units
- Single-Family Detached: 73 units
- Parks/Open Space: 554,853 SF (12.7 acres)



# NORTH SUBAREA LAND USE PLAN

The primary land use is residential. Single family attached residential units are located along Prairie Road and are buffered by a planted parkway. The remainder of the residential units on the interior of the site are single family detached units.



**Legend**

- Single-Family Detached
- Single-Family Attached (Townhome/Rowhome)

Parks/Open Space



# NORTH SUBAREA CENTRAL PUBLIC OPEN SPACE PERSPECTIVE

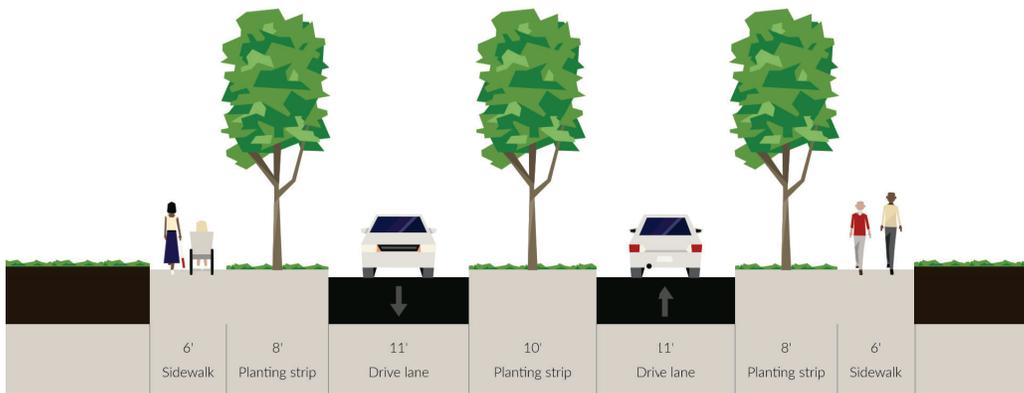
The north subarea is organized around a central green space that can be used for community programming. This gathering space adds amenity to the development and captures the identity of the community through integration of naturalized landscapes and diverse plantings. Residential units in the north subarea are clustered around shared open space. A variety of residential units including townhomes, single family home and attached rowhomes are located in the development. Higher density development along Prairie Road and Port Clinton Road is buffered by parkway plantings. A continuous trail system through the development encourages recreational walking and biking.



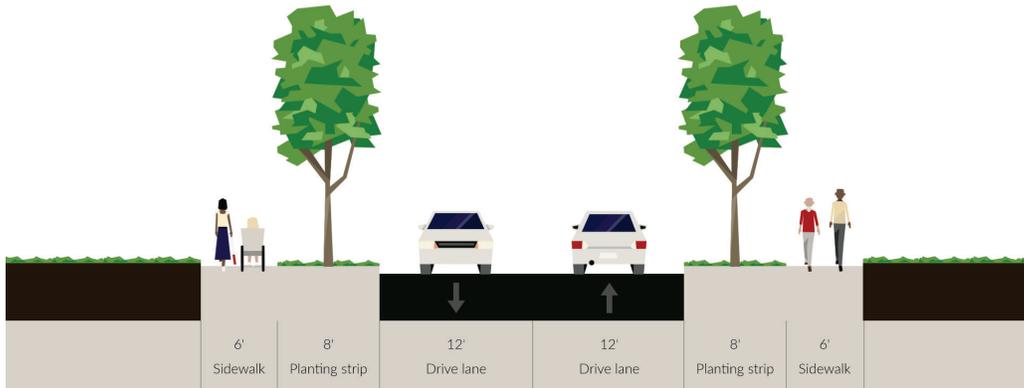
# NORTH SUBAREA CROSS SECTIONS



↑ 1 ↑  
60' ROW



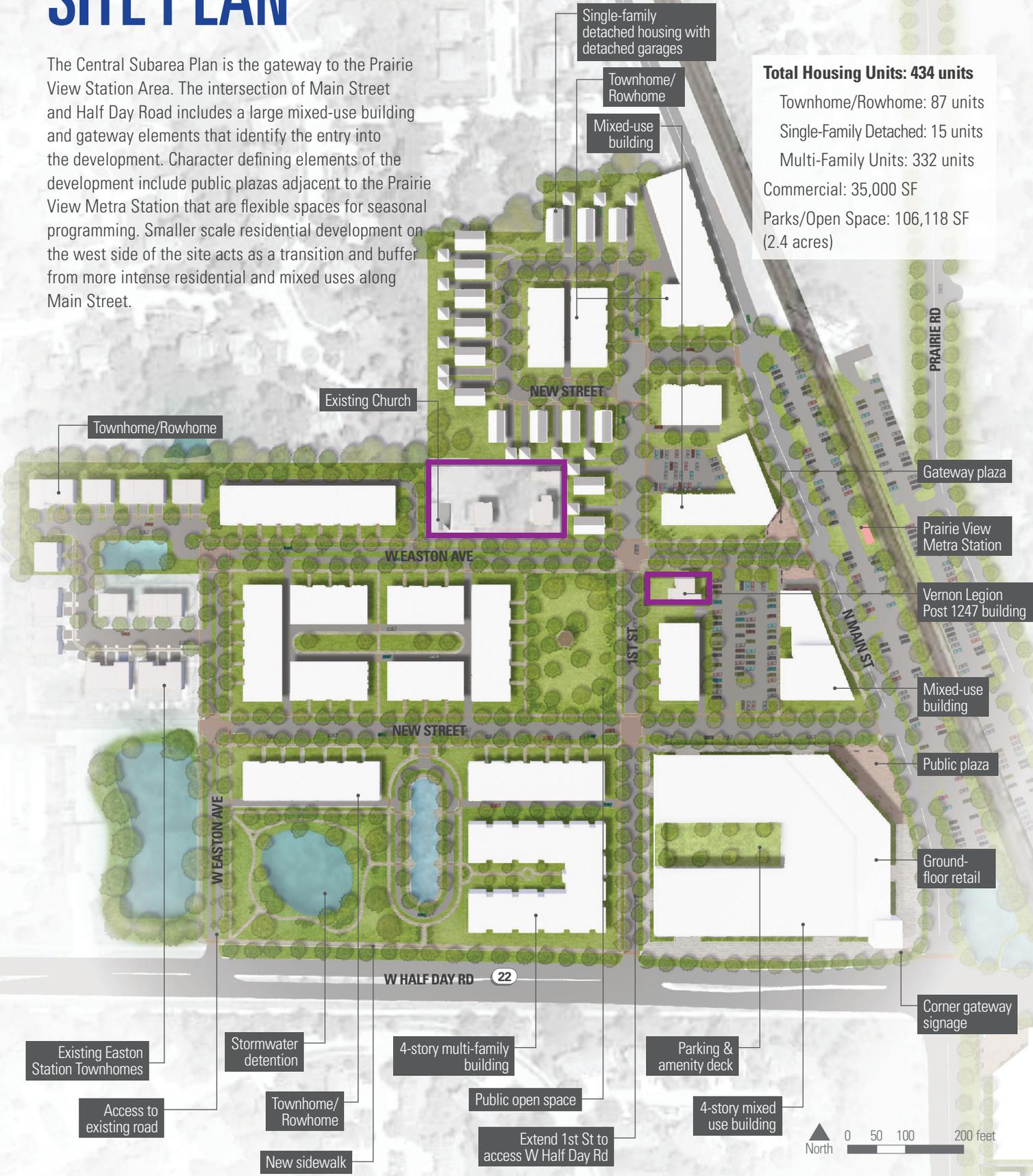
↑ 2 ↑  
52' ROW



# CENTRAL SUBAREA SITE PLAN

The Central Subarea Plan is the gateway to the Prairie View Station Area. The intersection of Main Street and Half Day Road includes a large mixed-use building and gateway elements that identify the entry into the development. Character defining elements of the development include public plazas adjacent to the Prairie View Metra Station that are flexible spaces for seasonal programming. Smaller scale residential development on the west side of the site acts as a transition and buffer from more intense residential and mixed uses along Main Street.

**Total Housing Units: 434 units**  
 Townhome/Rowhome: 87 units  
 Single-Family Detached: 15 units  
 Multi-Family Units: 332 units  
 Commercial: 35,000 SF  
 Parks/Open Space: 106,118 SF (2.4 acres)



# CENTRAL SUBAREA LAND USE PLAN

Retail development is located in high visibility areas along major arterials. Residential land uses become less dense towards the western portion of the site and act as a transition to existing neighborhoods. A variety of open space types and locations encourages recreation and offers additional potential for public gatherings throughout the subarea.



**Legend**

- Single-Family Detached
- Single-Family Attached (Townhome/Rowhome)
- Multi-Family

- Mixed-Use (Ground-floor Commercial with Multi-Family)
- Parks/Open Space
- Religious

Existing Buildings



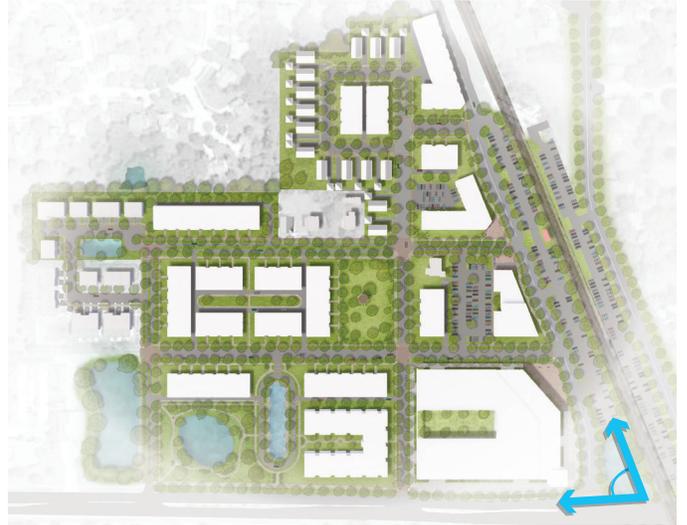
# CENTRAL SUBAREA EASTON STREET ENTRYWAY PERSPECTIVE

The Easton Street gateway is framed by two public plazas that can be programmed with seasonal events. Gateway elements identify this public gathering space as one of importance in the community. An enhanced streetscape along Main Street and Easton provide a highly amenitized public realm and encourage walking or cycling in the development. The landscape buffer along Main Street provides protection for pedestrians from car traffic. Gateway elements identify entry into the Prairie View development, add to its unique character, and create a sense of arrival for residents and visitors to the area.

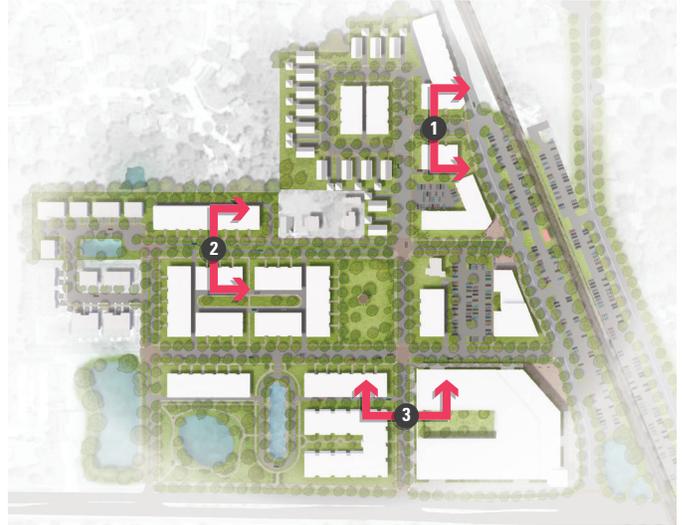


# CENTRAL SUBAREA N HALF DAY RD GATEWAY PERSPECTIVE

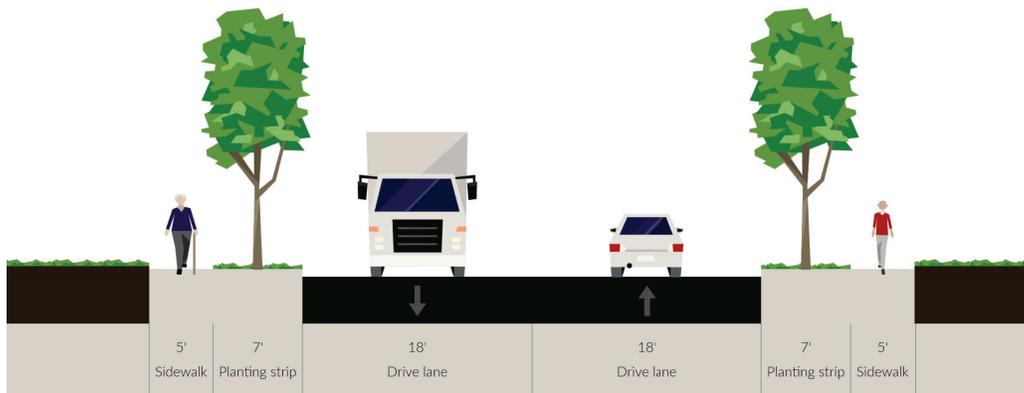
The corner of North Main Street and Half Day Road is an important intersection and gateway into the Prairie View Station Area. The intersection marks a transition of character between the major arterial of Half Day Road and the Station Area district. Discovery of Prairie View's unique character can be experienced at this intersection and movement along North Main Street. This also provides opportunities for native habitat for pollinators and green infrastructure elements to facilitate development. The sidewalk network is extended along Half Day Road to ensure continuity of the pedestrian framework.



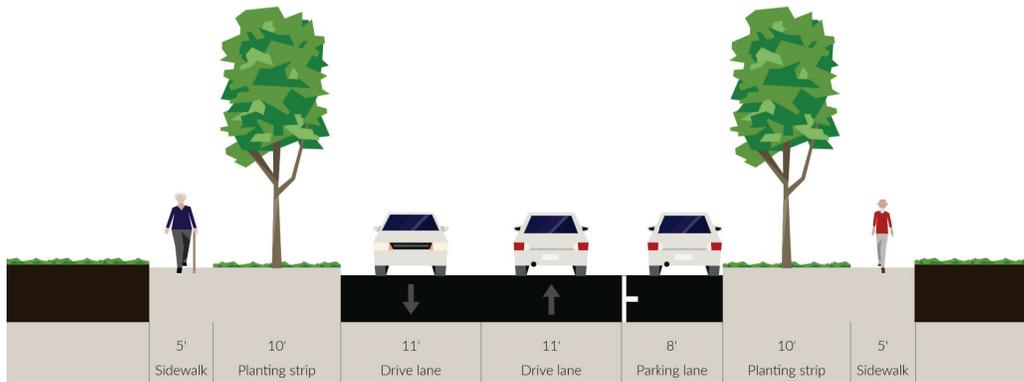
# CENTRAL SUBAREA CROSS SECTIONS



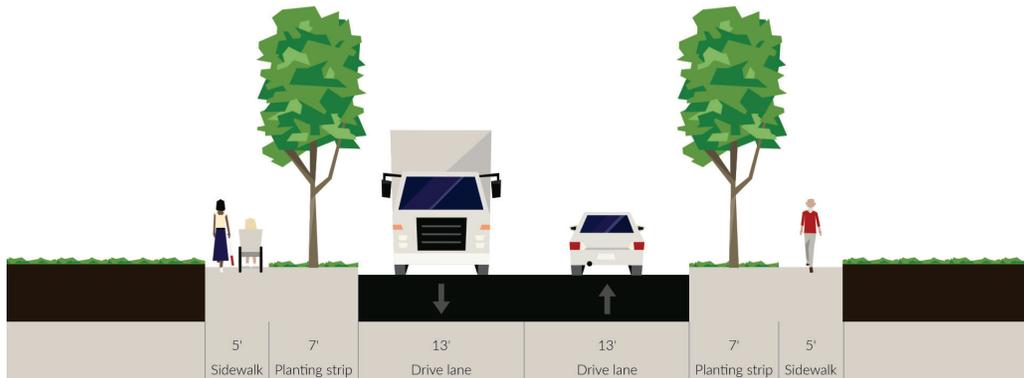
1  
60' ROW



2  
60' ROW



3  
50' ROW



# SOUTH SUBAREA SITE PLAN

The South Subarea is made up of primarily townhomes. The roadway framework and composition of the buildings takes its cues from adjacent developments of similar building typology. A main entrance off of Prairie Road leads to a central traffic island and access to the residential portion of the site. The north corner of the site including the intersection of Prairie Road and Half Day Road is occupied by a mixed use building with retail on the first floor. A public plaza is located in the northwest corner of the site and provides outdoor cafe seating and event space for seasonal programming.

**Total Housing Units: 103 units**

Townhome/Rowhome: 64 units

Multi-Family Units: 39 unit

Commercial: 16,000 SF

Parks/Open Space: 17,681 SF  
(0.4 acres)



- Rear alley
- Traffic circle
- WILLOW PKWY
- Access is aligned with Willow Pkwy
- Townhome/Rowhome
- Stormwater detention basin
- Access to adjacent subdivision

- Corner plaza with gateway signage
- Mixed-use building
- Loading / drop-off area
- Stormwater detention basin
- Public open space



# SOUTH SUBAREA LAND USE PLAN

Land uses for the South Subarea are primarily single-family residential with the exception of the north corner of the site which is occupied by mixed-use. Open space is distributed throughout the site and offers amenity and public open space to residents.



W HALF DAY RD 22

N PRAIRIE RD

WILLOW PKWY



### Legend

- Single-Family Attached (Townhome/Rowhome)
- Mixed-Use (Ground-floor Commercial with Multi-Family)
- Parks/Open Space

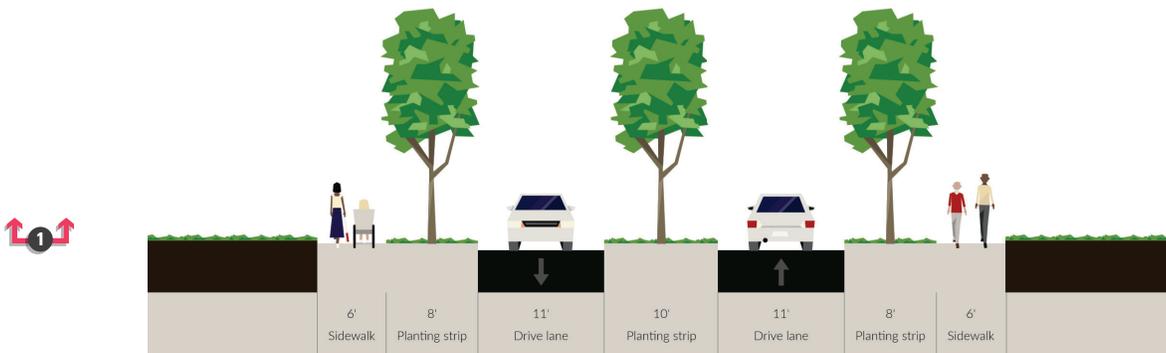


# SOUTH SUBAREA ENTRY GATEWAY PERSPECTIVE

The traffic circle that intersects the new streets is the focal point in the South Subarea. Diverse planting and a gateway signage are planted on the traffic circle to give signify entry into the South Subarea Townhomes and are oriented towards the traffic circle with ample front yard. Street trees are planted throughout the roadways to provide residents a sense of enclosure and act as buffer between pedestrians and vehicles.



# SOUTH SUBAREA CROSS SECTION



# SUBAREAS SUMMARY

## North Subarea



**Commercial Space**  
N/A



**Residential**  
131 units



**Retail Sales**  
N/A



**Market Value**  
\$60 M

## Central Subarea



**Commercial Space**  
35,000 SF



**Residential**  
434 units



**Retail Sales**  
\$10.1 M



**Market Value**  
\$70 M

## South Subarea



**Commercial Space**  
16,000 SF



**Residential**  
103 units



**Retail Sales**  
\$4.6 M



**Market Value**  
\$20 M

## Total

**Commercial Space**  
51,000 SF

**Residential**  
668 units

**Retail Sales**  
\$14.7 M

**Market Value**  
\$150 M



# Transportation & Infrastructure Plan

## CIRCULATION AND MOBILITY

As part of the implementation of the Prairie View Station Area Plan, opportunities to enhance the multimodal transportation network have been identified. These improvements enhance access and circulation options for vehicles, pedestrians, and bicycles throughout the station area through a more interconnected network of streets, sidewalks, and trails that link homes, businesses, recreation/open space, the Metra station, and nearby institutional uses. Furthermore, the multimodal transportation improvements seek to support the overall redevelopment goals, preserve the character and scale of the neighborhood, create a sustainable network, and enhance resident's quality of life.

## Street Network Improvements

The Prairie View Station Area Plan provides for an interconnected grid street network. The future streets would be designed to Village standards and would include pedestrian accommodations. A summary of the future street network is outlined below.

### Easton Avenue

This existing east-west street would be improved to Village standards with a 60-foot right-of-way. The recommended cross-section includes a single travel lane in each direction, on-street parking on one side of the street, curb, gutter, and a 15-foot pedestrian zone to include sidewalks and parkway landscaping. Because of its role in connecting the residential area to the Prairie View Metra Station, Easton Avenue should be a priority street for pedestrian improvements.



### Main Street

This street will continue to serve as a primary north-south route through the study area. The redevelopment concept introduces curb and gutter on the west side of the street and creates a continuous sidewalk in order to enhance pedestrian safety and comfort. Commuter parking would continue to be provided on the east side of the street. On the west side of the street, the existing perpendicular parking would be removed and the street would be aligned with commercial buildings.

### Half Day Road

This street would continue to be the primary east-west arterial with two travel lanes in each direction and exclusive left-turn lanes provided at key intersections. Based on the existing cross-section, limited improvements were identified for Half Day Road; however, as the roadway is under IDOT jurisdiction, right-of-way permits will be required for any work within the right-of-way, including access modifications and new crosswalks/sidewalks. The future intersection of Half Day Road/1st Street will be subject to IDOT review and approval.

### Prairie Road

The concept plan identified for the North Subarea will be connected to the Prairie View Metra Station via Prairie Road; therefore, pedestrian improvements along this street should be prioritized. Continuous sidewalk should be provided on both sides of Prairie Road between Port Clinton Road and Half Day Road. With the development concept identified for the North Subarea, access is contemplated via a new roadway aligned opposite Roslyn Lane, and an additional site access approximately 450 feet to the south. In addition, connectivity would also be provided via Port Clinton Road. Based on the average daily traffic (ADT) volume of 6,400 on Prairie Road, a new southbound left-turn lane is anticipated on Prairie Road at Roslyn Lane/Future Site Access. The existing striped median would be restriped to accommodate the left-turn lane.



## New Streets

Future completion of the grid street network would provide for an intuitive circulation pattern, distribute traffic volumes, and create an efficient development layout. Pedestrian-scaled design should be incorporated in order to promote a walkable character and support active transportation as a viable option for residents. A summary of the new streets planned for the Prairie View study area is outlined below.

### 1st Street

Within the Central Subarea, 1st Street would be relocated to the east in order to create a redevelopment opportunity site on the northwest quadrant of Main Street/Easton Avenue. Relocated 1st Street would align with the existing stub street on the south side of Easton Avenue and provide connectivity between Brockman Avenue and Half Day Road. 1st Street would be improved to Village standards with a 60-foot right-of-way and a recommended cross-section including a single travel lane in each direction, on-street parking on one side of the street, curb, gutter, and a 12-foot pedestrian zone to accommodate sidewalks and parkway landscaping. South of the Central Subarea's New Street, 1st Street's drive lanes will become narrower, resulting in a 50-foot-right-of-way.

### New Street

A new east-west street is recommended south of Easton Avenue (referred to as "New Street" for purposes of this study). A reduced cross-section should be considered in order to optimize the Central Subarea for redevelopment. Based on the Central Subarea plan, the street should be designed to prioritize pedestrians with an approximately 15-foot pedestrian zone to include sidewalks and parkway landscaping. On-street parking is recommended on one side of the street in order to provide short-term parking and loading zones supporting the adjacent residential and commercial uses.

## Site Design Considerations

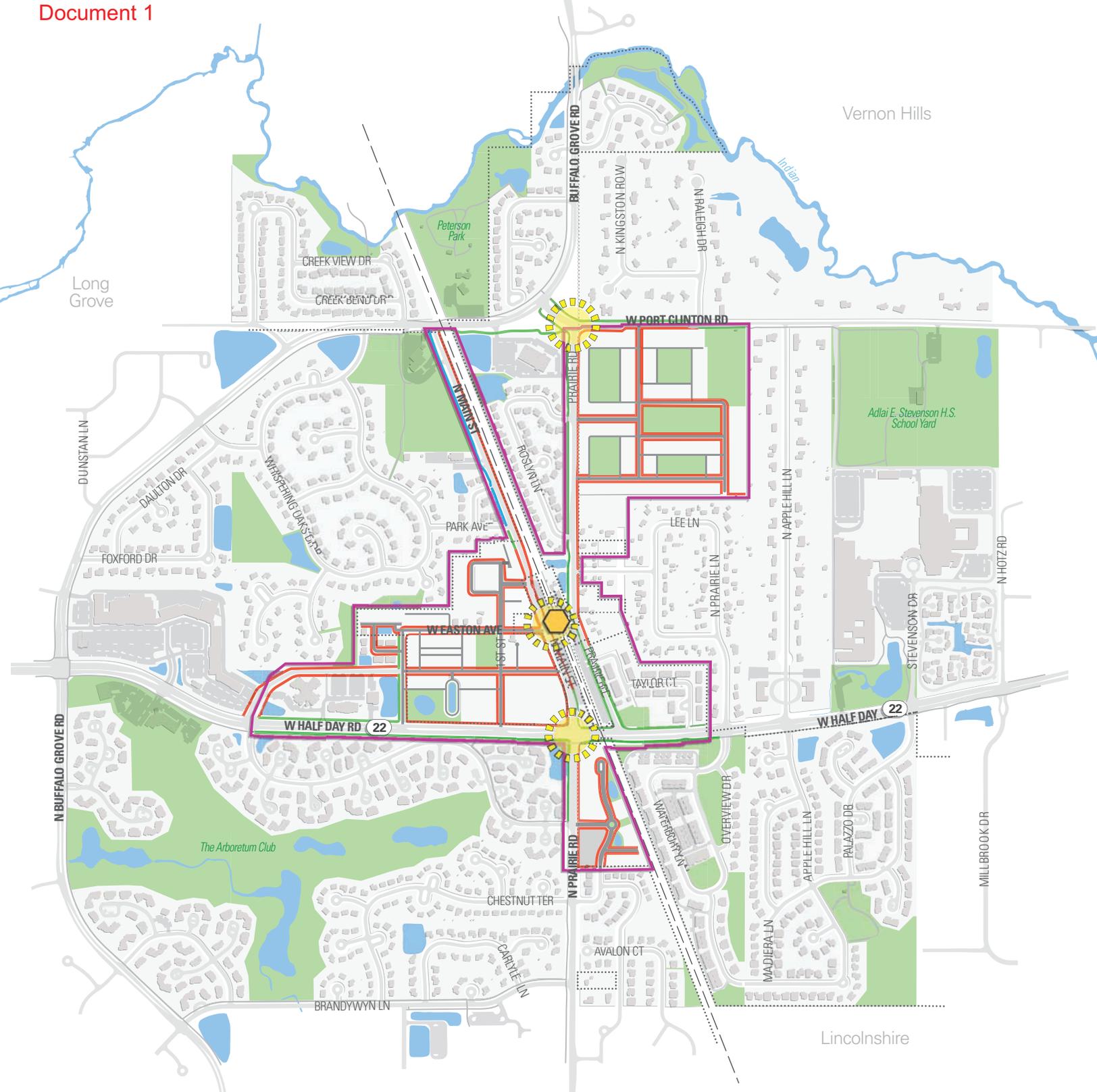
In addition to the improvements identified for the public right-of-way, site design should consider opportunities to support the transportation network and facilitate on-site access and circulation. Key site design considerations for the study area are outlined below.

### Access Management

Where feasible, commercial driveways should be consolidated to reduce vehicle conflicts with pedestrians and bicyclists, particularly along Main Street. As redevelopment occurs in the Central Subarea, opportunities to provide cross-access between properties and facilitate internal site circulation should be explored. Provision of shared parking that serves multiple uses, in combination with cross-access, should also be considered.

### Loading Activity

As commercial and mixed-use (re)development occurs within the Central Subarea and South Subarea, consideration should be given to the access and circulation for service/delivery truck loading activity. To the extent possible, consolidated delivery truck loading areas and dumpsters should be provided. Consolidated dumpsters should be provided in order to minimize the number of collection sites and reduce truck activity.



## FUTURE ROADWAY NETWORK

### Legend

-  Study Area Boundary
-  Village of Buffalo Grove Municipal Boundary
-  Incorporated Area within the Study Area Boundary
-  Unincorporated Area within the Study Area Boundary
-  Key Intersection
-  Existing Sidewalk
-  Shared Use Path
-  Future Sidewalk
-  Existing Street
-  Future Street
-  Metra Train Station





## Parking

In order to support existing and future commercial uses in the Prairie View study area, and to continue to support commuters at the Metra Station, the Village should continue to work with property owners and developers to identify site-specific parking solutions. To the extent possible and where appropriate, the Village should work with property owners and developers to identify opportunities for consolidated, shared parking. A summary of future parking conditions in the Prairie View Station Area is outlined below.

### Commuter Parking

Commuter parking should continue to be provided for the Prairie View Metra Station. Based on the 2016 Origin-Destination Survey completed by Metra, approximately 55 percent of commuters at the Prairie View Metra Station drive alone. While existing parking utilization data suggests approximately 74 percent of the spaces are occupied, this data reflects shared use of the daily fee spaces by Stevenson High School students. As (re)development occurs in the Prairie View Station Area and with any future changes to Metra ridership, commuter parking utilization should continue to be monitored and rebalanced in partnership with Metra and Vernon Township.

During off-peak commute periods, commuter parking should serve overflow parking needs for Prairie View Station Area businesses, such as restaurants, shopping, and entertainment uses, that experience peak parking demand that is complementary to commuter parking patterns. During evenings and weekends, opportunities to utilize the commuter parking lots for community programming and special events (e.g., farmer's market) should be explored. The Village should continue to coordinate with Vernon Township in order to optimize the existing commuter parking supply and existing infrastructure.

### (Re)development Parking Requirements

The parking supply provided for future mixed-use and residential developments should reflect the (re)development's proximity to the Prairie View Metra Station and the multimodal nature of resident commuting patterns.

While the proposed parking supply should reflect the unique characteristics of the proposed (re)development, data from the Regional Transportation Authority (RTA) Access & Parking Strategies for Transit-Oriented Development should be consulted as a resource for Village review and approval of proposed parking conditions. According to the RTA, a reduced parking ratio of 0.5 to 1.5 spaces per dwelling unit may be appropriate for multi-family residential developments in TOD areas. For any duplex, townhome, or multi-family residential development, consideration should be given to the supply and location of guest parking. Residential guest parking may be accommodated on-site or alternatively designated on-street parking spaces may be provided.

Due to the proximity to the Metra Station and the future pedestrian and bicycle network, future residents are anticipated to rely on active transportation (e.g., walk, bike) and transit; and therefore, parking requirements for future (re)development should be adjusted to reflect the transit-supportive and walkable nature of the Prairie View Station Area.



### On-Street Parking

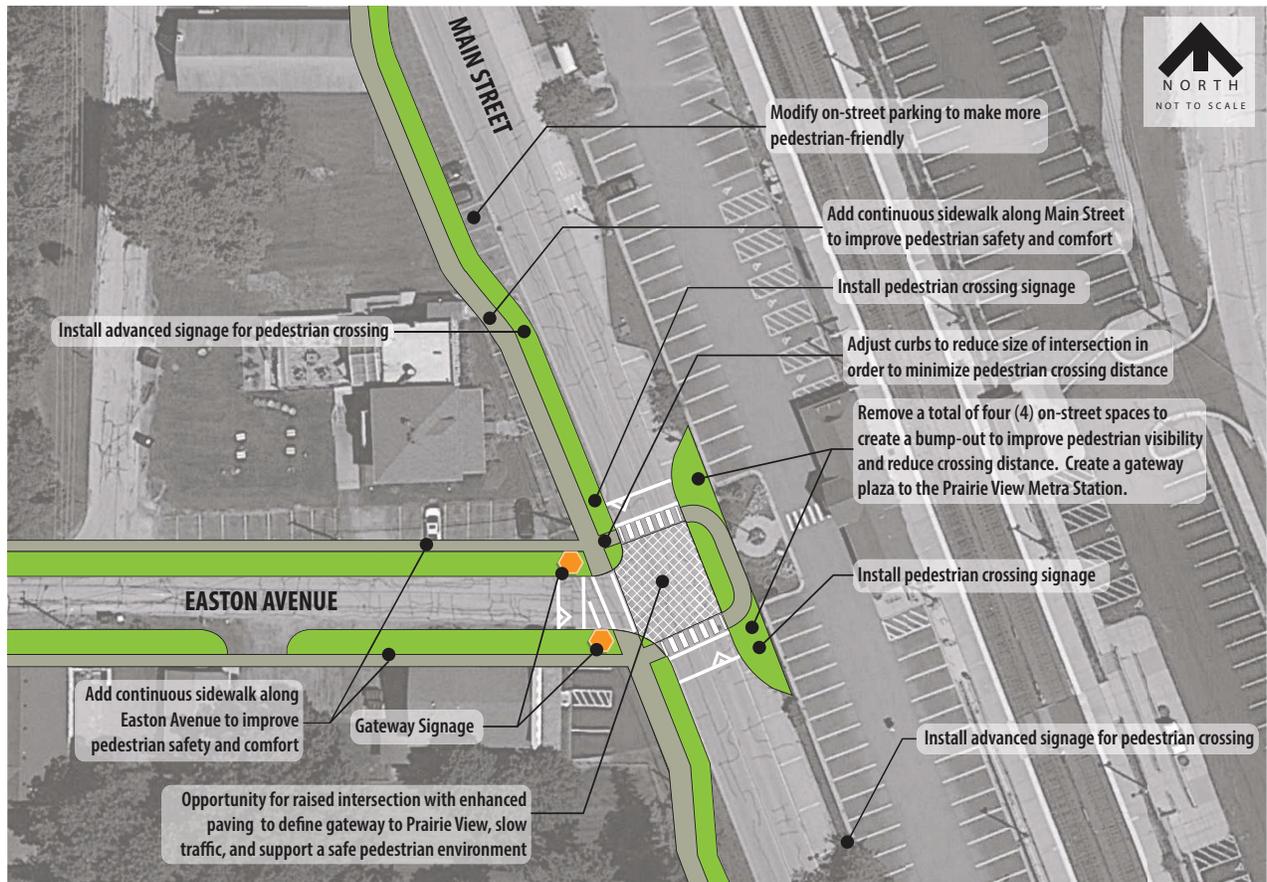
On-street parking should be provided adjacent to commercial and mixed-use developments. These parking spaces should be time restricted in order to promote healthy turnover of spaces for customer and visitor convenience. Employee parking should be prohibited on-street unless otherwise permitted by the Village while customer/visitor parking availability is prioritized. As (re) development occurs in the Central Subarea, the Village should explore opportunities to create short-term parking opportunities (e.g., 15-minute parking spaces for pick-up/drop-off activity) and loading zones in order to provide parking options for quick convenience-oriented trips and curbside loading activities for transportation network companies (TNCs), such as Uber and Lyft.

### Pedestrian Improvements

In general, pedestrian connectivity in the study area is discontinuous with sidewalk provided along some segments. The Village should prioritize continuous sidewalk through the study area as part of future (re)development. As property is (re)developed, the Village should require installation of continuous sidewalk across the property frontage, and as appropriate, to complete nearby gaps or connections. Alternatively, a fee-in-lieu contribution should be provided as part of the (re)development in order to allow the Village to install the sidewalk at a later date as part of a comprehensive roadway improvement project.

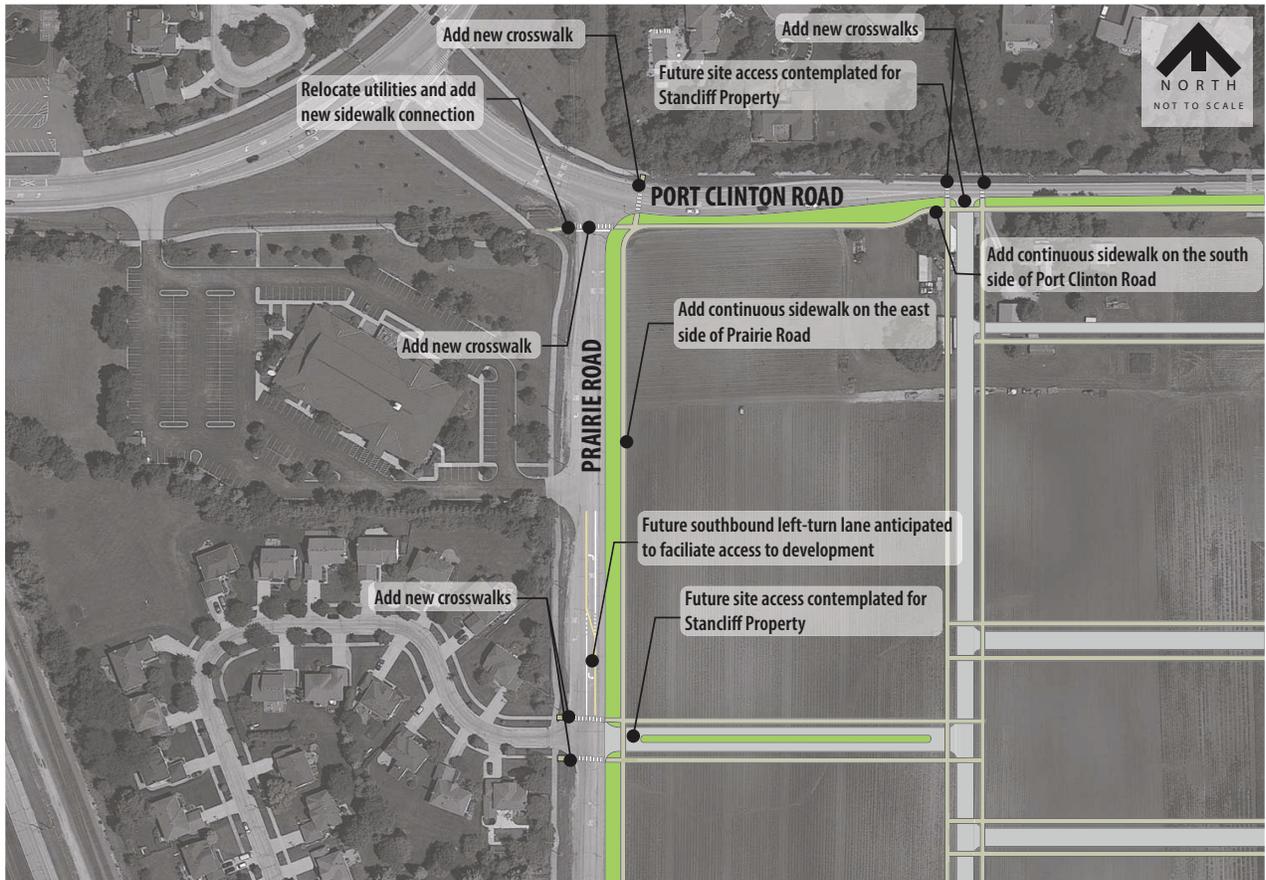
A continuous sidewalk network will improve access and overall connectivity for pedestrians. Sidewalk gaps should be filled with a minimum five-foot wide sidewalk. A landscaped parkway should be provided in order to buffer pedestrians from the adjacent street. In order to prioritize pedestrians in the study area, sidewalks should be delineated through access driveways. Redevelopment should incorporate new sidewalk installation at gap locations. In addition to new sidewalk, site design should consider opportunities for pedestrian accommodations, including sidewalk connections between the public sidewalk and the commercial or mixed-use development and outdoor seating areas or amenitized public places (e.g., parks, open spaces, plazas, courtyards, streetscape, etc.). Buildings should be positioned to define the street edge and incorporate pedestrian-scaled architectural details in order to enhance pedestrian safety and comfort.

Pedestrian improvements should also be provided at key intersections within the study area in order to enhance mobility, safety, and comfort. A summary of the pedestrian improvements identified for intersections within the study area are summarized in the following sections:



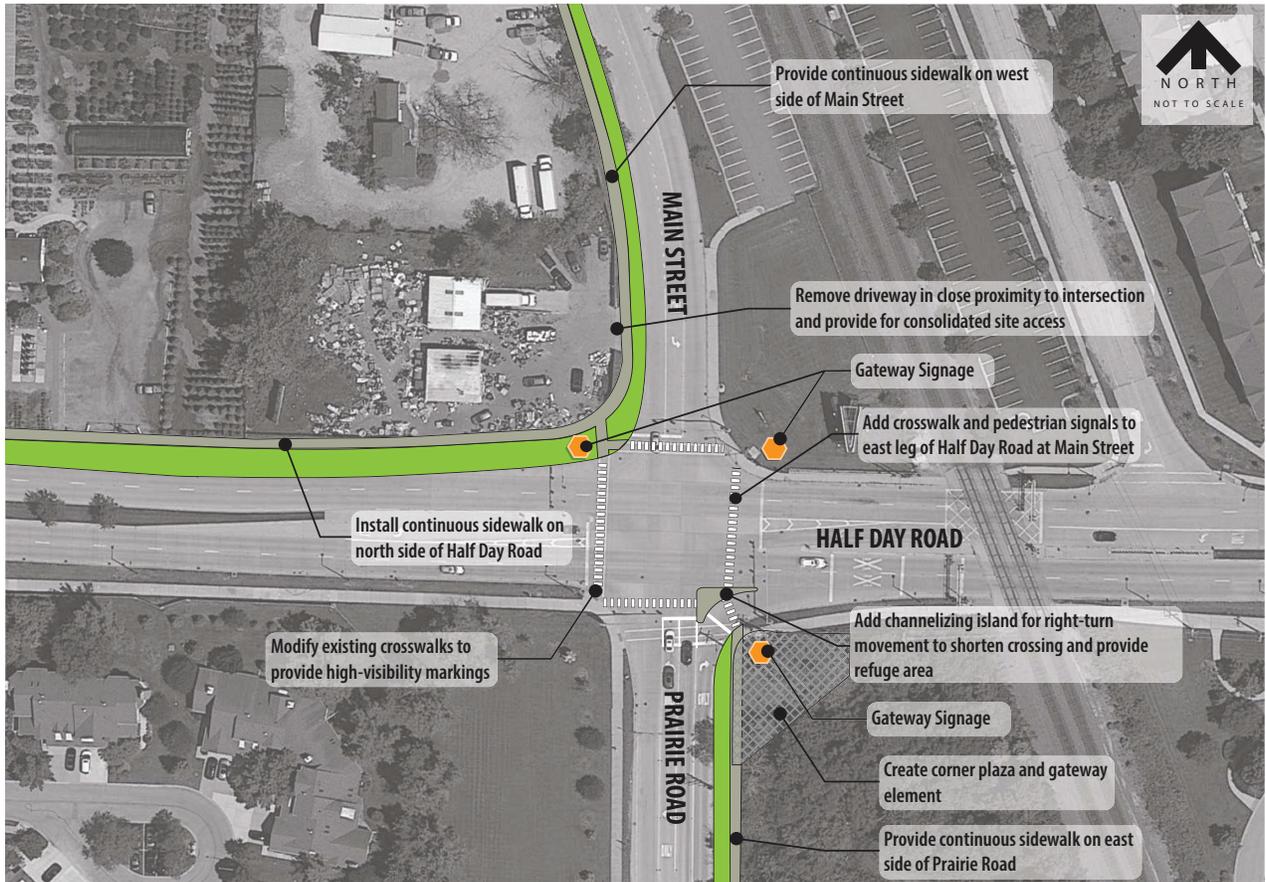
### Main Street / Easton Avenue

This intersection will continue to serve as a key gateway to the Prairie View area. Based on its adjacency to the Metra Station and close proximity to the residential and commercial uses planned in the area, striped crosswalks should be provided on both the north and south legs of Main Street at its intersection with Easton Avenue. In tandem with the crosswalks, a curb bump-out should be provided on the eastside of the street in order to reduce the crossing distance and improve visibility between pedestrians and motorists. The curb bump-out introduces an opportunity to provide a landscaped area further enhancing this intersection as a gateway. Additional intersection improvements include raising the intersection to slow traffic and emphasize the pedestrian environment and to incorporate an enhanced intersection paving treatment that improves the character, differentiates the intersections as a focal point, and contributes to the gateway opportunity. A summary of the recommended improvements is highlighted in the adjacent graphic.



### Prairie Road / Port Clinton Road

A striped crosswalk is recommended on Prairie Road at its intersection with Port Clinton Road. With the shared-use path on the south side of Port Clinton Road, the crosswalk would enhance connectivity and increase motorist awareness of pedestrian and bicyclist activity. Existing utilities on the southwest corner of Prairie Road/Port Clinton Road would require relocation in order to accommodate the sidewalk connection to the crosswalk. Additional crosswalks have been identified for the vicinity of the intersection in order to increase mobility and connectivity for existing and future area residents. A summary of the recommended crosswalk enhancements is depicted in the adjacent graphic.



### Half Day Road / Main Street / Prairie Road

The southeast quadrant has been identified as a redevelopment opportunity site. In order to facilitate pedestrian connectivity between North Prairie Road and the Prairie View Metra Station and Central Subarea, adding a striped crosswalk is recommended on the east leg of Half Day Road/Main Street/Prairie Road. High-visibility crosswalks are recommended on all four legs of this intersection. Based on the size of this intersection, a channelized island is recommended for the northbound right-turn movement in order to reduce new crosswalk distances and provide a pedestrian refuge area to/from the southeast corner of the intersection.

### Bicycle Improvements

The Village of Buffalo Grove has an extensive bicycle path network. In the vicinity of the study area, existing bicycle paths are provided on Port Clinton Road, Half Day Road, Buffalo Grove Road, Main Street, and Prairie Road. The existing bicycle paths will remain with the proposed improvements to the area. As part of the planned improvements the existing shared-use path on the north segment of Main Street should be extended south to Half Day Road. In addition, a shared-use path should be provided on the south side of Port Clinton Road across the Stancliff Property frontage.

In order to facilitate bicycle connectivity to the future redevelopment areas, site design should consider connections to the bicycle network and provide for convenient on-site bicycle parking.



## FUTURE BICYCLE PATH NETWORK

### Legend

- Study Area Boundary
- Village of Buffalo Grove Municipal Boundary
- Proposed Roadway Alignment
- Prairie View Metra Station
- N Prairie Road
- Existing Bicycle Path
- Planned Bicycle Path
- Planned Shared-Use Path





Source: Flickr | Jimmy Emerson, DVM



Source: Flickr / Amy Smith



Source: <https://bestoklahomahomes.com>



Source: City of Madras & brewpublic.com



Source: Corbin Designs



Source: Flickr / Robb Edge

## ENTRY NODES & GATEWAY

Gateways into the Prairie View Station Area communicate to visitors that they have arrived in an environment that values pedestrians and supports a high-quality multi-modal environment. The proposed gateways at the Prairie View Station area are at the following intersections:

- W Half Day Road and N Main Street
- N Main Street and N Buffalo Grove Road
- W Half Day Road Willow Parkway

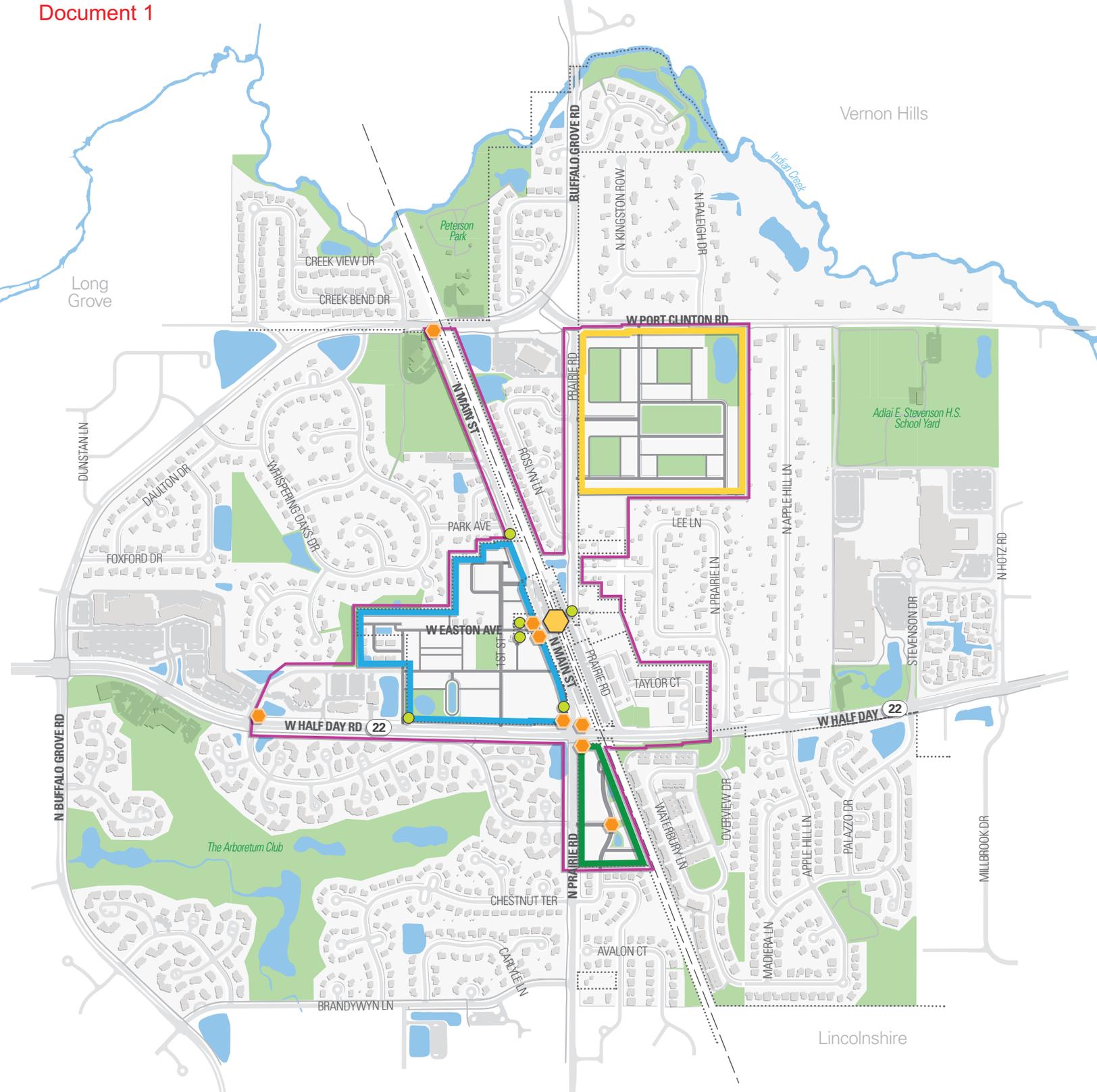
These intersections are considered entry points into Prairie View. Smaller, neighborhood-scale retail/commercial development is proposed along N Main Street with a transition into more residential land uses at W Easton Avenue and W Brockman Avenue. Gateway signage should be designed to provide a sense of arrival and sense of place.

## WAYFINDING

A legible environment helps orient travelers and communicates organization and structure in Prairie View. Wayfinding includes environmental signage and cues in the physical design of the public realm that support users comfortable navigation of a specific place. In addition, the road design helps travelers navigate through the different land uses to support ease of circulation. Public art can be installed at key locations to give residents and visitors orientation and educational information about their location relative to the Prairie View Station Area. Wayfinding signage can be installed at appropriate locations within and around Prairie View, in coordination with gateway signage, to communicate major destinations to travelers, parking lots, bicycle routes, and trails nearby.

The wayfinding signage should conform to a uniform design standard. Wayfinding best practices include:

- Provide a minimum amount of information on a sign.
- Mapping on the sign should be simplified.
- Present information logically.
- Use fonts and graphic elements that are easy to read. Bold colors and simple text are encouraged.
- Place so the view of the sign is not obstructed by other elements.
- Be sure that signs are mounted, they have font sizes and information that are appropriate to the mounting height.
- Locate signs ahead of where turns need to be made.
- The overall system must be cohesive.



## GATEWAY & WAYFINDING

### Legend

- Study Area Boundary
- Village of Buffalo Grove Municipal Boundary
- North Subarea
- Central Subarea
- South Subarea
- Gateway Signage
- Wayfinding
- Prairie View Metra Station



## INFRASTRUCTURE

The Village currently operates and maintains a vast network of public infrastructure, including stormwater, utilities, roadways, and sidewalks. In order to accommodate the future (re)development opportunities identified for the Prairie View Station Area, the existing infrastructure network will be expanded. A comprehensive plan to address future infrastructure construction, operation, and maintenance is recommended. A comprehensive plan will 1) provide for a connected system; 2) minimize disruptions to existing residents and commuters; and 3) optimize use of Village funds for capital improvements, operations, and maintenance.

### Stormwater

Future (re)development in the Prairie View Station Area will be subject to the Village of Buffalo Grove Development Ordinance and the Lake County Watershed Development Ordinance. Based on a review of topographical data available from Lake County, potential areas to be allocated for stormwater management have been incorporated into the concept plans for the Central Subarea, North Subarea, and South Subarea. The specific dimensions and design details for the stormwater management facilities will be subject to Village review as part of the (re)development plan.

Maintenance of the future public stormwater infrastructure is anticipated to be managed through the Village's Stormwater Management System. Under the Stormwater Management System (effective January 1, 2016), the Village would collect a bi-monthly Stormwater Utility Fee. The fee is based on the parcel size, land use, impervious area, and the funds required to maintain and update the stormwater infrastructure. Through this system, the Village would operate and maintain existing and future stormwater infrastructure including, but not limited to, storm drains, pipes, ditches, curbs, gutters, culverts and open stream channels.

### Public Utilities

The Village of Buffalo Grove currently operates and maintains the following water and sanitary systems:

- 136 miles of sanitary sewer pipe, force mains, manholes, catch basins, and inlets
- 12 sanitary lift stations
- Water supply from Lake Michigan, purchased from Evanston and delivered through the Northwest Water Commission
- 4 emergency back-up aquifer wells

As (re)development is proposed within the Prairie View Station Area, the Village will require abatement of existing well(s) and septic systems (as applicable) and connection to the Village's water and sanitary system.

Under the Village's current practice, the developer would be obligated to extend the water and sanitary mains and provide a service connection(s) to the subject (re)development site. Based on the infrastructure currently available in the Prairie View Station Area, extension of existing water and sanitary mains may be extensive. Phased (re)development may provide for reduced utility extension costs and minimize disturbance to area residents.

In order to plan for future (re)development in the Prairie View Station Area, a comprehensive assessment of existing and future water and sanitary sewer capacity is recommended in order to determine future system needs. The analysis of future conditions should reflect the anticipated residential and commercial densities shown on the concept plans prepared for the Central Subarea, North Subarea, and South Subarea.

Based on a previous analysis completed in 2006 by Hancock Engineering, sanitary sewer extension is anticipated as follows:

- **Central Subarea** – Extend sanitary sewer on Easton Lane north of Half Day Road and/or the sewer at the intersection of Half Day Road/Willow Parkway
- **North Subarea** – Extend existing sanitary sewer from Roslyn Lane
- **South Subarea** – Extend sanitary sewer from intersection of Prairie Road/Willow Parkway

## Private Utilities

As (re)development plans are reviewed for the Prairie View Station Area, the Village should work in coordination with property owners, developers, and utility companies to bury existing overhead utility lines. The Village should continue to review the placement of transformers and switch boxes in order to minimize their appearance.

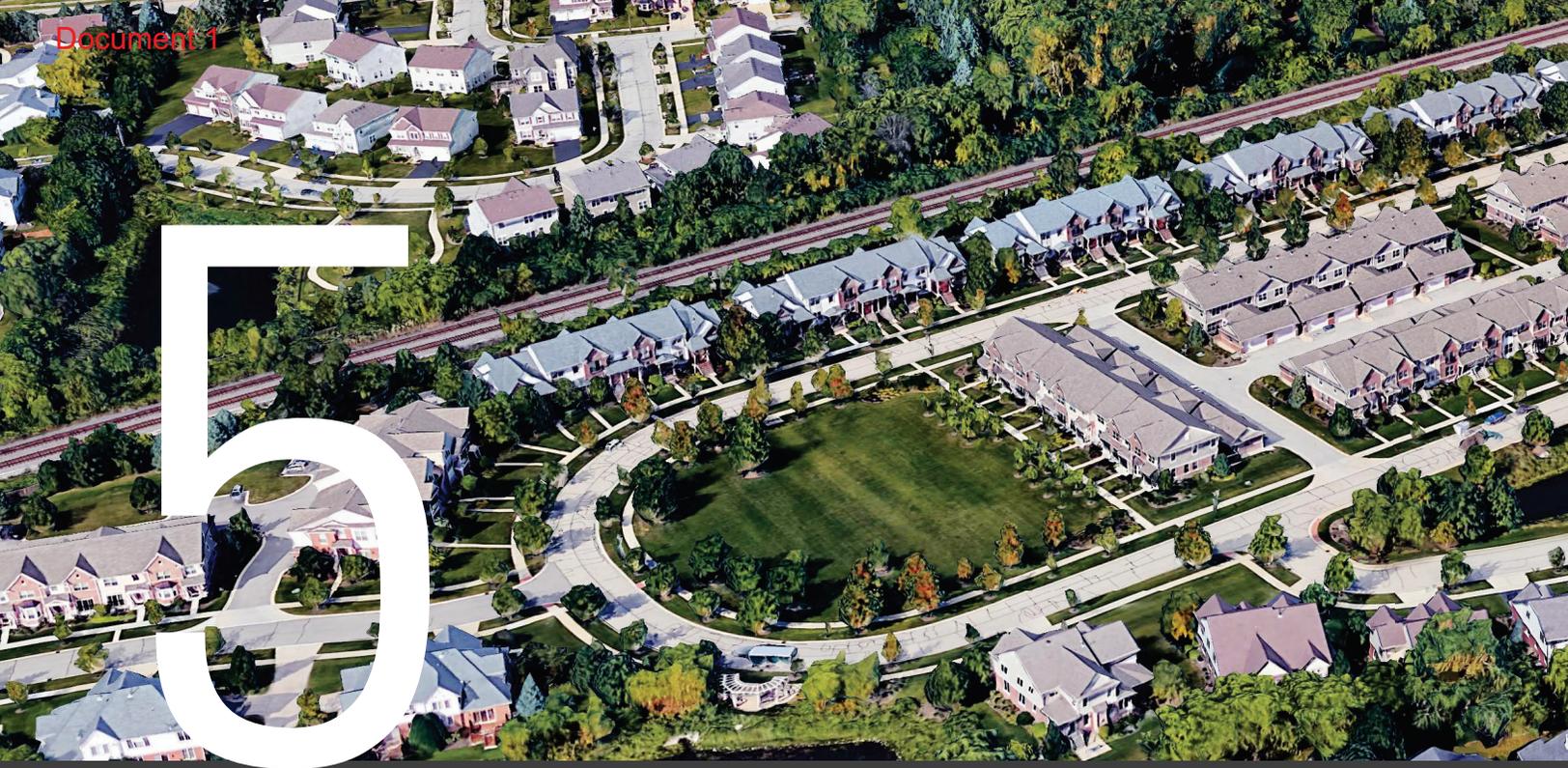
## Transportation

The existing and future multimodal transportation network will be designed to support the needs of residents, businesses, commuters, and visitors within the Prairie View Station Area. Multimodal improvements identified for the study area are defined earlier in this chapter. Dedication of right-of-way will be required in order to accommodate the future roadways, including but not limited to 1st Avenue and New Street. As (re)development proposals are submitted to the Village, right-of-way dedication should be required in order to complete the 60-foot cross-section where applicable. Installation of the streets, sidewalk, and parkway should be designed and constructed in coordination with public utilities.

## Implementation

Several options exist for infrastructure investments in the Prairie View Station Area. Based on the significant infrastructure needs in the study area, a comprehensive approach to design and construction should be considered as properties are assembled. In order to provide for a comprehensive approach to infrastructure implementation, the following financing options could be considered by the Village:

- **Impact Fees** – An impact fee is a charge assessed by a public agency against new (re)development that generates revenue to fund the master improvements or recover capital costs.
- **Bonds** – A bond may be assessed for future public improvements. Note that the assessment of an impact fee is one way to repay the bond. A bond could also be repaid through implementation of an increased utility fee on a monthly utility bill or through a utility rate increase.
- **Special Service Area (SSA)** – A special service area is a taxing mechanism that can be used to fund a range of infrastructure improvements and services (e.g., storm sewers, sanitary sewers, roadway improvements, sidewalks, curbs, lighting, etc.) in a defined geographic area.
- **Tax Increment Financing (TIF)** – This financing tool allocates future increases in property taxes within a defined area to pay for local capital improvements. All property within the TIF district is subject to the tax based on a common rate. Establishment of a TIF district does not increase property taxes; this tool reallocates how taxes are used. TIF funds may be allocated to public improvements (e.g., roadways, streetscape) and development activity (e.g., property assembly or land acquisition).
- **Recapture Agreements** – Through the use of recapture agreements, the Village could require a developer to pay for the extension of public infrastructure necessary to support the proposed (re)development. If the infrastructure serves additional users associated with the future (re)development of adjacent property, a recapture agreement is executed. Based on the recapture agreement, the developer would be reimbursed for a pro rata portion of the improvement cost. Recapture agreements are an effective tool to manage the overall cost of public infrastructure; however, the upfront cost to the developer may be an obstacle to initiating (re) development within the Prairie View Station Area.



# Open Space & Sustainability

Sustainable strategies are incorporated in each subarea and project theme to ensure the longevity, health and wellness of Prairie View and its residents. Strategies include social, economic and physical opportunities to lower costs, create value and maintain a high quality of life for residents. Efficient utilization of the area's resources contributes to the character and identity of Prairie View. Open space is prioritized in greenfield areas, utilized as functional green infrastructure where detention is required, and incorporated into developments as a public space amenity.

## LANDSCAPE & STREETSCAPE DESIGN STRATEGY

Based on the input received throughout the planning process, there is a strong desire to incorporate the natural elements of Prairie View and promote the concept of a conservation community. The landscape and streetscape strategy responds to this desire with the incorporation of landscaped parkways on existing and proposed streets within the Prairie View Metra Station Area. The reduction of curb-cuts provides longer stretches of uninterrupted and planted parkway without impeding vision or safety of travelers.



Additional street trees can be planted to increase Prairie View's existing tree canopy and will include both ornamental and shade trees. Integration of green infrastructure projects, such as rain gardens and stormwater planters, can be accommodated as part of the Village's larger environmental and green infrastructure strategy. Areas of proposed stormwater detention can include additional green infrastructure elements to mitigate runoff and flooding problems as well as incorporating special planting palettes to differentiate these places.



## GREEN INFRASTRUCTURE TOOLS

Green infrastructure tools assist with stormwater management by slowing the rate of runoff into the stormwater sewer system as well as contributing to improved water quality through natural filtration of runoff. Rain gardens, bioswales, stormwater planters, water harvesting, and permeable pavement are the possible green infrastructure tools identified in this Plan. Other tools may be selected in the future as conditions and needs change. The tools vary in their purpose, design, requirements, and costs. Parameters that need to be considered when choosing the appropriate tool include:



Source: Flickr/MSU Infrastructure Planning and Facilities Facility Information Services



Source: Flickr/cara ruppert



Source: Flickr/Center for Neighborhood Technology

### Raingardens

Rain gardens, also known as bioretention or bioinfiltration, are designed to divert a portion of the initial runoff from the storm sewer system and allow it to reenter the system through an underdrain or leave the system at a designed infiltration rate. They are shallow depressions with engineered soils and various deep-rooted plantings and usually include an overflow to the storm sewer system.

### Bioswales

Bioswales serve a combination of infiltration and conveyance functions. Unlike traditional drainage swales or ditches, they include significant vegetation as well as engineering soils to slow the conveyance rate of runoff so that some of the runoff can be infiltrated or removed through evapotranspiration prior to entering the storm sewer system. Bioswales fit well in longer, linear spaces.

### Stormwater Planters

Stormwater planters are considered urban rain gardens by the EPA. They include engineered soils and overflow piping systems, but they are designed as streetscape elements. The soil level is lower than the sidewalk and street curb so that runoff can be collected and detained in the planter.



Source: Flickr/Philadelphia Water Department



Source: Flickr/Craig T



Source: Flickr/Stanley Zimny

### Permeable Pavers

Permeable pavement allows infiltration of rainwater through the pavement. This lowers the amount of runoff that is delivered to the storm sewer system. Permeable pavement can include paver systems, porous asphalt, or pervious concrete.

### Water Harvesting

Water harvesting practices promote productive reuse of rainwater by capturing and storing rainwater on-site for a variety of uses, such as toilet flushing and irrigation. Water harvesting can be achieved by either downspout disconnection or installing rain barrels. The former involves diverting runoff from the roof to on to the property's yard for irrigation. The latter involves capturing and storing rainwater

### Tree Planting

Tree planting provides a multitude of benefits, in terms of both a tree-by-tree basis and of larger scale like an urban tree forest. Trees assist in reducing stormwater runoff by intercepting rainfall and help soil better store rainwater. It also can assist in groundwater recharge. When trees are properly placed, they can provide shade, which helps reduce energy consumption to cool a home, and they can block winds that increase the need for indoor heating. Trees are vital carbon sinks, absorbing carbon dioxide and air pollutants.



# Implementation

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To implement the Vision set forth in the Plan, the Village should undertake the following recommendations and action steps. These recommendations and action steps can be grouped under four main categories:

- Village Commitment
- Leverage Market Opportunities
- Create a Neighborhood Identity and Brand
- Enhance Sustainability and Quality of Life

## VILLAGE COMMITMENT

The Village of Buffalo Grove should provide its commitment and support to the Plan by undertaking regulatory, communication, and planning initiatives as well as anticipate and support infrastructure improvements that create an enhanced network throughout the Prairie View Metra Station Area.



**Recommendation 1:  
Continue to engage with the Buffalo Grove community, property owners, and other stakeholders about the Plan.**

**Action Steps:**

- A. Make the plan available online and advertise the availability and contact information of Village staff to address questions and comments.
- B. Meet with residents, businesses, key property owners, Vernon Township, Metra, Lake County, institutions, and others in the Prairie View Area to review the Plan, discuss future improvements, considerations, and work together for a shared future.

**Recommendation 2:  
Integrate the Prairie View Metra Station Area Plan into long-term planning documents including the comprehensive plan and the Lincolnshire/Buffalo Grove Boundary Agreement.**

**Action Steps:**

- A. Amend Buffalo Grove’s current Comprehensive Plan (2009) with the Prairie View Metra Station Area Plan.

- B. Integrate the Prairie View Metra Station Area Plan as part of the forthcoming Comprehensive Plan and establish the Prairie View Metra Station Area as one of the several key sub-areas in the community.
- C. Revisit the Lincolnshire/Buffalo Grove Boundary Agreement with the Village of Lincolnshire and update the agreement to reflect the vision set forth in the Plan.

**Recommendation 3:  
Explore policies to support and advance future annexation opportunities.**

**Action Steps:**

- A. Identify and prioritize development sites that can encourage the extension of utilities to help reduce the utility connections for existing residential parcels.
- B. Promote the expansion of Village utilities to encourage annexation into the Village. Explore tools such as special service areas to help offset the costs of extending utilities into the unincorporated areas.
- C. Determine alternate sites for existing businesses that may want to relocate to other locations in the nearby area

## **Recommendation 4: Explore amending zoning and associated building and design regulations to help achieve the vision.**

### **Action Steps:**

- A. Consider amendments to the zoning ordinance or utilize existing zoning tools to help implement the Vision. Specific actions may include:
  - A1. Considering a form-based code or form-based elements.
  - A2. Introducing overlay zones and/or creating a better defined Planned Unit of Development (PUD).
  - A3. Incorporating multi-generational housing elements such as accessory dwelling units as well as aging in place.
- B. Align vision with current parking standards. Specific actions may include:
  - B1. Evaluate and potentially modify parking minimum requirements to align with the Plan.
  - B2. Partner with Vernon Township to explore potential shared parking opportunities the Prairie View Metra Station.
  - B3. Identify opportunities for complementary or alternate uses of the commuter parking lots during non-peak commuter periods (e.g., farmer's market, neighborhood festival, shared parking for nearby restaurants and businesses).
- C. Review and potentially modify the sign code to ensure consistency with the Plan.

## **Recommendation 5: Support an improved road network.**

### **Action Steps:**

- A. Review development and redevelopment plans for the inclusion of an improved road network consistent with the Plan.
- B. Explore jurisdictional options for the ownership.
- C. Ensure future development provides the necessary right-of-way (ROW) dedication needed to accommodate for future improvements.
- D. Monitor changes in emerging technology, policy, and requirements related to autonomous vehicles and use of shared mobility options to inform design of (re)development and both public and private infrastructure.

## **Recommendation 6: Plan and Budget for Capital Improvements.**

### **Action Steps:**

- A. Modify and update, as needed, the Capital Improvement Plan (CIP) to reflect infrastructure needs as outlined in the Plan.
- B. Perform financial analysis to determine potential options for funding improvements. Options may include but not exclusive to pay- as-you go, debt financing, payment-in-kind.

## LEVERAGE MARKET OPPORTUNITIES

The Village of Buffalo Grove should leverage market opportunities to spur development/redevelopment within the Station Area Plan that aligns with the Plan.

### **Recommendation 7: Communicate and market the Plan to the development community to convey Buffalo Grove's vision and commitment to the area's future.**

#### **Action Steps:**

- A. Continue to meet with those interested in selling and redeveloping the properties in the area and present and discuss the Plan.
- B. Target brokers and other representatives who could best understand and deliver the character and residential and retail needs unique to this area. Look to attract neighborhood-scaled commercial and retail uses.
- C. Showcase the Plan at developer industry events.

### **Recommendation 8: Explore Economic Development Incentives.**

Development/redevelopment sites may have challenges that require economic development incentives to encourage development based upon fiscal impact and return on investment analyses. If appropriate, Buffalo Grove should explore such opportunities and size them appropriately. A list of common economic development incentive tools includes:

#### **Common Economic Development Incentives**

- Tax-Increment Financing (TIF) District
- Special Service Area (SSA)
- Business District (BD)
- Property Tax Abatement
- Sales-Tax Sharing

#### **Action Steps:**

- A. Evaluate the need and potential for economic development incentives to facilitate projects that implement the vision and ensure market competitiveness.
- B. Consider conducting feasibility studies for specific economic development tools including but not limited to Tax-Increment Financing (TIF) District and/or Business District.
- C. Establish a team that would include legal, financial, and development specializations to carry out the evaluation and development (if needed) of incentives.

### **Recommendation 9: Target retail and restaurant businesses and other services that are compatible with the vision for Prairie View Area and are suitable for its market.**

#### **Action Steps:**

- A. Target retail and restaurant neighborhood businesses that are oriented towards commuter and residential needs as well as destination businesses. Potential niches could be towards health and fitness – particularly women-focused wellness, specialty foods, local food incubators, as well as other drinks and food establishments.
- B. Work collaboratively with neighborhood businesses on leveraging existing assets such as nearby Stevenson High School, which allows open campus during lunch hours for upper grades and is an employment center.

## CREATE NEIGHBORHOOD IDENTITY AND BRAND

Create an authentic identity and brand for the area that reflects the history, character and uses in the area.

### Recommendation 10: Draw upon the area's history to help establish an identity and "sense of place."

#### Action Steps:

- A. Explore retaining existing buildings such as the Westminster Reformed Presbyterian Church and former Vernon Legion Post 1247 Building.
- B. Utilize the historical architecture as an inspiration for design and character for future buildings and development, wayfinding, and branding.
- C. Consider providing an indoor or outdoor dedicated space that would be devoted to showcasing the Prairie View's history.

### Recommendation 11: Create a Prominent Unified Streetscape and Design.

#### Action Steps:

- A. Explore developing unified design criteria for the area and incorporating the recommendations into Buffalo Grove's Appearance Plan. This should include design guidelines illustrating the Village's desired architectural characteristics of non-residential and residential buildings which can be inspired by the area's historical buildings.
- B. Review the future plans for the inclusion of landscaping, lighting, benches, gateway signage, and wayfinding signage that provide a unified design.

### Recommendation 12: Incorporate civic, community, institutional and art in spaces that can enhance the identity and local culture of the Prairie View Metra Station Area.

#### Action Steps:

- A. Work with Stevenson High School on utilizing space in the Prairie View Area for academic, extracurricular, and/or administrative purposes.
- B. Collaborate with Buffalo Grove Park District and Vernon Township to program and activate open spaces and plazas.
- C. Explore opportunities to display public art to help activate public spaces through collaboration with local partners.

### Recommendation 13: Establish a Brand for the Prairie View Metra Station Area.

#### Action Steps:

- A. Develop a brand for the Corridor that would represent its uses and importance to the Village.
- B. Integrate this brand throughout the Corridor especially in signage and other gateway features as well as in marketing materials.

## ENHANCE SUSTAINABILITY AND QUALITY OF LIFE

The Village of Buffalo Grove should look to promote sustainability and quality of life in the Prairie View Area.

### Recommendation 14: Enhance the Area's Walkability.

#### Action Steps:

- A. Review all future plans for an inclusion of a comprehensive sidewalk/pedestrian path system that connects to the surrounding neighborhoods and the Village's larger sidewalk network.
- B. Develop a unified streetscape treatment within the Corridor consisting of evenly spaced right-of-way trees, pedestrian scale lighting, shrubbery and hedges, flower beds, way-finding signage, and other pedestrian-oriented improvements.
- C. Strengthen and enhance crosswalks throughout the area to improve pedestrian orientation and safety.
- D. Emphasize active transportation and transit-supportive design as part of Village review of future (re)development concept plans (e.g., curbside designations, pedestrian-scale character.)

### Recommendation 15: Improve Bicycle Accessibility.

#### Action Steps:

- A. Ensure all proposed plans complete the existing bicycle network in the Prairie View Metra Station Area and connect with larger existing bicycle networks, parking, and other infrastructure.
- B. Provide appropriate signage and maps to for trail connections and networks.

### Recommendation 16: Collaborate with partners on improving the Metra North Central Service (NCS) line's ridership and frequency of service.

#### Action Steps:

- A. Continue to serve on the Steering Committee for the RTA Community Planning Program North Central Service Corridor Analysis and Implementation Plan. This Plan, which is guided by a steering committee of municipalities along the NCS line, aims to identify the major investments needed to accommodate more service along the NCS line.
- B. Educate businesses and residents about transportation connections from the Prairie View Metra Train to places of residence or employment, which includes shuttle services such as Transportation Management Association of Lake-Cook Shuttle Bug, Chariot Shuttle as well as carpooling options such as SPLT and Pace Vanpool.

### Recommendation 17: Optimize open space opportunities as they arise.

#### Action Steps:

- A. As redevelopment and development occurs, look for development plans to provide open space, in large open parks, tot lots, small plazas, and/or other types of open space.
- B. Work collaboratively with partners such as Vernon Township, Buffalo Grove Park District, and Stevenson High School, on ideas for shared open spaces and programming.

### Recommendation 18: Encourage sustainable development, stormwater management and other best practices.

#### Action Steps:

- A. Promote energy efficiency programs such as Illinois Homeowner Performance with ENERGY STAR and Come Ed's Net Metering.
- B. Encourage green infrastructure as a strategy for stormwater management with a focus on detention areas, parks, and right of way plantings.
- C. Select a landscaping and planting palette that is representative of plant types that thrive in the Midwest and are low-maintenance.

## VILLAGE COMMITMENT



- Engage Community and Property Owners
- Amend Comprehensive Plan and Boundary Agreement
- Support Future Annexation
- Amend Zoning Regulations
- Improve Road Network
- Capital Improvement Planning
- Enhance Street Grid

## LEVERAGE MARKET OPPORTUNITIES



- Market Plan
- Explore Incentives
- Target compatible businesses

## CREATE A NEIGHBORHOOD IDENTITY AND BRAND



- Create Unified Streetscape and Building Design
- Incorporate Civic, Community, and Art Uses
- Establish Brand

## ENHANCE SUSTAINABILITY AND QUALITY OF LIFE



- Expand Walkability
- Improve Bicycle Accessibility
- Collaborate to improve Metra Train Service
- Optimize open space
- Encourage sustainable practices

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# Appendices

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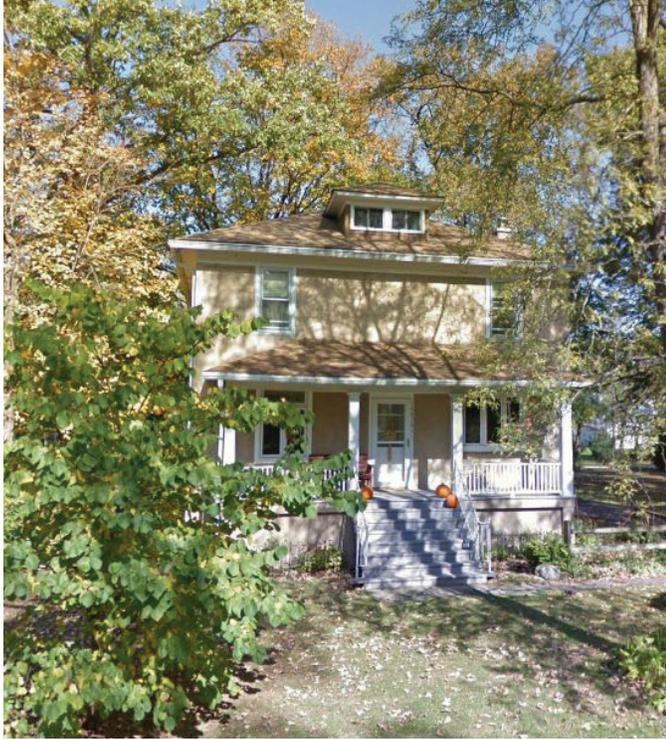
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# TPOLOGY #1: THE AMERICAN FOURSQUARE

Location: Zone 5

Uses: Residential, Commercial

## Architectural Typology Analysis



## FEATURES

The American Foursquare is one of the most dominant styles of home from the early 20th century. These homes feature a compact, square plan, are two-and-a-half stories in height, and topped with a simple roof accentuated by a central dormer.

These houses developed in reaction to the overly ornate Victorian homes of the late 1800s and eschew excess detail in favor of handcrafted "craftsman" woodwork.

**Years Constructed:** circa 1890 - 1930

**Construction Type:** Wood Framed or Brick

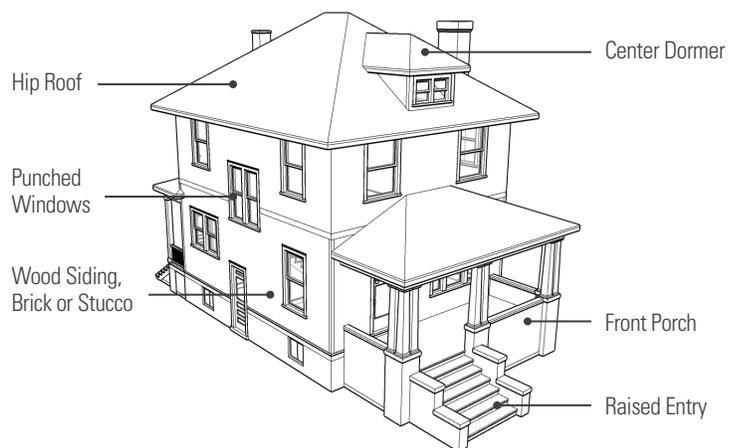
**Exterior Materials:** Wood Siding, Brick or Stucco

**Use:** Single Family Residential, later Commercial

**Height:** Two and a half Stories

**Parking:** One-Two Cars, Detached, in rear (potentially added later)

**Special Features:** Front Porch (potentially enclosed for addition)



# TPOLOGY #2: POST-WAR BUILDING BOOM

Location: Zone 4

Uses: Residential



## FEATURES

The suburban building boom following World War II established the ranch-style house, and its variants the Raised Ranch and Split-Level as the dominant housing styles of the latter half of the 20th century.

Its long, low profile reflected American desires for wide-open spaces, with an open layout that embraces a more informal living style. The “model home” design method allowed for rapid construction to suit soaring demand.

**Years Constructed:** circa 1940s - 1970s

**Construction Type:** Wood Framed

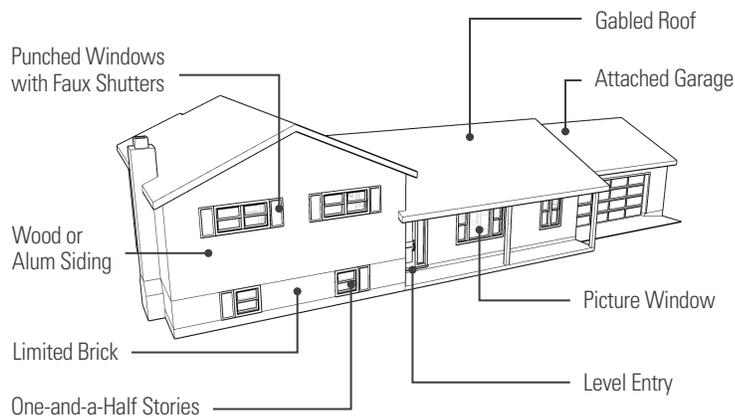
**Exterior Materials:** Limited Brick, Wood or Aluminum Siding

**Use:** Single Family Residential

**Height:** One Story, One-and-a Half Stories

**Parking:** One-Two Cars, Attached, in Front / Detached, in Rear

**Special Features:** Raised Ranch / Split Level allowed for large windows into basement levels, Bay Windows, Picture Windows



# TPOLOGY #2: POST-WAR BUILDING BOOM

Location: Zone 4

Uses: Residential



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**Construction Type:** Wood Framed

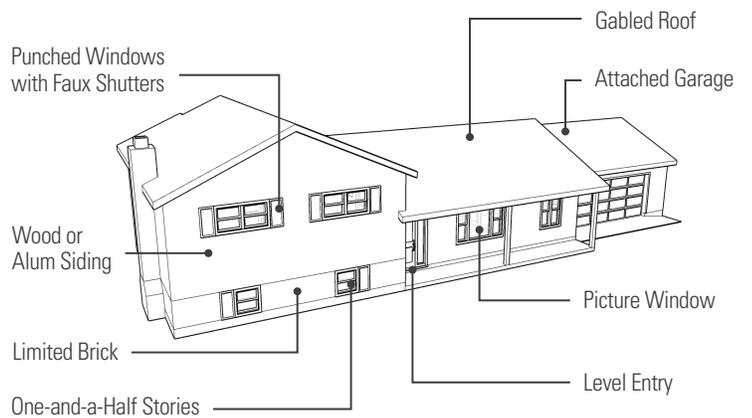
**Exterior Materials:** Limited Brick, Wood or Aluminum Siding

**Use:** Single Family Residential

**Height:** One Story, One-and-a Half Stories

**Parking:** One-Two Cars, Attached, in Front / Detached, in Rear

**Special Features:** Raised Ranch / Split Level allowed for large windows into basement levels, Bay Windows, Picture Windows



# TPOLOGY #3: LATE 20TH CENTURY SINGLE FAMILY HOME

Locations: Zone 1. Zone 3

Uses: Residential



## FEATURES

Towards the end of the 20th century and into the 21st century, residential construction turned toward a contemporary version of American Revivalism known as Neo-Eclecticism. These homes combine a wide array of decorative techniques from an assortment of traditional styles, resulting in a more complex aesthetic than the simple post-war residence.

Much of the revivalist elements are exclusively decorative, while the informal, casual interiors and construction methods of the American Ranch remain. Highly pitched, complex roofs often combine a variety of styles and features including gables, hips, dormers, and special accents.

**Years Constructed:** circa 1980s - 2000s

**Construction Type:** Wood Framed

**Exterior Materials:** Brick, Stone, Vinyl Siding, EIFS

**Use:** Single Family Residential

**Height:** One - Two Stories

**Parking:** Two-Three Cars, Attached, Front or Side

**Special Features:** Complex Rooflines, Dormers, Prominent Entrance



# TPOLOGY #4: 21ST CENTURY PAIRED HOME

Locations: Zone 6

Uses: Residential



## FEATURES

The 21st Century has seen a revision on the Neo-Eclectic Single Family Home that focuses on smaller living and reduced maintenance, popular with both "Empty Nest"-ers and young families with fewer children.

Many of the styles and features of the larger sibling remain, but with two units combined into a single structure, separated by a party wall. Smaller lot sizes over increased density and walkability, with less lawn area to maintain. Often part of a larger planned development, greater aesthetic harmony exists across several homes

**Years Constructed:** circa 2000s

**Construction Type:** Wood Framed

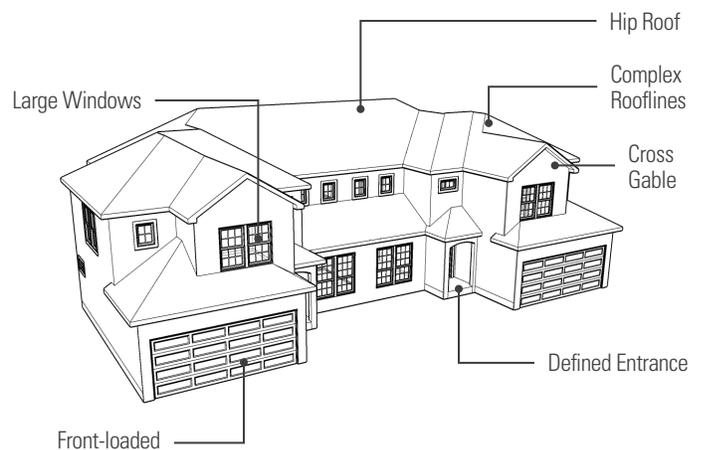
**Exterior Materials:** Brick, Stone, Vinyl Siding, EIFS

**Use:** Multi-Family Residential

**Height:** One - Two Stories

**Parking:** Four Cars, Attached, Front (2 per unit)

**Special Features:** Complex Rooflines, Dormers, Prominent Entrance



# TPOLOGY #5: NEW URBANIST TOWNHOME

Locations: Zone 2

Uses: Residential



## FEATURES

Continuing the trend towards denser living and less emphasis on the automobile, this model crafts a next iteration of the 21st century duplex, combining 5-6 units into a single structure. Parking is moved to the rear of the house, offering a more traditional curb appear less dominated by the car.

Arranged to emphasis walkability, the houses are sited closer to the street, and consistent aesthetics across entire neighborhoods offers a picturesque, traditional scene.

**Years Constructed:** circa 2010s

**Construction Type:** Wood Framed

**Exterior Materials:** Brick, Stone, Vinyl Siding, EIFS

**Use:** Multi-Family Residential

**Height:** One - Two Stories

**Parking:** Two Cars, Attached, Rear

**Special Features:** Rear patio framed by attached garages, visual consistency.





## RATIO

Project:	Prairie View Station Area Plan
RATIO Project No.:	18021
Date/Time:	August 2, 2018, 6pm
Purpose:	Existing Conditions Report Review and Goal Setting
Held at:	Village of Buffalo Grove, Village Hall
Participants:	Jim Kedroski William Hoffman Frank Cesario Chris Stilling Dane Brags Allison Buchwach Darren Monico Nicole Woods Eric Smith Lesley Roth, RATIO Daniel Tse, RATIO

The Steering Committee met to discuss the findings in the Existing Condition Report and to create goals for the Station Area Plan. RATIO led the Steering Committee through a presentation that included the updated project schedule, project objectives, a summary of the input gathered to date from the planning process, land use analysis, transportation analysis, infrastructure analysis, and market analysis.

Based on the findings during the analysis phase of the project and documented in the Existing Conditions Report, key themes were presented to the Steering Committee that will guide the creation of the Draft Station Area Plan in the next phase of the project. The key themes are:

- Architectural Character
- Transportation Network
- Fiscal Impact/Development Scenarios
- Sustainable Strategies
- Character and Identity

### Architectural Character

The architectural character of the Prairie View Metra Station Area will include a general understanding of the historical and cultural significance of the existing building typologies. Currently, there are several types of buildings including residential, institutional, office, and commercial. Each of these building typologies have unique characteristics and elements that contribute to the overall identity of the Station Area. The goals for architectural character focus on creating a cohesive physical framework that has longevity and is timeless. The goals also provide a framework for developers to understand the Village's vision for the Station Area. The architectural character of Prairie View includes development of appropriate housing typologies that respond to the area's changing demographic; creation of a neighborhood/district center; increased density in appropriate areas; and identification of infill opportunities and unincorporated property.

**Goal 1:** The core area of the district should incorporate the Metra Station and leverage the 20% of ridership that walks to the train.

**Goal 2:** Build on the historic building's unique characteristics and architectural elements to create a unique Station Area identity. Architecturally significant buildings in the district include the Prairie House Building, Church (gothic, classic style), and American Legion building.

**Goal 3:** New architecture should consider a variety of building heights and materials while being sensitive to maintaining pedestrian scale along the street.

**Goal 4:** The Main Street and Route 22 intersection is a natural gateway into the Station Area and development on this high-visibility corner should express a cohesive identity for the district.

**Goal 5:** Create a mix of housing types throughout the district that are transitional, flexible and able to accommodate young families, aging in place, multi-generational, home office, and young couples.

**Goal 6:** Utilize rooftops for additional outdoor activity space in addition to at-grade shared outdoor space.

### Transportation Network

The Prairie View Station Area includes great access to the nearby freeway system, Half Day Road and Metra to western suburbs and Downtown Chicago. Prairie View is differentiated from the Buffalo Grove station in that it is located within the heart of a residential district with historical significance to the Village. The district is walkable and is less dense than the Buffalo Grove station. The roadway network is also more rural in nature and the right of way has an opportunity to incorporate modern best practices to create a complete street that fosters multi-modal options. The goals for the transportation network prioritize the design of roadways that are multi-modal and accessible to the population of Prairie View and are intuitive for visitors. The goals illustrate opportunities for more efficient circulation systems that move traffic and people to the destinations of choice. Differentiation of modes is also considered with regard to the walkability of the district and high number of Metra commuters that originate from Prairie View.

**Goal 1:** Create continuity of the existing street grid and sidewalk system.

**Goal 2:** Assess lot depths of businesses along Main Street and identify designated loading and delivery areas at the rear of the buildings.

**Goal 3:** Shared parking at the Metra station should be studied further in coordination with improving connectivity east and west of the Metra rail tracks.

**Goal 4:** Work with area employers to coordinate an integrated transportation system that includes both public and private modes.

### Fiscal Impact/Development Scenarios

The alternative development scenarios will test the market analysis recommendations and provide concept designs that create value in the physical environment. These scenarios are tailored to Prairie View and are sensitive to its context within the Village of Buffalo Grove. Each redevelopment area offers opportunities for investment and impacts the area's economic contributions. Development should also satisfy the Village's long-term goals identified in relevant past plans and regulatory documents. Goals for the development scenarios consider the fiscal impact on the Village and balance resident needs with market realities.

**Goal 1:** Consider a partnership with the school district in Prairie View that extends engagement and educational opportunities for students.

**Goal 2:** Leverage Prairie View's high concentration of young families to create highly amenitized housing and neighborhoods.

**Goal 3:** Encourage a mix of residential, office and retail/commercial building types that prioritize clustered and compact development with highly-developed shared outdoor space and pedestrian-focused public realm improvements.

### Character and Identity

The character and identity of the Station Area is an important element in developing a desirable destination within the Buffalo Grove. Prairie View has a unique character that is grounded in its history and legacy within the Village. It also plays an important role in the lives of generations of residents who call Prairie View home. This character and identity are incorporated into the development scenarios and strengthens the district as a whole.

**Goal 1:** Prioritize local, stand-alone businesses that are destinations for Buffalo Grove residents and offer amenity to Prairie View residents, the nearby school population and visitors.

**Goal 2:** Create a gathering space with all-season programming.

**Goal 3:** Create streetscape and landscape design guidelines that unify Prairie View, including the Metra station, and promote its focus on natural landscapes.

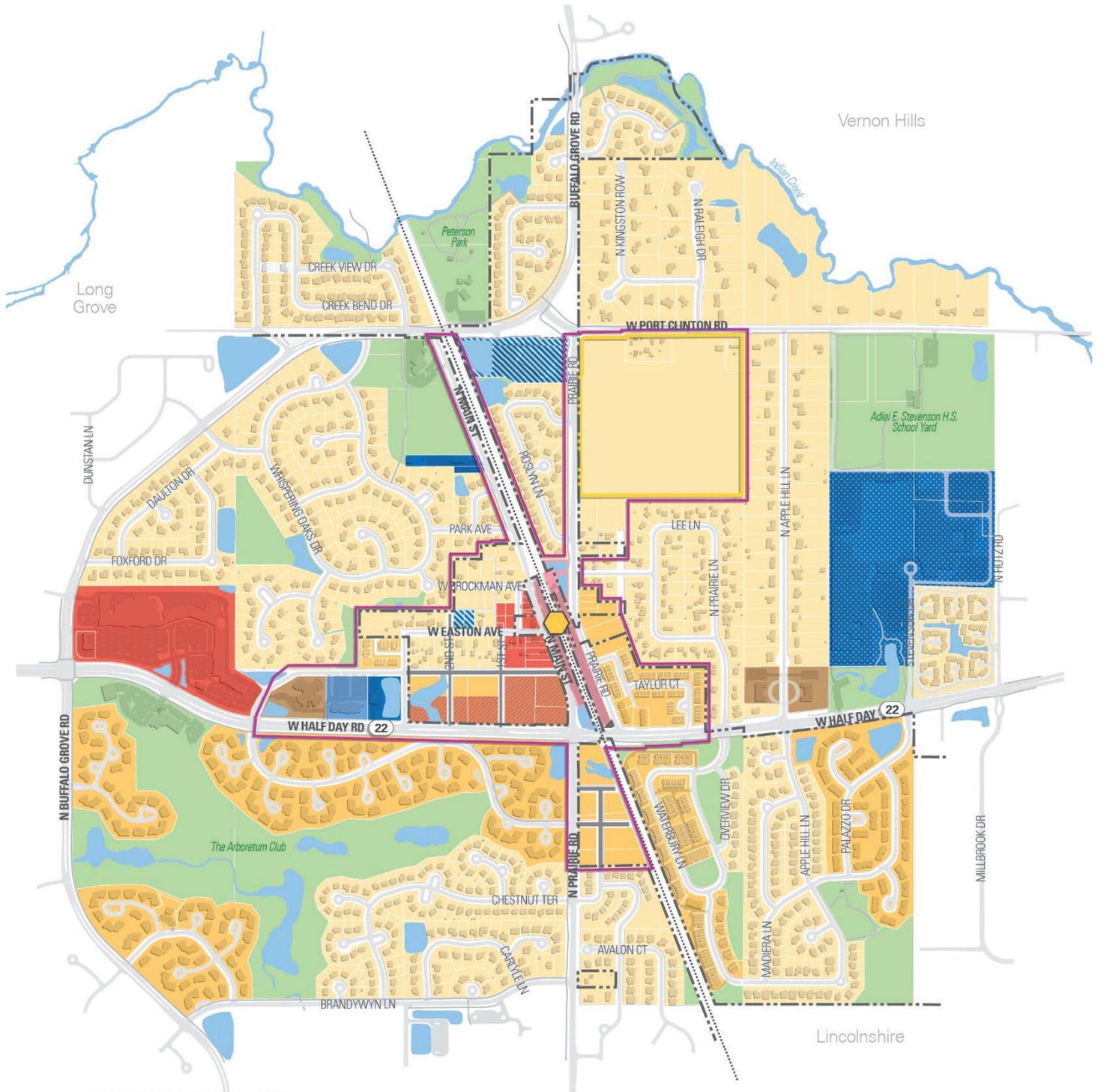
### Sustainable Strategies

To ensure the longevity, health and wellness of Prairie View and its residents, sustainable strategies will be incorporated in each concept alternative and project theme. Strategies will include social, economic and physical opportunities to lower costs and create value in construction and the natural environment. Efficient utilization of the area's resources contributes to the character and identity of Prairie View and preserves its natural elements.

**Goal 1:** Preserve Prairie View's natural resources and unique landscapes by creating landscape zones within the study area that are both functional and add to the identity of the district.

**Goal 2:** Identify appropriate areas for density and greater development intensity in areas that can accommodate it.

**Goal 3:** Conduct an analysis energy savings using renewable energy sources and create pilot demonstration projects to educate homeowners on its potential.

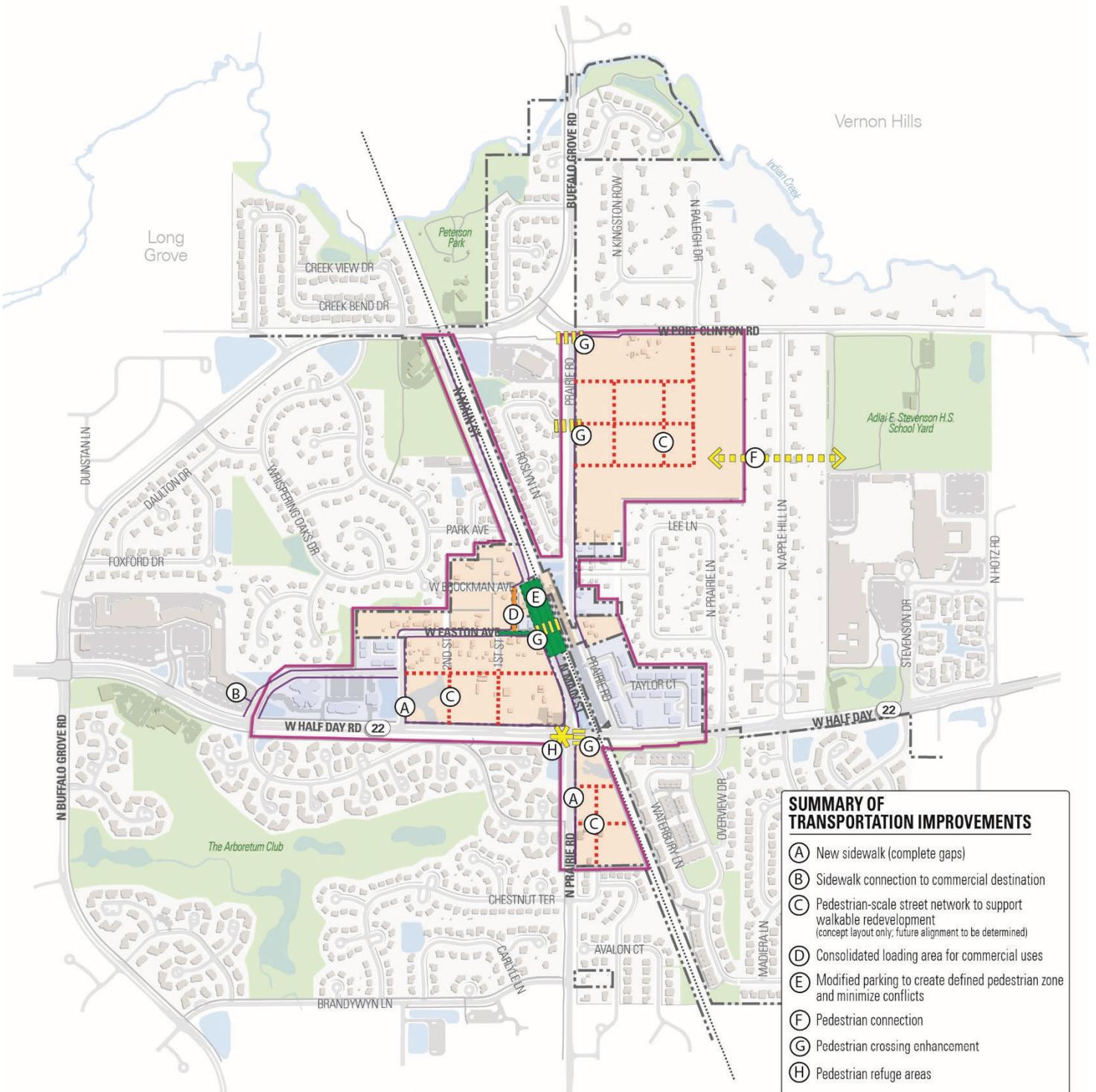


**FUTURE LAND USE**

**Legend**

- Study Area Boundary
- Village of Buffalo Grove Municipal Boundary
- Prairie View Metra Station
- Potential Roadway Connection
- Stancliff Farm (40 acres)
- Single-Family Detached
- Single-Family Attached (ex: townhomes)
- Multi-Family (ex: apartments / condominiums)
- Commercial
- Mixed-Use (commercial & multi-family apartments)
- Medical
- Education
- Government
- Religious
- Parks/Open Space
- Transportation







Architecture

Preservation

Interior Design

Landscape Architecture

Urban Planning &amp; Design

To: Project Team, Village of Buffalo Grove

RATIO Project No.: 18021

Date: May 30, 2018

Subject: Prairie View Metra Station Plan – Public Workshop #1 Outreach Summary

On May 10, 2018, the RATIO team and Village of Buffalo Grove planning staff hosted a workshop to solicit public input on the strengths, weaknesses, opportunities, and threats of the area surrounding the Prairie View Metra Station. This is the first public workshop for the Prairie View Metra Station Plan and was held at the Arboretum Club (401 Half Day Rd, Buffalo Grove, IL 60089). Over 90 participants attended, including: residents, stakeholders, business leaders, Steering Committee members, Village staff, and elected and appointed Village officials. The workshop was structured as an informal interactive event. The workshop consisted of five stations where participants shared their thoughts and opinions during the 2-hour workshop. These stations were designed to optimize input from the public, while gaining meaningful input into preferences and address concerns. The five stations included:

- Station 1 – Welcome
- Station 2 – Visioning
- Station 3 – Be the Planner
- Station 4 – Area Destinations
- Station 5 – What Makes A Great Place?

The results and input gathered from the public pertaining to each station are summarized in the following sections.

### Station 1 - Welcome

The first station included a staffed sign-in and welcome table. Participants then proceeded to watch a brief presentation that was displayed on a monitor. The presentation provided an overview of the planning process and the public workshop, in addition to the purpose of the Prairie View Metra Station Plan. Snacks and refreshments for participants to enjoy were provided.





**Station 2 – Visioning**

The second station was a topical visioning exercise that was designed to create a vision statement for the Station Plan. Participants were engaged in an activity where a series of cards with various images were available at the station for review. Participants picked a card that symbolizes their vision for the Prairie View Metra Station area. After selecting the card, each participant spoke with the facilitator and other participants about their vision for the Prairie View Metra Station area. The consultant staff facilitated this exercise and recorded the results on a flipchart. The bullet points below are the participant’s vision for the Prairie View Metra Station area that were recorded on the flipchart (the numbers next to each vision indicates how many times a vision was repeated by multiple participants):



**3 Votes**

- Restaurant
- Café/Bakery
- Quaint/charming

**2 Votes**

- Smaller scale development / keep area density
- Add sidewalks
- Natural landscapes
- More shops – boutiques (unique)
- Better train service

**1 Vote**

- Add weekend train service
- Walkable
- Unique homes
- Improved streetscape
- Shopping
- School capacity
- Reuse of homes
- Preserve Prairie View
- No parking (students)
- No noxious (industrial) uses
- No junkyard
- No condos
- More parks and gardens

**1 Vote, cont.**

- More homeowners
- More compact
- Main St. needs traffic-calming
- Complete the last mile -> train to home
- Keep SFH in boundary agreement
- Keep historic homes
- Inclusive of township
- Improve traffic flow (Eastbound on Rte 22)
- Ice cream shop
- Higher home values
- Greenery
- Future focused development
- Farm to table
- Family
- Evaluate historic significance
- Downtown needed
- Dark skies
- Cultural heritage center
- Creative space
- Country feel
- Connect bike paths
- Capitalize on train commuters
- Bucolic
- Best practices
- Affordable housing (\$200k-\$300k)



### Station 3 – Be the Planner

The third station invited participants to ‘Be the Planner’ for the Prairie View Metra Station area. A board was provided that showed the study area’s existing conditions. Participants were given foam blocks, which represented potential building massing, and were asked to place the blocks on the board to create their ‘vision’ of what the Prairie View Metra Station area should look like in the future. During the workshop, there were many discussions among participants regarding future (re)development within the study area. The following summarizes the main points gathered from the discussions:

- Most of the participants were very vocal about opposition to development around Prairie View. Many pointed out previous plans that didn’t meet with the community’s expectations and ultimately weren’t implemented.
- Prairie View has a strong identity within the community however, some participants asked the question “why do we need development or additional amenities here if I can get it in neighboring communities such as Long Grove/Lincolnshire, etc.”
- Participants voiced concerns about the impact of additional development on Stevenson school. Over-crowding is perceived to be a problem at Stevenson. New development could increase pressure on this desirable resource.



### Existing conditions in the Prairie View Station Area

- Existing traffic patterns, specifically turning movements, are problematic, particularly during rush hour.
- Student traffic in and out of Stevenson creates congestion twice a day.
- Neighbors have observed increased cut-through traffic in their neighborhoods from travelers wishing to avoid congestion along major arterials.
- Strategies to mitigate traffic around Prairie View have created additional challenges. For example, there is no left turn permitted from Prairie Road to eastbound Half Day Road.
- Current parking is a challenge at certain times of day around the Prairie House. Metra Parking is by permit, not available after hours, and participants wondered if this can be improved.



### What would a future Prairie View look like?

- The community’s historic and unique character should remain. However, there were contradicting viewpoints.
  - Some participants expressed a preference for narrow roads with overhanging trees, without sidewalks.
  - Some participants believed there could be higher-density development around the rail line.
- Main Street has potential to be a more robust commercial corridor with the following elements:
  - Community amenities
  - Experiential retail
  - No chain or franchise establishments
  - Multi-story mixed-use (not family-residential)
  - Walkable
  - Low rise, mixed use
  - Arts District
  - Restaurants (some participants asked how many the community and the market can support)
  - Boutique-scale retail
  - Experiential retail
  - Incubator-type commercial
  - Lower-scale
  - More affordable than



### Station 4 – Area Destinations

This station involved two maps where participants used colored stickers to identify destinations, gateways, and other important design features on the map. Participants were also asked to identify their homes and places they regularly frequented in the 1-mile radius. One map illustrated a ¼ mile (5-minute walking distance) and the other illustrated a ½ mile (10-minute walking distance) circle around the station area. Both map boundaries reached 1-mile from the station area. Each color sticker represented a theme, as follows:

- **Yellow:** Home
- **Red:** Commercial
- **Green:** Recreational
- **Light Blue:** Institutional

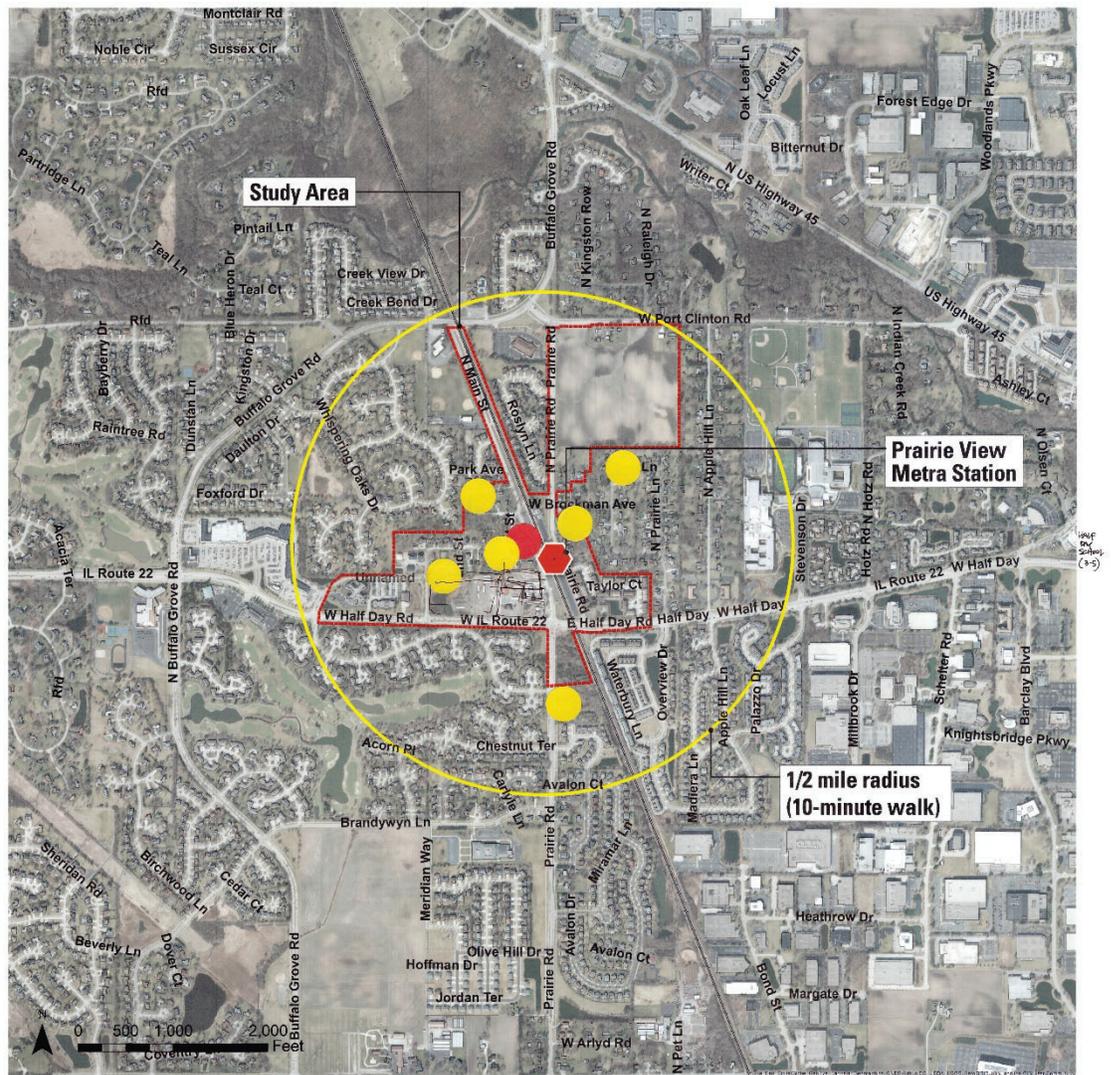


The station facilitator asked the participants to write on the maps any other ideas they had that were not necessarily site-specific or represented by the colored stickers. The following graphics show the features the participants identified.

# AREA DESTINATIONS



Identify nearby destinations (ex: schools, restaurants, stores), gateways, and other important features around the Prairie View Metra Station.

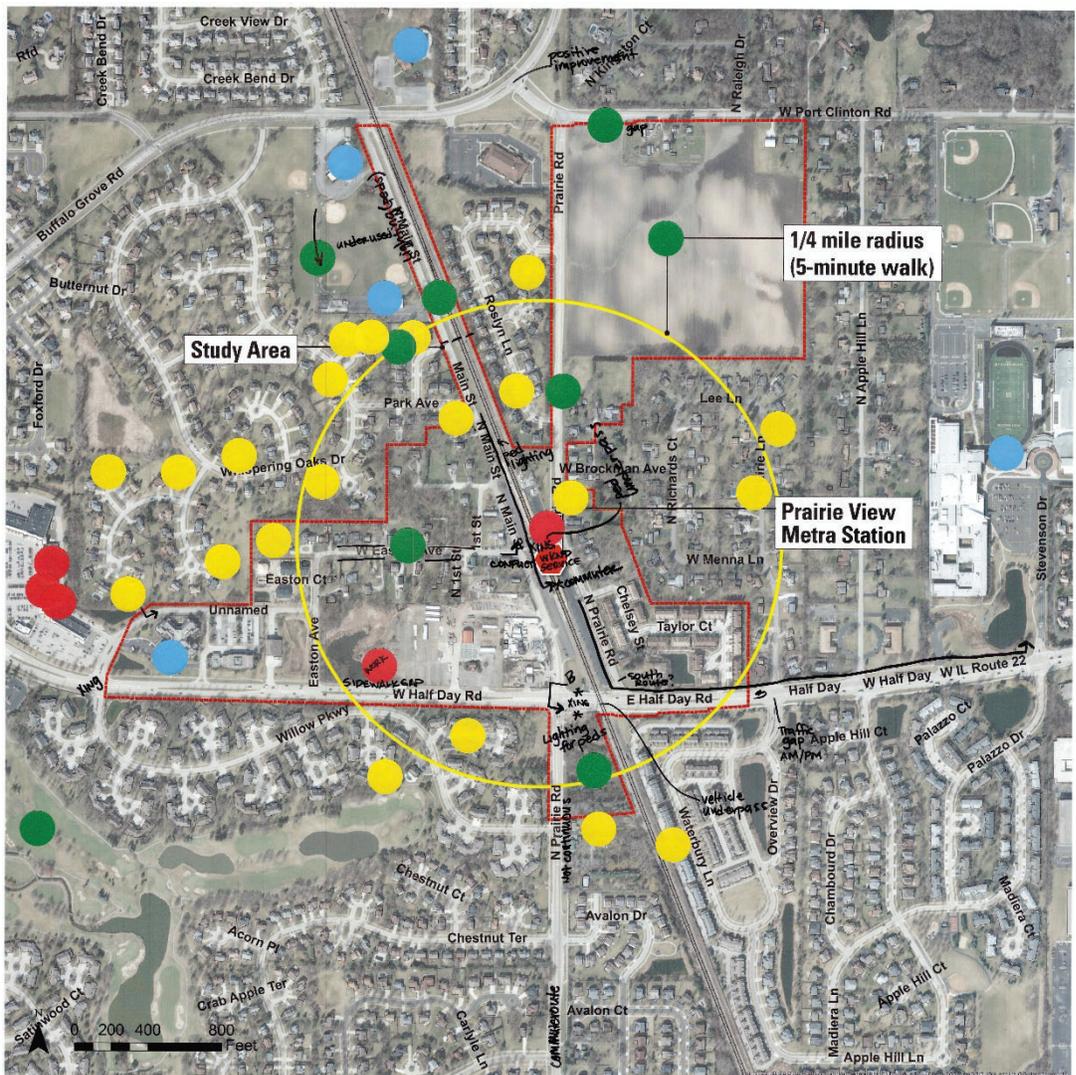


- HOME
- RECREATIONAL
- COMMERCIAL
- INSTITUTIONAL

# AREA DESTINATIONS



Identify nearby destinations (ex: schools, restaurants, stores), gateways, and other important features around the Prairie View Metra Station.



- HOME
- COMMERCIAL
- RECREATIONAL
- INSTITUTIONAL
- (WALK NEARBY SCHOOL)



**Station 5 – What Makes A Great Place?**

At this station, participants selected words they felt answered the question ‘What Makes a Great Place?’. The station facilitator documented each participant’s selection. Participants were encouraged to write new words if those provided did not capture their vision. The following summary shows the descriptive words that participants selected. The number next to the words indicate the number of times a word was re-selected among different participants. Words with stars (\*) are descriptive words written by participants. It should be noted that some of the words participants wrote occurred throughout public workshop. Some participants who completed the exercise earlier may not have seen some of these words.

**20 Votes**

Walkable

**15 Votes**

Family Friendly

Dining Options

**14 Votes**

Bike Lanes

**10 Votes**

Festival/Events

Accessible

**9 Votes**

Safe/Protected

**7 Votes**

Streetscape

Residential Neighborhood

Nature Based

**6 Votes**

Recreation

Destination

**5 Votes**

Heart of Community

Entertainment

Diverse

**4 Votes**

Retail District

\*Rural

**3 Votes**

Multi-Generational

Cultural Heritage

Celebrate History

**2 Votes**

\*Was Affordable

\*Separated

\*Quaint

\*More Metra Service

\*Lower Taxes

\*It Was A Community with Roots

\*Better infrastructure (roadways)

**1 Vote**

\*Downsizing in the area

Vibrant

Public Art

Convenient

\*Quiet

\*Park

\*Less Rules

\*Housing for Senior

\*Dark Skies





## Steering Committee / Kick-Off Meeting (April 12, 2018) Meeting Summary

## MEMORANDUM

### RATIO

To: Nicole Woods, Village of Buffalo Grove  
 RATIO Project No.: 18021  
 Date: April 20, 2018  
 Subject: Prairie View Station Area Plan – Kick-off Meeting

The following constitutes the meeting minutes from the Prairie View Kick off meeting on April 12, 2018 from 5.30-7pm. The meeting was held at Village Hall.

The meeting began with a presentation by the RATIO team.

1. Welcome & Introductions
  - a. Area Opportunities
    - i. Interesting opportunity with some of the lowest density in the community abutting some of the highest density in the community
    - ii. While the METRA station is certainly an economic driver of the area, the activity generated by Stevenson High School may be an even greater driver
  - b. METRA Station
    - i. Owned by the Village Township
      1. Some beautification projects have been planned, but the larger planning effort seems to be a much more exciting long-range project
    - ii. Prairie View has 2<sup>nd</sup> highest ridership on that line (BG main is 1<sup>st</sup>)
2. Planning Process Overview
  - a. 7-Month Project Plan
    - i. Stage 1: Project Initiation
      1. Opportunity for stakeholders to engage with the planning process – will be the ultimate ambassadors of the final Area Plan
    - ii. Stage 2: Data Collection
    - iii. Stage 3: Draft Station Area Plan
      1. Project has a Village-hosted website that is currently live.
        - a. Will be a critical tool for communicating the process to the public
    - iv. Stage 4: Final Station Area Plan
  - b. Plan Objectives
    - i. Objective 1: Provide a Framework for the Communities Aspirations
      1. First public workshop being planned for May 10<sup>th</sup>
        - a. A number of stations to orient participants to the process, establish a vision, and give people an opportunity to get hands-on with concepts
    - ii. Objective 2: Integrate Previous Plans to Strengthen Community Integrity
      1. The Village shouldn't have to be repeating efforts from previous planning work
    - iii. Objective 3: Respond Quickly and Support Activation of the Station Area
    - iv. Objective 4: Build Consensus through Targeted Outreach with Key Stakeholders
  - c. Placemaking
    - i. Identifying PPP opportunities or other creative financing options
    - ii. Public Realm and Streetscape Improvements
    - iii. People Places
      1. Current not much to keep people in the Prairie View area – how do we change this? Should we change it?
    - iv. Architectural Representation
      1. Establish some sense of what sort of development / character is appropriate for the district
      2. Massing, aesthetic, articulation
      3. Establish an expectation for what should be developed
    - v. Graphic Communication
  - d. Steering Committee Roles and Responsibilities

Nicole Woods  
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April 23, 2018

- i. Offer thoughts and comments
      - ii. Be the eyes and ears of the community – express valid concerns and issues
      - iii. Help build support for the goals, objectives, and policies in the plan
      - iv. Help build awareness of the Station Area Plan
        - 1. Economic environment has totally changed since the last time a plan was established for Prairie View
  - e. The Market
    - i. Focus on local and regional position
    - ii. Create a neighborhood center
    - iii. Leverage “Catchment Area”
    - iv. Understand Rider Needs / Destination Retail
  - f. Mobility
    - i. Multi-modal transit connectivity
    - ii. Pedestrian & Bike Access & Safety
    - iii. Roadway Alignment & Sidewalk Continuity
      - 1. Our reconnaissance showed a lot of walkers / bikers – how do we more appropriately accommodate them with vehicular traffic
      - 2. Sidewalk gap analysis
    - iv. Accommodate Emerging Technology
      - 1. What can a road do other than just move cars?
        - a. Stormwater
  - g. Infrastructure
    - i. Study area includes non-annexed parcels as well as areas under shared municipality agreements
  - h. Project Implementation
    - i. This plan needs to facilitate results
    - ii. Phasing
    - iii. Identification of project partners
    - iv. Early developer engagement
- 3. 2007 Station Area Plan wasn't well received at the time
  - a. The imagery of the plan was thought to be too urban and not representative of Buffalo Grove
  - b. The economic realities have shifted the perspective
    - i. The need to mixed use is far more accepted as critical to overall success
    - ii. Density is critical, though not necessarily the end-all, be-all
  - c. Recent success with the Lake-Cook corridor has revitalized interest in successful planning density
- 4. Visioning
  - a. A vision should be **place based, future focused** and **aspirational**
  - b. A vision should resolve area dichotomies and bring ideas together cohesively
  - c. Station Area Identity
    - i. What is your favorite thing about the Station Area?
      - 1. Walkability – one of the few areas of Buffalo Grove that has true walkability
        - a. The only bar in Buffalo Grove that you can walk home from
          - i. Prairie View Tavern is very neighborly – you see people you know
      - 2. One of the view “public areas” in the community
        - a. The “front porch” – maybe not a huge economic driver, but looks best
      - 3. Woodland Commons – restaurants, shopping, services, grocery
      - 4. High density housing opportunities that are unique to the area
      - 5. Well positioned in the community – high connectivity
      - 6. Opportunity to assemble large, high-impact development opportunities
      - 7. Hometown / small-town feel
        - a. Walkability
        - b. Quaintness
        - c. Sense of Place
      - 8. Potential to be a unique main street identity for Buffalo Grove

Nicole Woods  
Page 3  
April 23, 2018

9. Bill enjoys watching people coming and going from the station
  - a. 200 people running to the 7:22 AM
  - b. Easton Ave is Prairie View – everyone knows everyone’s business
10. Wonderful sense of small-town community
  - a. Safe, welcoming, what a great place to live
  - b. Very neighborly
11. Most of the communities along the line developed into industrial / office areas near the Metra line
  - a. Prairie View stayed a walkable, residential community
12. Unique pocket of wonderful identity

The RATIO team facilitated a discussion around participant’s experiences and opinions about the Station Area:  
Note: All cards from this exercise have been collected for reference.

- What is your least favorite thing about the Station Area?
  - a. Feels haphazard
    - i. Sidewalk access is part of village DNA, but not reflected in Prairie View
    - ii. Needs to be safe, clear, convenient access
  - b. Feels a little empty
    - i. Tons of potential, but underutilized
  - c. The scrap yard needs to go
  - d. Poorly laid out, needs clearer circulation
  - e. Incompatible uses – feels a bit “bipolar”
  - f. Infrequency of METRA service
    - i. Biggest challenge for METRA to expand service is the lack of funding
    - ii. Trains not currently running at capacity
      1. Chicken or Egg
    - iii. Other study being led by Mundelein – what funding options exist to support improvements and expanded service along the NCS line? BG is part of that service
  - g. Lost opportunity for a destination supported by the train station
    - i. Off-campus lunch at Stevenson?
  - h. Communication towers/cell towers/telephone poles
    - i. Hasn’t been investigated, but would be required as part of a redevelopment
- What do you want the Prairie View Station Area to be?
  - a. Downtown Arlington Heights as a model for development
    - i. Restaurants, bars, retail, residential
    - ii. Both a destination and an asset for the community
  - b. Should be focused as an amenity for the people of Buffalo Grove
  - c. A natural center of activity for the community
  - d. Small downtown environment
    - i. Independently owned shops / restaurants
    - ii. Highwood
  - e. How can we create a unique collection of high-character, high-quality places and spaces?
  - f. Economically sustainable
    - i. Retail is good, but we don’t need another Starbucks or Jimmy Johns
    - ii. Needs the necessary density of housing to support commerce
      1. Most disappointing aspect of Buffalo Grove is that there’s nothing to do in the community – have to leave for dinner, shopping etc
  - g. Neotraditional “Chicago commercial corridor”
  - h. A unique corridor that offers an identity for BG but unique and different from Lake-Cook
- What don’t you want the Station Area to be?
  - a. Same old, same old
  - b. Not another strip wall
  - c. Don’t want to look back in 5 years and see a district that is unchanged from today

Nicole Woods  
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April 23, 2018

- i. Can hold ourselves to a high standard of developmental quality
- d. The community doesn't need any more chain destinations
- e. Don't want a purely residential development
- f. A haphazard, choppy development
  - i. Protect key assets but knit the area together in a cohesive fashion
- g. A sloppy area without a unifying quality
- h. Not merely a remote parking lot for Stevenson HS

The participants gathered expressed additional concerns in the area:

- 5. Freight Traffic
  - a. Lines are owned by Canadian National
    - i. Purchased E&J lines in last 15 years and more and more of the trains are routing around the region, rather than through
      - 1. Maybe down to 2-3 freight trains a day

The end of the presentation focused on the project process:

- 6. What role should the station area serve in the community?
  - a. Has been covered
- 7. Next Steps
  - a. Project website launched: [www.prairieviewbg.com](http://www.prairieviewbg.com)
  - b. Public workshop: May 10<sup>th</sup>, 6-8PM
  - c. Stage 1 deliverable (May)
  - d. Stage 2 deliverable (July)
  - e. Next Steering Committee Meeting (August)
    - i. Will likely look to meet sooner than this
  - f. Other potential meeting opportunities
    - i. Evening generally better
    - ii. If a meeting needs to be during the day to facilitate membership, people can be flexible
      - 1. Circulate a Doodle poll to set the time for the next meeting
- 8. Working with stakeholders on Easton Ave will be critical – lots of hostility amongst these stakeholders and they don't want to be left in the dark
  - a. Anti-Village sentiments
  - b. Don't self-identify as Prairie View, not Buffalo Grove
  - c. May want to set up a special workshop with a bit more of a personal touch
  - d. Culture shock amongst the formerly rural area against the tide of urbanization

METRA survey data demonstrates that walkers are coming to the station from as far as Buffalo Grove Rd

5-10-05

Prepared By and  
After recording, return to:

Adam B. Simon, Esq.  
Ancel, Glink, Diamond, Bush,  
DiCianni & Rolek, P.C.  
415 W. Washington Street, Suite 202  
Waukegan, Illinois 60085

Village of Lincolnshire  
One Olde Half Day Road  
Lincolnshire, IL 60069

5832724



FILED FOR RECORD BY:  
MARY ELLEN VANDERVENTER  
LAKE COUNTY, IL RECORDER  
08/09/2005 - 09:30:55 A.M.  
RECEIPT #: 238342  
DRAWER #: 29

**BOUNDARY AND PLANNING AGREEMENT BETWEEN THE  
VILLAGE OF LINCOLNSHIRE AND  
VILLAGE OF BUFFALO GROVE**

THIS AGREEMENT is entered into by and between the Village of Buffalo Grove, Cook and Lake Counties, a municipal corporation ("Buffalo Grove") and the Village of Lincolnshire, Lake County, Illinois a municipal corporation ("Lincolnshire").

WHEREAS, this Agreement is being created in order to establish a consensus defining logical municipal boundaries and areas of municipal authority as well as clarifying planning concepts between both municipalities for the purpose of insuring the development of certain territory in a manner consistent with the Comprehensive Plans of both communities; and

WHEREAS, the purpose of this Agreement is to define those areas which might be annexed to each of the municipalities at such time as annexation becomes feasible; and

WHEREAS, it is further the purpose of this Agreement to set forth a consistent and compatible approach toward land use, zoning and development planning within those areas which might be annexed by either Village so as to create a uniform sense of community; and

WHEREAS, the municipalities have consulted and have determined that the establishment of a proper boundary delineation as herein set forth would be in the best interests of each Village as well as the citizens and property owners of the area generally; and

WHEREAS, each Village has determined that it shall not seek annexation as between each other beyond the boundaries hereinafter defined; and

WHEREAS, Section 10 of Article VII of the Constitution of the State of Illinois of 1970 authorizes units of local government, including municipalities, to contract, to exercise, combine, or transfer any power or function not prohibited to them by law or ordinance; and

WHEREAS, in arriving at this Agreement, Buffalo Grove and Lincolnshire have given consideration to the natural flow of stormwater drainage and, when practical, have included all of a single tract of land having common ownership within the jurisdiction of only one municipality; and

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WHEREAS, the Intergovernmental Cooperation Act of 1973 (5 ILCS, 220/1 et seq.) authorizes municipalities to exercise jointly with any public agency of the State, including other units of local government, any power, privilege, or authority which may be exercised by a unit of local government individually, and to enter into contracts for the performance of governmental services, activities, and undertakings; and

WHEREAS, The Village of Lincolnshire and the Village of Buffalo Grove are both home rule units of government pursuant to Article VII of the Constitution of the State of Illinois; and

WHEREAS, Section 6 of Article VII of the Constitution of the State of Illinois of 1970 provides that a home rule unit may exercise any power and perform any function pertaining to its government and affairs including, but not limited to, the power to regulate for the protection of the public health, safety morals and welfare; and

WHEREAS, the Illinois Courts have held that a municipality has a legitimate interest in regulating the development of land near its borders to protect the health and safety of its people [*City of Urbana v. County of Champaign*, 76 Ill. 2d 63 (1979) and *Village of Lake Bluff v. City of North Chicago*, 224 Ill.App.3d 866 (1992)] and to provide uniformity and continuity [*Petterson v. City of Naperville*, 9 Ill. 2d 233 (1956) and *Village of South Elgin v. City of Elgin*, 203 Ill.App.3d at 364 (1990)]; and

WHEREAS, unincorporated territory lies within one and one-half miles of the boundaries of Buffalo Grove and Lincolnshire; and

WHEREAS, Buffalo Grove and Lincolnshire have authorized, by ordinance, the execution of this Agreement as an exercise of their home rule powers and authority under the Constitution of the State of Illinois of 1970.

NOW THEREFORE, in consideration of the mutual covenants hereinafter set forth, it is agreed by and between the Buffalo Grove and Lincolnshire as follows:

Section 1. Incorporation of Preambles

The foregoing Preambles are hereby incorporated herein as if fully set forth hereby.

Section 2. Boundary Line

Buffalo Grove and Lincolnshire agree that in the unincorporated area lying between and near the two municipalities, the boundary line for annexation, governmental planning, subdivision control, official map, ordinances, utility services and other municipal purposes and services shall be as is depicted on the Map attached hereto and made a part hereof as Exhibit "A" and as described on the Area Description and Land Use attached hereto and made a part hereof as Exhibit "B". Neither Village will allow the extension of utilities into any area designated to the other Village without the express written consent of that Village.

Section 3. Disconnection

A. The Villages acknowledge that Area “H” is currently within the incorporated boundary of Lincolnshire. It is agreed that for zoning, planning, and municipal services purposes that Area “H” would be better served by being disconnected from Lincolnshire and annexed to Buffalo Grove. Both Villages shall take what ever actions may be deemed necessary to disconnect Area “H” from Lincolnshire and to have it annexed to Buffalo Grove.

In the event any owner or several owners of Area “H” shall request disconnection from Lincolnshire and shall have entered into an annexation agreement with Buffalo Grove which is not inconsistent with the Land Uses permitted herein, as described in Exhibit “B”, Lincolnshire shall act to disconnect the subject property. In the event that during the term of this Agreement any owner or the several owners of all or any portion of Area “H” fail or refuse to file a petition for disconnection from Lincolnshire, or otherwise frustrate the intent of this Section 3, the Villages hereby agree that this Section 3 shall be severable from the remainder of this Agreement and such failure, refusal or frustration shall not be cause for termination or cancellation of this Agreement.

B. Both parties hereto shall take all necessary action, to include but not by way of limitation Buffalo Grove instituting a *Quo Warranto* action, to disconnect Area I from Lincolnshire. If Buffalo institutes a *Quo Warranto* action regarding Area I, Lincolnshire shall not contest or object to such action and shall consent to a judgment finding Lincolnshire’s annexation of Area I as invalid.

Section 4. Land Planning and Zoning

Buffalo Grove and Lincolnshire acknowledge that each has a legitimate interest in regulating the development and/or redevelopment of land near their borders to protect the health and safety of their residents and to provide uniformity and continuity. In furtherance of said interest, both municipalities further acknowledge that certain uses of property may be incompatible to their interests.

Therefore, Buffalo Grove and Lincolnshire agree that land uses for the designated Areas shall be in accordance with the Land Use as depicted on Exhibit “B” hereto.

The Villages shall not have breached this Agreement if a court orders other uses or development. Both municipalities shall contest any application or litigation requesting permission to conduct, operate or improve a use not described as a Land Use in Exhibit “B” to this Agreement.

Modifications of existing land uses in the designated Areas may be made by mutual consent of the Villages.

Section 5. Jurisdiction

A. With respect to the property located within the area assigned to Buffalo Grove, identified on Exhibit “A” as Areas “C”, “F”, “G”, “H”, “I”, “K” and “L”, Lincolnshire agrees that it shall not annex any unincorporated territory nor shall it exercise or attempt to exercise or enforce any zoning ordinance, subdivision control, official map, or other municipal authority or ordinances, except as may be hereinafter provided in this Agreement.

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B. With respect to the property located within the area assigned to Lincolnshire, identified on Exhibit "A" as Areas "A", "B", "D", "E", and "J", Buffalo Grove agrees that it shall not annex any unincorporated territory nor shall it exercise or attempt to exercise or enforce any zoning ordinance, subdivision control, official map, or other municipal authority or ordinances, except as may be hereinafter provided in this Agreement.

C. Notwithstanding anything herein to the contrary, in the event the properties identified above and depicted on Exhibit "A" as Areas M-1 and M-2 come under common ownership during the term of this Agreement, both Area M-1 and Area M-2 shall be annexed to the same municipality according to the following direction:

(i) If the owner(s) of M-1 purchases M-2, the combined M-1 and M-2 shall not be annexed to Buffalo Grove; and

(ii) If the owner of M-2 purchases M-1, the combined M-1 and M-2 shall not be annexed to Lincolnshire.

(iii) If another owner purchases both M-1 and M-2, that owner may elect to annex to either Lincolnshire or Buffalo Grove, unless such owner owns another Area identified on Exhibit "A", in which case both M-1 and M-2 shall be treated in the same manner as the owner's other Areas subject to this Agreement, provided that such treatment would not affect contiguity of other Areas described herein.

Nothing in this Subsection C. is intended to affect the land uses for Areas M-1 and M-2 described in Exhibit B.

Section 6. Prohibited Annexations or Disconnections

Each municipality shall actively oppose any attempt to effectuate a voluntary or involuntary annexation or disconnection initiated by a third party to have territory annexed to its respective municipality which annexation would have the effect of changing the boundaries established under this Agreement.

Section 7. Statutory Objections

This Agreement shall not be construed so as to limit or adversely affect the right of either municipality to file a statutory objection to proposed rezonings that have been presented to the corporate authorities of Lake County and that are within one and one-half miles of its corporate limits.

Section 8 Notification

Upon receipt by either Village of an annexation, disconnection or zoning petition or proposal or inquiry on the properties included herein, that Village shall as early as possible, notify the other Village, in writing, of said proposal/petition, but in no event later than two (2) business days before the first meeting of said Village when the petition/proposal is discussed or within seven (7) days of an inquiry, whichever occurs earlier.

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Section 9. Annexation of Roadways

For territory that has been annexed by either municipality prior to the date of this Agreement, then the roadway shall be deemed to be located within the municipality to which the roadway has been annexed either by ordinance or by operation of state law (Chapter 65, ILCS, Section 5/7-1-1). For unincorporated territory that is located on either side of the boundary line and that will be annexed to the designated municipality in the future, the roadway shall be deemed to be located within the municipality that first annexes its respective territory adjacent to the roadway. The boundary line set by this Agreement shall be deemed to have been amended accordingly without further action by either municipality.

Section 10. Waiver of Annexation Challenges

Each municipality agrees that it waives any right to challenge or otherwise contest the validity of any annexation the other municipality has effected, is effecting, or will effect in the future for territory located within the other municipality's designated areas as depicted in Exhibit "A" and Exhibit "B".

Section 11. Amendment of Agreement

Neither Buffalo Grove nor Lincolnshire shall either directly or indirectly seek any modification of this Agreement through court action, and this Agreement shall remain in full force and effect until amended or changed by the mutual agreement of the corporate authorities of both municipalities.

Section 12. Severability

If any provisions of this Agreement shall be declared invalid for any reason, such invalidation shall not affect other provisions of this Agreement which can be given effect without the invalid provisions and to this end the provisions of this Agreement are to be severable.

Section 13. Duration of Agreement

This Agreement shall be in full force and effect for a period of twenty (20) years from the date hereof and shall be automatically renewed thereafter for successive five (5) year terms unless either party serves written notice upon the other party not less than 180 days prior to the scheduled expiration of the initial term or renewal thereof of its desire that this Agreement not be so renewed.

Section 14. Applicable Law and Recording Requirements

This Agreement shall be construed in accordance with the laws of the State of Illinois and shall be published by the respective municipalities and recorded or filed with the Lake County Recorder and the Cook County Recorder.

Section 15. Litigation

If either Village successfully enforces the provisions of this Agreement against the other Village by judicial proceedings, the prevailing party shall be reimbursed for all costs of litigation,

including reasonable attorneys' fees and court costs, by the losing Village. If litigation should be initiated by any third party challenging this Agreement, or any application thereof to any person or territory, the expenses of defending such litigation shall be shared equally by the two Villages.

The defense of such litigation and the choice of attorneys shall be controlled by the Village whose annexation is being challenged or from whose territory land is to be disconnected. In the event the other Village desires to engage additional counsel to defend such litigation it shall be at its own expense. Venue for all litigation related to or arising out of this Agreement shall be in Lake County, Illinois:

Section 16. Notice and Service

Any notice hereunder from either party hereto to the other party shall be in writing and shall be served by certified mail, postage prepaid, return receipt requested addressed as follows:

To Buffalo Grove: Village of Buffalo Grove  
50 Raupp Boulevard  
Buffalo Grove, Illinois 60089  
Attn: Village Manager

To Lincolnshire: Village of Lincolnshire  
One Olde Half Day Road  
Lincolnshire, Illinois 60069  
Attn: Village Manager

or to such persons or entities and at such address as either party may from time to time designate by notice to the other party. Notice shall be deemed received on the third business day following deposit in the U.S. Mail in accordance with this Section.

Section 17. Adoption of Ordinances

The parties agree that this Agreement shall be adopted by ordinance approved by the corporate authorities of each municipality.

Section 18. Recording.

The parties agree that this Agreement shall be recorded in the Lake County Recorder of Deeds as against those properties depicted on Exhibit A and more specifically described in Exhibit C, attached hereto and incorporated by reference.



IN WITNESS WHEREOF, the parties hereto have caused the execution of this Agreement by their duly authorized officers as of the 23<sup>rd</sup> day of May, 2005.

Village of Lincolnshire

Village of Buffalo Grove

BY: Brett Blumberg  
Mayor

BY: Elliott Huster  
Village President

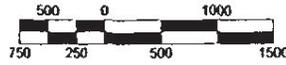
Attest: Barbara Mastandrea  
Village Clerk

Attest: Janet M. Sebastian  
Village Clerk

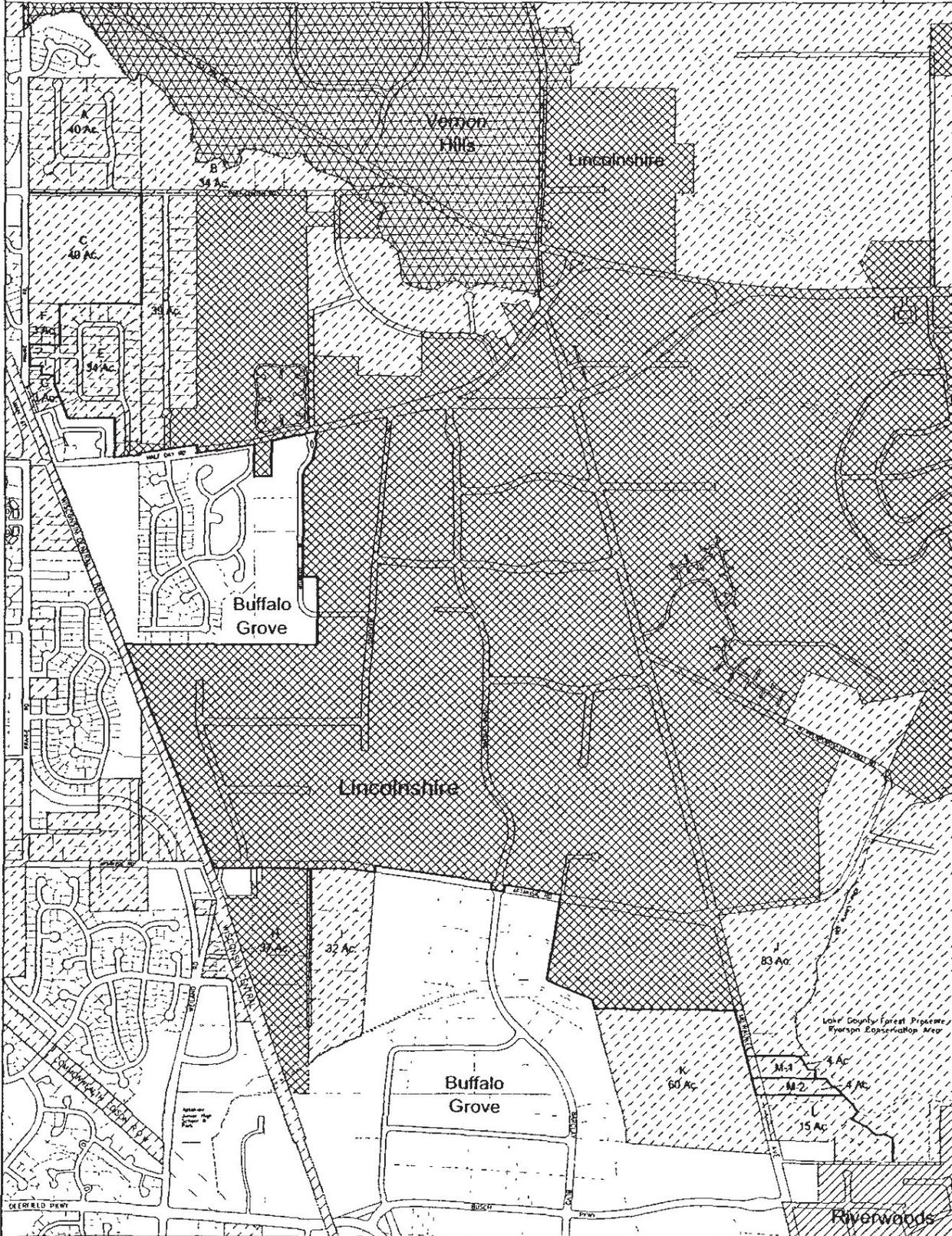
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# Exhibit A -- Buffalo Grove & Lincolnshire Boundary & Planning Agreement Map

-  Village of Buffalo Grove
-  Village of Lincolnshire
-  Village of Riverwoods
-  Village of Vernon Hills
-  Unincorporated Lake County
-  Proposed Boundary Line



Prepared by the Village of Buffalo Grove -- Division of Planning Services, 4/27/05  
All area calculations are approximate and rounded to the nearest one acre interval



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## Exhibit B -- Buffalo Grove & Lincolnshire Boundary & Planning Agreement

### Area Description & Land Use

**Village of Buffalo Grove**

Area	Acreage <sup>1</sup>	Description	Land Use/Zoning District <sup>2</sup>
C	+ 40	Ac. Stancliff Property	Single Family Detached, 2 Units / Acre
F	+ 3	Ac. Prairie Road / Opposite Roslyn Woods	Single Family Detached, 2 Units / Acre
G	+ 3	Ac. Prairie Road / North of Noah's Landing	Single Family Attached, 6 Units / Acre
H	+ 37	Ac. Industrial Drive / Dick's Crane Service	Office & Research / Industrial
I	+ 32	Ac. Peerless / Cole Wire / Laidlaw	Office & Research / Industrial
K	+ 60	Ac. Land & Lakes / West Milwaukee Avenue	B1/B2/B3/B4 or Recreational / Open Space
L	+ 15	Ac. Cowhey / East of Milwaukee Avenue	B1/B2/B3/B4 or Recreational / Open Space
	190	Ac.	

**Village of Lincolnshire**

Area	Acreage <sup>1</sup>	Description	Land Use/Zoning District <sup>2</sup>
A	+ 40	Ac. Meadows of Prairie View Subdivision	Single Family Detached, 2 Units / Acre
B	+ 34	Ac. East of the Meadows / North of Port Clinton Road	Single Family Detached, 2 Units / Acre
D	+ 39	Ac. Prairie Ridge Subdivision (Apple Hill Lane)	Single Family Detached, 2 Units / Acre
E	+ 34	Ac. Kris View Acres Subdivision	Single Family Detached, 2 Units / Acre
H	- 37	Ac. Industrial Drive / Dick's Crane Service	Office & Research / Industrial
J	+ 83	Ac. Boznos / East Milwaukee Avenue	B1/B2 or Recreational / Open Space
	193	Ac.	

**Flex Parcels**

Area	Acreage <sup>1</sup>	Description	Land Use <sup>2</sup>
M-1	+ 4	Ac. PIN # 15-26-100-030 & PIN # 15-26-100-040	Lincolnshire - Commercial / Parking / Open Space Buffalo Grove - Commercial / Transfer Station / Open Space
M-2	+ 4	Ac. PIN # 15-26-100-035	Lincolnshire - Commercial / Parking / Open Space Buffalo Grove - Commercial / Transfer Station / Open Space
	8	Ac.	

<sup>1</sup> All area calculations are approximate and rounded to the nearest one acre interval.

<sup>2</sup> Notwithstanding any mixed uses allowed by each party's zoning code, no residential uses are permitted except where expressly described herein.

## Exhibit C - - Buffalo Grove & Lincolnshire Boundary and Planning Agreement

### Legal Description of Properties

#### AREA A

LOTS 1 THROUGH 32 AND OUTLOT A IN THE MEADOWS OF PRAIRIE VIEW, A SUBDIVISION OF THE SOUTHWEST ¼ OF THE NORTHEAST ¼ OF SECTION 16, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN LAKE COUNTY, ILLINOIS.

PINS: 15-16-202-001, 15-16-202-002, 15-16-202-003, 15-16-202-004, 15-16-202-005, 15-16-202-006, 15-16-202-007, 15-16-202-008, 15-16-202-009, 15-16-202-010, 15-16-202-011, 15-16-202-012, 15-16-202-013, 15-16-203-001, 15-16-203-002, 15-16-203-003, 15-16-203-004, 15-16-203-005, 15-16-203-006, 15-16-203-007, 15-16-203-008, 15-16-203-009, 15-16-203-010, 15-16-203-011, 15-16-203-012, 15-16-203-013, 15-16-203-014, 15-16-203-015, 15-16-203-016, 15-16-203-017, 15-16-203-018, 15-16-203-019, 15-16-203-020

#### AREA B

THAT PART OF THE SOUTHEAST ¼ OF THE NORTHEAST ¼ OF SECTION 16, AND THAT PART OF THE SOUTHWEST ¼ OF THE NORTHWEST ¼ OF SECTION 15, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING SOUTH OF THE CENTERLINE OF THE INDIAN CREEK, IN LAKE COUNTY, ILLINOIS.

PINS: 15-15-100-008, 15-15-100-009, 15-15-100-017, 15-15-100-018, 15-15-301-001, 15-16-200-016, 15-16-200-033, 15-16-200-035, 15-16-200-037, 15-16-200-039, 15-16-200-040, 15-16-200-041

#### AREA C

THE NORTHWEST ¼ OF THE SOUTHEAST ¼ OF SECTION 16, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN LAKE COUNTY, ILLINOIS.

PINS: 15-16-400-018, 15-16-400-019, 15-16-400-021

#### AREA D

LOTS 1 THROUGH 34 IN PRAIRIE RIDGE, A SUBDIVISION OF THE EAST ½ OF THE SOUTHEAST ¼ OF SECTION 16; AND

THE NORTH 312 FEET OF THE WEST ½ OF LOT 29 IN SCHOOL TRUSTEE'S SUBDIVISION OF SECTION 16; AND,

LOT 2 IN WOODMAR, A SUBDIVISION OF THE NORTHEAST ¼ OF THE NORTHEAST ¼ OF SECTION 21 , ALL IN TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN LAKE COUNTY, ILLINOIS.

PINS: 15-16-400-014, 15-16-400-015, 15-16-400-016, 15-16-400-017, 15-16-401-022, 15-16-401-023, 15-16-401-024, 15-16-401-025, 15-16-401-026, 15-16-401-027, 15-16-401-028, 15-16-401-029, 15-16-401-030, 15-16-401-031, 15-16-401-032, 15-16-401-033, 15-16-401-034, 15-16-401-035, 15-16-401-036, 15-16-401-037, 15-16-402-001, 15-16-402-002, 15-16-402-003, 15-16-402-004, 15-16-402-005, 15-16-

402-006, 15-16-402-007, 15-16-402-008, 15-16-402-009, 15-16-402-010, 15-16-402-011, 15-16-402-012, 15-16-402-013, 15-16-402-014, 15-16-402-015, 15-16-402-016, 15-16-402-017, 15-16-402-018, 15-16-402-019, 15-21-203-005, 15-21-203-006, 15-21-203-007, 15-21-203-008, 15-21-203-009, 15-21-203-010, 15-21-203-011, 15-21-203-012, 15-21-203-013, 15-21-203-014, 15-21-203-015, 15-21-203-016, 15-21-203-017, 15-21-203-018, 15-21-203-019, 15-21-203-020, 15-21-203-021, 15-21-203-022, 15-21-203-023, 15-21-203-024

**AREA E**

LOTS 2 THROUGH 54 IN KRISVIEW ACRES, A SUBDIVISION OF PART OF SECTION 16 AND PART OF SECTION 21; AND

LOTS 1 THROUGH 4, 7 AND 8, BLOCK 2 AND LOTS 5, 6, 11, AND 12 IN BLOCK 1 OF SPRAGUE'S SUBDIVISION, A SUBDIVISION OF SECTION 16;

ALL IN TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN LAKE COUNTY, ILLINOIS.

PINS: 15-16-401-001, 15-16-401-002, 15-16-401-003, 15-16-401-004, 15-16-401-005, 15-16-401-006, 15-16-401-007, 15-16-401-008, 15-16-401-009, 15-16-401-010, 15-16-401-011, 15-16-401-012, 15-16-401-013, 15-16-401-014, 15-16-401-015, 15-16-401-016, 15-16-401-017, 15-16-401-018, 15-16-401-019, 15-16-401-020, 15-16-401-021, 15-16-403-005, 15-16-403-006, 15-16-403-015, 15-16-404-001, 15-16-404-002, 15-16-404-003, 15-16-404-004, 15-16-404-005, 15-16-404-006, 15-16-404-007, 15-16-404-008, 15-16-404-009, 15-16-404-010, 15-16-404-011, 15-16-404-012, 15-16-404-013, 15-16-404-014, 15-16-404-015, 15-16-404-016, 15-16-405-013, 15-16-405-014, 15-16-405-015, 15-16-405-016, 15-16-405-017, 15-16-405-018, 15-16-405-019, 15-16-405-020, 15-16-405-021, 15-16-405-022, 15-16-405-023, 15-16-405-028, 15-16-405-033, 15-21-202-001, 15-21-202-002, 15-21-202-003, 15-21-203-001, 15-21-203-002

**AREA F**

LOTS 1 THROUGH 4 AND 7 THROUGH 10 IN BLOCK 1 IN SPRAGUE'S SUBDIVISION; AND

THE NORTH 4.06 CHAINS OF THE WEST 5.46 CHAINS;

OF AND IN THE SOUTHWEST 1/4 OF THE SOUTHEAST 1/4 OF SECTION 16, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN LAKE COUNTY, ILLINOIS.

PINS: 15-16-400-009, 15-16-400-010, 15-16-403-001, 15-16-403-002, 15-16-403-007, 15-16-403-008, 15-16-403-013, 15-16-403-014

**AREA G**

LOTS 5 AND 6 AND 9 THROUGH 12 IN BLOCK 2 OF SPRAGUE'S SUBDIVISION; AND

LOTS 1 THROUGH 3 IN KENDLER'S SUBDIVISION;

BOTH SUBDIVISIONS OF THE SOUTHWEST 1/4 OF THE SOUTHEAST 1/4 OF SECTION 16, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN LAKE COUNTY, ILLINOIS.

PINS: 15-16-405-010, 15-16-405-011, 15-16-405-012, 15-16-405-029, 15-16-405-030, 15-16-405-031, 15-16-405-032, 15-16-405-034

**AREA H**

THE WEST 1/2 OF THE NORTHWEST 1/4 OF SECTION 27, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN EXCEPTING THEREFROM THAT PART LYING WEST OF THE EAST LINE OF THE CANADIAN NATIONAL RAILROAD RIGHT-OF-WAY, AND ALSO EXCEPTING THEREFROM THE NORTH 190 FEET OF THE WEST 60 FEET; AND THAT PART OF THE NORTHEAST 1/4 OF THE NORTHEAST 1/4 OF SECTION 28, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING EAST OF THE EAST LINE OF THE CANADIAN NATIONAL RAILROAD RIGHT-OF-WAY, AND EXCEPTING THEREFROM THAT PART LYING SOUTH OF THE SOUTH LINE OF PARK PLACE IN THE FRILLMAN PARK SUBDIVISION, ALL IN LAKE COUNTY, ILLINOIS.

PINS: 15-27-100-002, 15-27-100-013, 15-27-100-014, 15-27-100-016, 15-27-100-019, 15-27-100-021, 15-28-200-009, 15-28-200-013, 15-28-200-064

**AREA I**

THAT PART OF THE NORTHWEST 1/4 OF SECTION 27, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS FOLLOWS, TO-WIT: COMMENCING AT A POINT ON THE SOUTH LINE OF THE NORTHWEST 1/4, AFORESAID, SAID POINT BEING 571.75 FEET EAST OF SOUTHEAST CORNER OF THE WEST 1/2 OF THE WEST 1/2 OF THE NORTHWEST 1/4; THENCE NORTHERLY 660.48 FEET ALONG A STRAIGHT LINE CONNECTING THE COMMENCEMENT POINT TO A POINT ON THE CENTERLINE OF APTAKISIC ROAD, SAID POINT BEING 156.93 FEET EASTERLY OF THE INTERSECTION OF SAID CENTERLINE OF ROAD WITH THE NORTH LINE OF THE NORTHEAST 1/4 OF SECTION 27, TOWNSHIP AND RANGE AFORESAID, AT THE CENTERLINE OF APTAKISIC CREEK FOR THE PLACE OF BEGINNING; THENCE CONTINUING NORTHERLY ALONG THE SAID STRAIGHT LINE 1970.15 FEET, THE SAID POINT ON THE CENTERLINE OF APTAKISIC ROAD; THENCE WESTERLY ALONG THE CENTERLINE OF SAID APTAKISIC ROAD, 156.93 FEET TO THE NORTH LINE OF THE SAID NORTHWEST 1/4; THENCE WEST 573.15 FEET ALONG THE LAST SAID NORTH LINE OF THE NORTHWEST 1/4 TO THE EAST LINE OF THE SAID WEST 1/2 OF THE WEST 1/2 OF THE NORTHWEST 1/4; THENCE SOUTH 2318.35 FEET ALONG THE LAST SAID EAST LINE TO A POINT AT THE CENTERLINE OF APTAKISIC CREEK 332.45 FEET NORTH OF THE SAID SOUTHEAST CORNER OF THE WEST 1/2 OF THE WEST 1/2 OF THE NORTHWEST 1/4; THENCE NORTHEASTERLY 690.66 FEET ALONG THE SAID CENTERLINE OF APTAKISIC CREEK TO THE PLACE OF BEGINNING, EXCEPT THAT PART THEREOF LYING NORTHERLY OF THE SOUTHERLY RIGHT-OF-WAY OF APTAKISIC ROAD DEDICATED BY DOC. 598568, AND FILED FOR RECORD ON AUGUST 27, 1946, IN LAKE COUNTY, ILLINOIS.

PINS: 15-27-100-017, 15-27-100-031, 15-27-100-032, 15-27-100-034

**AREA J**

LOT 2, EXCEPT THE SOUTH 256.2 FEET THEREOF IN TRIPP'S SUBDIVISION, A SUBDIVISION OF SECTION 26, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING EAST OF THE CENTERLINE OF MILWAUKEE AVENUE AND WEST OF THE CENTERLINE OF THE DES PLAINES RIVER, AND

THAT PART OF THE NORTHWEST 1/4 OF SECTION 26, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING EAST OF THE CENTERLINE OF MILWAUKEE AVENUE, WEST OF THE CENTERLINE OF THE DES PLAINES RIVER, AND SOUTH OF THE CENTERLINE OF VACATED MILL ROAD; AND THAT PART OF THE NORTHEAST 1/4 OF THE NORTHWEST 1/4 OF SECTION 26, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING NORTHERLY AND WESTERLY OF THE CENTERLINE OF VACATED RIVERSIDE ROAD (ALSO KNOWN AS SOUTH MILL ROAD); AND

THAT PART OF THE SOUTHWEST 1/4 OF SECTION 23, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN DESCRIBED AS FOLLOWS: BEGINNING IN THE

CENTERLINE OF RIVERSIDE ROAD AT A POINT 2393.02 FEET SOUTHEASTERLY OF SAID WEST LINE AS MEASURED ALONG THE CENTERLINE OF SAID ROAD; THENCE NORTHWESTERLY ALONG SAID CENTERLINE, 1427.00 FEET; THENCE NORTHEAST AT RIGHT ANGLES TO SAID CENTERLINE 493.00 FEET; THENCE SOUTHEASTERLY 767.00 FEET, TO A POINT 550.00 FEET NORTH OF, MEASURED PARALLEL TO SAID ROAD; THENCE SOUTHEAST 40 DEGREES TO THE RIGHT, 190.00 FEET; THENCE NORTH 86 DEGREES TO THE LEFT, 675.25 FEET; THENCE SOUTHWEST 963.40 FEET, TO THE POINT OF BEGINNING; AND

THAT PART OF THE SOUTHWEST 1/4 OF SECTION 23, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS FOLLOWS: BEGINNING ON THE WEST LINE 8.01 CHAINS SOUTH OF THE NORTHWEST 1/4; THENCE SOUTHEAST ALONG THE CENTERLINE OF RIVERSIDE ROAD (ALSO KNOWN AS NORTH MILL ROAD) TO THE CENTERLINE OF VACATED RIVERSIDE DRIVE (ALSO KNOWN AS NORTH MILL ROAD); THENCE SOUTHWESTERLY ALONG THE CENTERLINE OF RIVERSIDE ROAD (ALSO KNOWN AS NORTH MILL ROAD AND SOUTH MILL ROAD) TO THE SOUTH LINE OF THE SOUTHWEST 1/4 AFORESAID; THENCE WESTERLY ALONG SAID SOUTH LINE TO THE WEST LINE OF THE SOUTHWEST 1/4 AFORESAID; THENCE NORTH ALONG THE WEST LINE OF THE SOUTHWEST 1/4 AFORESAID TO THE POINT OF BEGINNING, EXCEPTING THEREFROM THAT PART LYING WEST AND SOUTH OF A LINE DESCRIBED AS FOLLOWS: BEGINNING AT THE INTERSECTION OF THE WEST LINE AFORESAID AND THE CENTERLINE OF RIVERSIDE ROAD (ALSO KNOWN AS NORTH MILL ROAD); THENCE SOUTH EAST ALONG THE CENTERLINE SAID ROAD, 1899.47 FEET; THENCE SOUTHWEST 663.35 FEET; THENCE SOUTH, 678.47 FEET, TO THE SOUTH LINE AFORESAID; THENCE WEST ALONG THE SOUTH LINE AFORESAID TO THE WEST LINE OF SAID SOUTHWEST 1/4; THENCE NORTH ALONG THE WEST LINE OF THE SOUTHWEST 1/4 AFORESAID, TO THE POINT OF BEGINNING OF THE LINE BEING DESCRIBED, AND

THAT PART OF THE SOUTHWEST 1/4 OF SECTION 23, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN DESCRIBED AS FOLLOWS: BEGINNING AT THE INTERSECTION OF THE CENTERLINE OF VACATED RIVERSIDE ROAD (ALSO KNOWN AS SOUTH MILL ROAD) AND THE SOUTH LINE OF THE SOUTHWEST 1/4 AFORESAID; THENCE NORTH ALONG THE CENTERLINE OF SAID VACATED ROAD, 982.40 FEET; THENCE SOUTH 71 DEGREES EAST, 309.80 FEET, TO THE CENTERLINE OF THE DES PLAINES RIVER; THENCE SOUTHERLY ALONG THE CENTERLINE OF THE DES PLAINES RIVER TO THE SOUTH LINE OF THE SOUTHWEST 1/4 AFORESAID; THENCE WEST ALONG THE SOUTH LINE OF THE SOUTHWEST 1/4 AFORESAID, TO THE POINT OF BEGINNING,

ALL OF THE FOREGOING IN LAKE COUNTY, ILLINOIS.

PINS: 15-23-300-010, 15-23-300-018, 15-23-300-019, 15-23-300-024, 15-23-300-026, 15-26-100-004, 15-26-100-032, 15-26-100-038, 15-26-100-039, 15-26-100-043, 15-26-100-044, 15-26-100-045, 15-26-100-046, 15-26-100-047

**AREA K**

A TRACT OF LAND IN THE WEST 1/2 OF SECTION 26 AND THE EAST 1/2 OF SECTION 27, BOTH IN TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LAKE COUNTY, ILLINOIS, BOUNDED AND DESCRIBED AS FOLLOWS: BEGINNING AT THE INTERSECTION OF THE SOUTH LINE OF LOTS 1 AND 2 IN TRIPP'S SUBDIVISION OF PART OF SECTIONS 26 AND 27, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN AND THE WESTERLY LINE OF MILWAUKEE AVENUE (ILLINOIS ROUTE 21) AS PER DOCUMENT NO. 395843 RECORDED NOVEMBER 14, 1933; THENCE NORTH 85° 53' 43" WEST ALONG THE SOUTH LINE OF SAID LOTS 1 AND 2, A DISTANCE OF 1626.85 FEET; THENCE NORTHERLY ALONG THE FOLLOWING FIVE COURSES, NORTH 01° 48' 31" WEST A DISTANCE OF 144.67 FEET, NORTH 12° 09' 08" WEST A DISTANCE OF 150.00 FEET, NORTH 16° 56' 08" WEST A DISTANCE OF 359.78 FEET, NORTH 18° 28' 43" WEST A DISTANCE OF 399.83 FEET AND NORTH 15° 19' 35" WEST A DISTANCE OF 540.65 FEET TO A POINT IN THE NORTH LINE OF SAID LOTS 1 AND 2 IN TRIPP'S

B

SUBDIVISION; THENCE SOUTH 89° 30' 39" EAST ALONG SAID NORTH LINE OF LOTS 1 AND 2 A DISTANCE OF 1629.13 FEET TO A POINT ON THE WESTERLY LINE OF MILWAUKEE AVENUE; THENCE SOUTHEASTERLY ALONG SAID WESTERLY LINE OF MILWAUKEE AVENUE BEING THE ARC OF A CIRCLE CONVEX EASTERLY AND HAVING A RADIUS OF 68,704.94 FEET, AN ARC DISTANCE OF 1687.72 FEET TO THE POINT OF BEGINNING, ALL IN LAKE COUNTY, ILLINOIS.

PINS: 15-26-100-007, 15-26-100-036, 15-27-200-041, 15-27-200-042

**AREA L**

LOTS 1 THROUGH 5 IN MCKNIGHT'S RESUBDIVISION, A SUBDIVISION OF THE NORTHEAST 1/4 OF THE SOUTHWEST 1/4 OF SECTION 26, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN; ; AND

THAT PART OF LOT 4 (EXCEPT THE NORTH 208.25 FEET THEREFROM) IN TRIPP'S SUBDIVISION, A SUBDIVISION OF SECTION 26, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING EAST OF MILWAUKEE AVENUE, WEST OF THE CENTERLINE OF THE DES PLAINES RIVER, ALL IN LAKE COUNTY, ILLINOIS.

PINS: 15-26-300-055, 15-26-300-056, 15-26-300-057, 15-26-300-058, 15-26-300-059, 15-26-300-060, 15-26-301-003, 15-26-301-006, 15-26-301-010, 15-26-301-011, 15-26-301-012

**AREA M-1**

THAT PART OF THE SOUTH 256.20 FEET OF LOT 2 IN TRIPP'S SUBDIVISION, A SUBDIVISION OF THE NORTHWEST 1/4 OF SECTION 26, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING EAST OF THE CENTERLINE OF MILWAUKEE AVENUE AND WEST OF THE CENTERLINE OF THE DESPLAINES RIVER , ALL IN LAKE COUNTY, ILLINOIS.

PINS: 15-26-100-030, 15-26-100-040, 15-26-100-041

**AREA M-2**

THAT PART OF THE NORTH 208.25 FEET OF LOT 4 IN TRIPP'S SUBDIVISION, A SUBDIVISION OF THE NORTHWEST 1/4 OF SECTION 26, TOWNSHIP 43 NORTH, RANGE 11, EAST OF THE THIRD PRINCIPAL MERIDIAN, LYING EAST OF THE CENTERLINE OF MILWAUKEE AVENUE AND WEST OF THE CENTERLINE OF THE DES PLAINES RIVER, ALL IN LAKE COUNTY, ILLINOIS.

PIN: 15-26-100-035

# Ceremonial Boundary and Planning Agreement



between the  
*Village of Buffalo Grove*  
and the  
*Village of Lincolnshire*



*THIS AGREEMENT* is entered into by the Village of Buffalo Grove, Cook and Lake Counties, and the Village of Lincolnshire, Lake County, Illinois.

*WHEREAS*, the Agreement was created in order to establish a consensus defining logical municipal boundaries and areas of municipal authority. It clarifies planning concepts between both municipalities and insures the development of certain territory in a manner consistent with the Comprehensive Plans of both communities; and

*WHEREAS*, the purpose of our Agreement is to define those areas which might be annexed to each of the municipalities at such time as annexation becomes feasible; and

*WHEREAS*, it is further the purpose of our Agreement to set forth a consistent and compatible approach toward land use, zoning and development planning within those areas which might be annexed by either Village in order to create a uniform sense of community; and

*WHEREAS*, each municipality has consulted and determined that the establishment of a proper boundary delineation is in the best interests of each Village, its citizens, and property owners of the area; and

*NOW THEREFORE*, it is agreed by and between Buffalo Grove and Lincolnshire as follows:

*IN WITNESS WHEREOF*, the parties hereto have caused the execution of an Agreement by their duly authorized officers as of the 1<sup>st</sup> day of June, 2005.

*Village of Lincolnshire*

*Village of Buffalo Grove*

by: Brett Ranby  
Mayor

by: Elliott Houtsten  
Village President

15



# Buffalo Grove and Lincolnshire Boundary and Planning Agreement

## Proposed owner

Flex Parcels

Village of Buffalo Grove

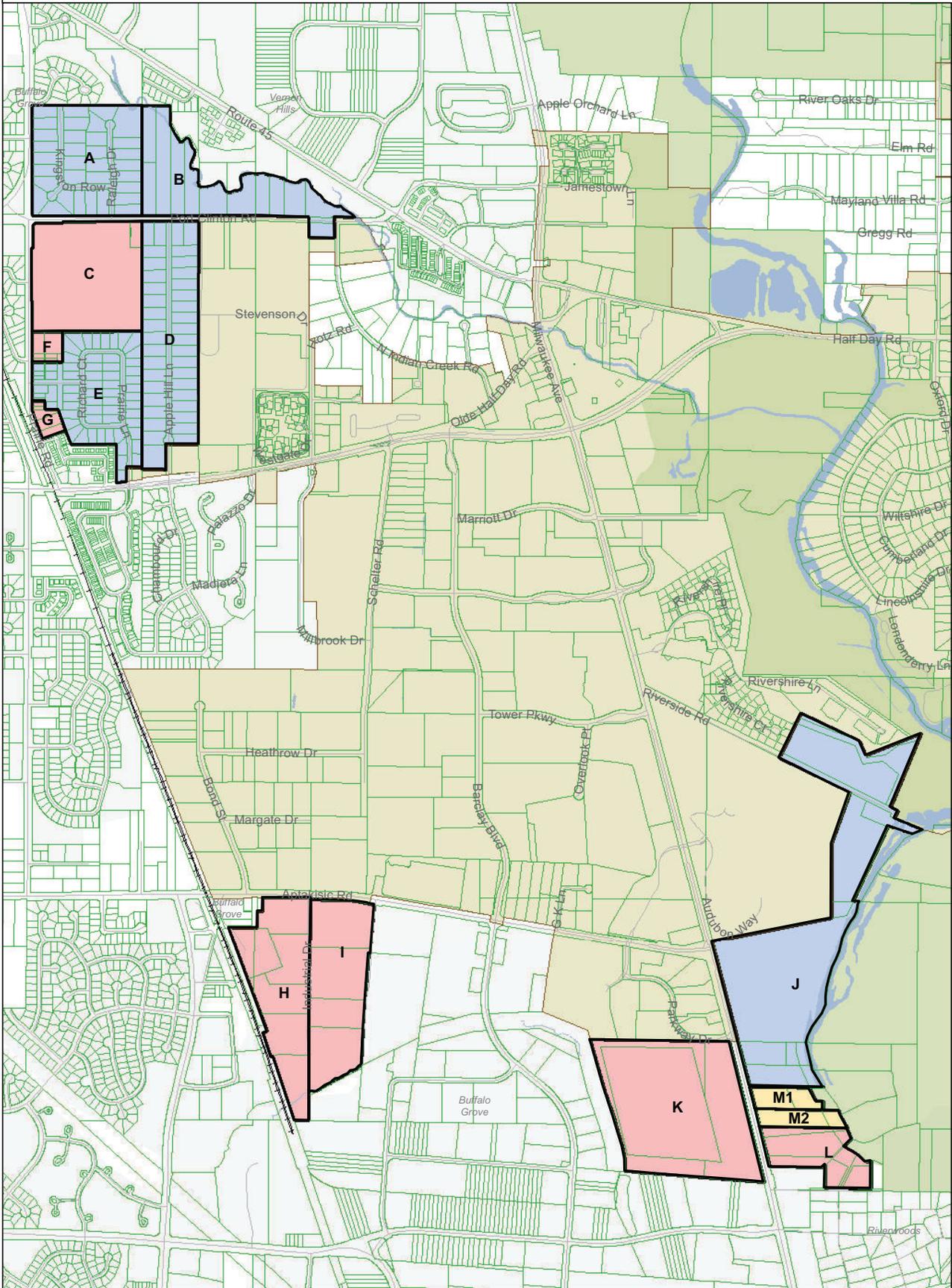
Village of Lincolnshire

Parcel boundary

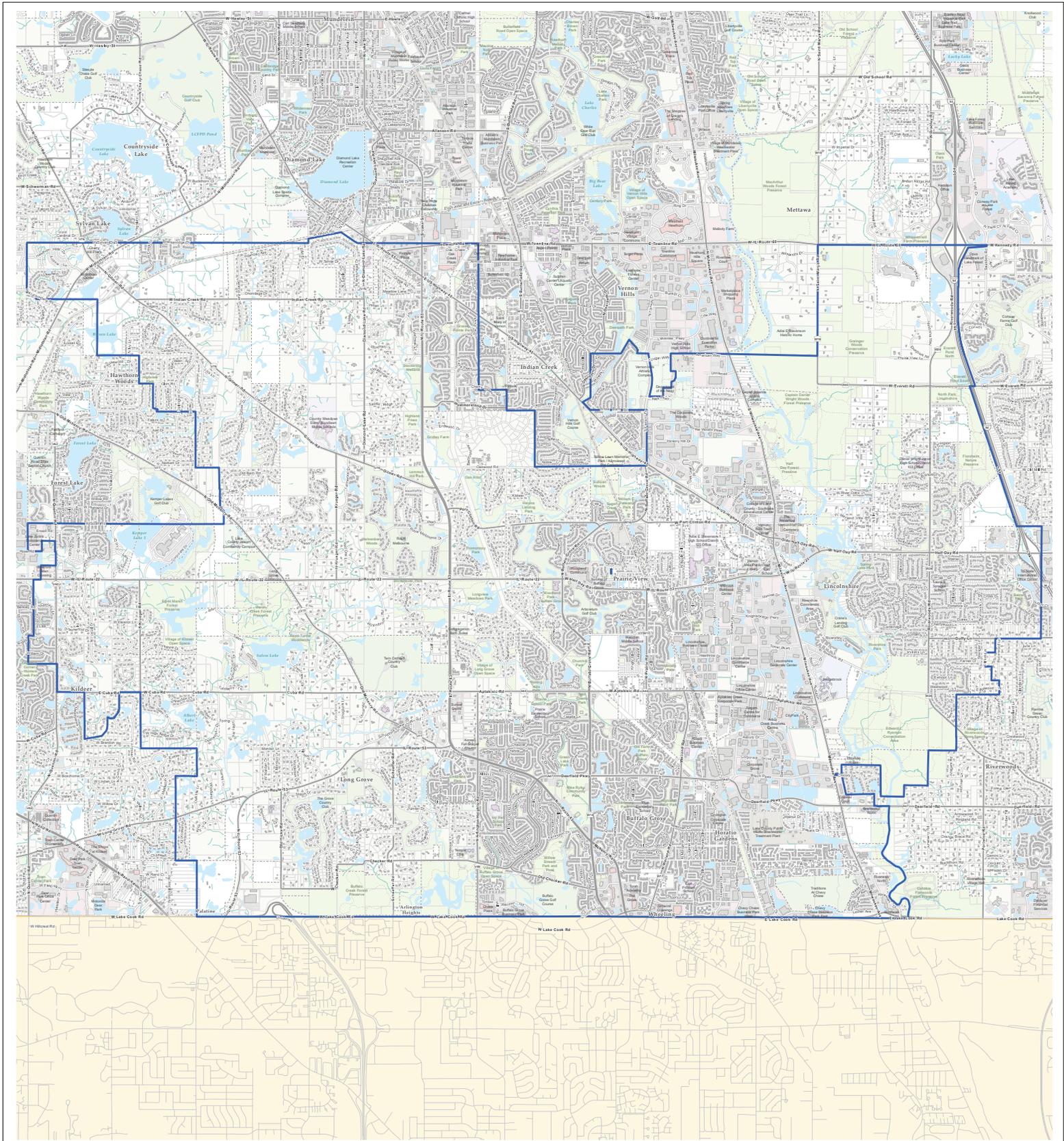
Street

Lincolnshire

1 inch = 1,083 feet



# Adlai E Stevenson District 125



**Lake County**  
 Prepared by:  
 Lake County Department of Information Technology  
 GIS Division  
 18 North County Street  
 Waukegan, Illinois 60085-4357  
 (847) 377-2388

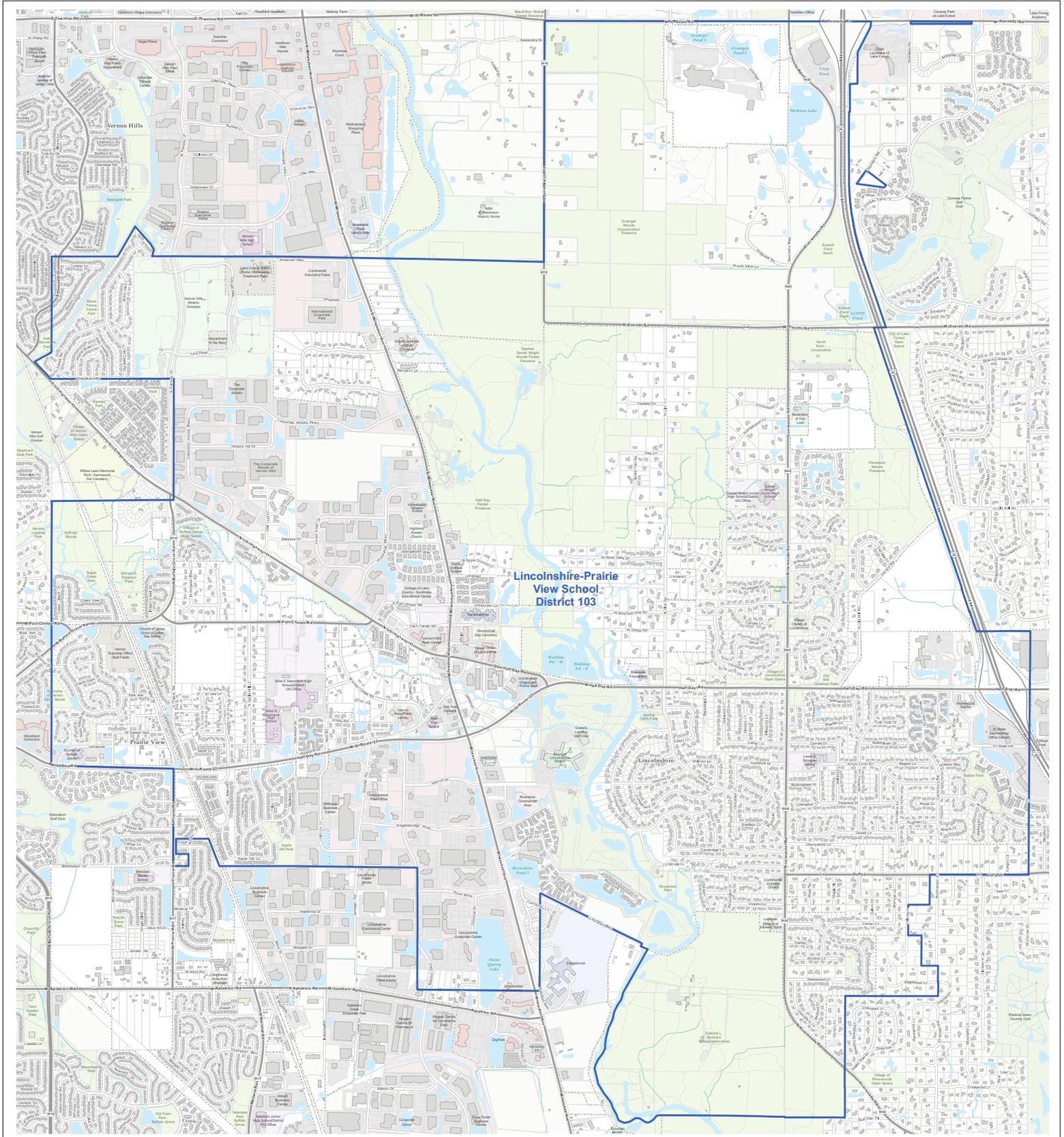


1,500 0 1,500 3,000 4,500 Feet

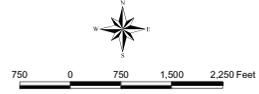
Revised:  
2017

Landmark Classification		District Boundary
Agriculture, Food, and Livestock	Public Attractions	District Boundary
Banking and Finance; Commercial and Retail	Parks and Forest Preserves	Tax Parcels
Emergency Response and Law Enforcement	Water	Railroad
Education	Cemetery	Trails and Paths
Health and Medical	Golf Course	
	Industry; Mail and Shipping	

# Lincolnshire-Prairie View School District 103




  
 Prepared by:
   
 Lake County Department of Information Technology
   
 GIS Division
   
 18 North County Street
   
 Waukegan, Illinois 60085-4357
   
 (847) 377-2388



Revised:
   
 2017

Landmark Classification		District Boundary
	Agriculture, Food, and Livestock	 District Boundary
	Banking and Finance; Commercial and Retail	 Tax Parcels
	Emergency Response and Law Enforcement	 Railroad
	Education	 Trails and Paths
	Health and Medical	
	Industry; Mail and Shipping	
	Public Attractions	
	Parks and Forest Preserves	
	Water	
	Cemetery	
	Golf Course	

**REQUEST FOR BOARD ACTION  
Committee of the Whole Meeting  
April 8, 2019**

<b>Subject:</b>	Consideration and Discussion of a Picnic Permit Fee Waiver Request (Girl Scout Troop #45713 & Troop #45936)
<b>Action Requested:</b>	Consideration of a Picnic Permit Fee Waiver Request Pursuant to Section 8-1B-2(O) of the Village Code (Girl Scout Troop #45713 & Troop #45936)
<b>Petitioner:</b>	Girls Scout Troops #45713 & #45936
<b>Originated</b>	
<b>By/Contact:</b>	Bradford H. Woodbury, Public Works Director
<b>Referred To:</b>	Mayor and Board of Trustees

**Background:** Girl Scout Troops #45713 and #45936 are planning a multi-troop picnic at North Park on Saturday August 24, 2019 from 10:00am-12:00pm. The two troops have applied for a Picnic Permit and request Village consideration of a waiver of the required \$50fee. In accordance with Village Code Section 8-1B-2(O), any petitioner may request a picnic fee waiver. The request must be considered by the Park Board and Village Board. It is common for religious institutions and other not-for-profit organizations in the Village to request various fee waivers. Village records do not show any previous fee waiver request from Girl Scout Troop #45713 or #45936. At its March 21, 2019 meeting, the Park Board voted to recommend the Village Board approve a waiver of the picnic permit fee for Girl Scout Troops #45713 and #45936.

**Summary:** In accordance with Village Code Section 8-1B-2(O), any picnic permit applicant is eligible to petition the Park Board and Village Board as outlined below :

*“An applicant may petition the Park Board for a waiver to the above and if deemed appropriate a recommendation shall be made to the Village Board who will determine if a waiver shall be granted.”*

This request is consistent with past fee waiver approvals and is recommended by Staff.

**Recommendation:** Consideration of approval of a request to waive Picnic Permit fees of \$50, in accordance with Village Code Section 8-1B-2(O).

**Reports and Documents Attached:**

- Picnic Permit Application
- Fee waiver request email from Girl Scout #45713/#45936 Troop leader, Paula Lanni, dated March 3, 2019

<b>Meeting History</b>	
Committee of the Whole Meeting	April 8, 2019
Park Board	March 21, 2019



One Olde Half Day Road  
 Lincolnshire, IL 60069  
 Ph: 847.883.8600 | Fax: 847.883.8608  
[www.village.lincolnshire.il.us](http://www.village.lincolnshire.il.us)

STAMP HERE

# PICNIC PERMIT APPLICATION FORM

Permit No: \_\_\_\_\_

**Area requested: Please check one selection below**

North Park Concession Area...Max. 200 <input checked="" type="checkbox"/>	Spring Lake Park Picnic Area...Max. 100 (Not available May 1 – October 31 Unless approved by Mayor & Board of Trustees)	Bicentennial Park...Max. 50
North Park Picnic Pavilion...Max. 40 (Fire Place requires separate permit & fee)	Whytegate Park...Max. 50	

## APPLICANT INFORMATION

Name: Paula Lanni Name of Organization: Girl Scouts  
 Address: 34 Lincolnshire Drive Unit #: \_\_\_\_\_  
 City: Lincolnshire State: IL Zip Code: 60069  
 Telephone: (847) 744-0643 Cell: (847) 744-0643  
 Resident Yes:  No:   
 Email: paulamlanni@gmail.com  
 Date & Time Requested: August 24, 2019 (Saturday)

Total number of people who will attend the picnic: @ 50-70

### PAYMENT (see page two for fee schedule)

Non-refundable permit fee <small>Receipt code #174</small>	\$ _____
Refundable deposit fee	\$ _____

On behalf of Girl Scouts USA (organization), I have read Village of Lincolnshire Title 8, Chapter 1B of the Village of Lincolnshire Municipal Code and do hereby agree to comply with the rules and regulations outlined in this Ordinance.

## SIGNATURE

Signature: Paula Lanni Date: 3/3/19

===== For Office Use Only =====

Picnic Area Inspected By: \_\_\_\_\_ Date: \_\_\_\_\_

Condition of Picnic Area: \_\_\_\_\_

Date Deposit Returned: \_\_\_\_\_ Explanation of any part of deposit withheld: \_\_\_\_\_

\_\_\_\_\_

## Bradford Woodbury

---

**From:** Paula Lanni <paulamlanni@gmail.com>  
**Sent:** Sunday, March 03, 2019 11:40 AM  
**To:** Bradford Woodbury  
**Subject:** Re: North Park Picnic Permit for Girl Scouts  
**Attachments:** GS Picnic Permit Northpark.pdf

Bradford,

Please find attached our completed Picnic Permit Application Form for the concession area at North Park on Saturday, August 24, 2019. As previously mentioned, we will be bridging 2 local Girl Scout troops (1 from Sprague and 1 from Half Day schools). I'd like to petition the Park and Village Boards to consider waiving the permits fees for our non-profit group. Please let me know if you require any further information.

Thank you in advance for your assistance in this matter.

Regards,  
Paula Lanni  
Troop Leader #45713 & #45936

---

**REQUEST FOR BOARD ACTION  
Committee of the Whole Meeting  
April 8, 2019**

**Subject:** Consideration of a Name and Dedication Date for the Lincolnshire Downtown Pocket Park (Village of Lincolnshire)

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**Action Requested:** Approval of a Name and Dedication Date for the Lincolnshire Downtown Pocket Park (Village of Lincolnshire)

---

**Originated  
By/Contact:** Bradford H. Woodbury, Public Works Director

---

**Referred To:** Mayor and Board of Trustees

---

**Summary / Background:**

The construction of the Pocket Park located within the Lincolnshire Downtown area was completed in the fall/winter of 2018. At its March 21, 2019 meeting, the Park Board provided input and recommendations on a name for the new park itself and is seeking final recommendation and input from Village Board. Proposed names considered by the Park Board include:

- Creekside Park
- City Center Park
- Indian Creek Park
- Indian Creek Pocket Park
- Half Day Park
- Lincolnshire Pocket Park

Additional input and ideas received from the Park Board at their March 21, 2019 meeting were as follows:

- Provide wayfinding signage off Milwaukee Avenue directing people to the park
- To engage the public on naming the park
  - potentially provide a Voting Area at the 2019 Memorial Day Ceremony
  - engage Lincolnshire area students in District 103 to solicit names
  - consider engaging the South Village Green Homeowner's Association

An alternate idea is to host an online survey via Survey Monkey. The online survey would give all members of the public a limited number of options to consider. This online survey could be sent to the parties listed above, as well as published this on the Village website, Facebook, e-news and twitter and allow all members to vote from a list potential names.

Staff also seeks Village Board input to identify a date to hold a dedication event for this park. The ceremony would be held prior to a Village Board meeting and the dates for consideration are as follows:

- Monday July 22, 2019 6:00pm
- Monday August 12, 2019 6:00pm
- Monday August 26, 2019 6:00pm
- Monday September 9, 2019 6:00pm

**Budget Impact:** None.

**Service Delivery Impact:**

The dedication and naming of this park is important part of the Village’s history as well as the residents of Lincolnshire.

**Recommendation:** Staff recommends consideration of a name, feedback on whether or not to survey residents and also approval of a date for a dedication ceremony.

Staff recommends consideration of a name, feedback on whether or not to survey residents and also approval of a date for a dedication ceremony.

**Reports and Documents Attached:**

- Pocket Park Location Map
- Photo of Area

<b>Meeting History</b>	
<b>Committee of the Whole Meeting</b>	<b>April 8, 2019</b>
<b>Park Board</b>	<b>March 21, 2019</b>

### Lincolnshire Pocket Park (location)



Map created on April 3, 2019.  
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